



County Hall  
Cardiff  
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Neuadd y Sir  
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## AGENDA

<b>Committee</b>	PLANNING COMMITTEE
<b>Date and Time of Meeting</b>	WEDNESDAY, 18 DECEMBER 2019, 1.30 PM
<b>Venue</b>	COMMITTEE ROOM 4 - COUNTY HALL
<b>Membership</b>	Councillor Jones (Chair) Councillors Lay, Ahmed, Asghar Ali, Driscoll, Gordon, Hudson, Jacobsen, Jones-Pritchard, Keith Parry, Sattar and Stubbs

### 1 Apologies for Absence

### 2 Minutes

To approve as a correct record the minutes of the meeting held on 20 November 2019.

### 3 Declarations of Interest

To be made at the commencement of the agenda item in question, in accordance with the Members Code of Conduct.

### 4 Petitions

Petitions have been received in relation to the following applications in accordance with Committee Meeting Procedural Rule 14.2. The petitioners have been advised of their right to speak and the applicants/agents of their right to reply:

Application no,19/02020/MNR, 29 Alexandra Road, Canton  
Application no,19/02075/MNR, Brodawel, Druidstone Road, Old St Mellons  
Application no,19/02512/MNR, Coffee Bunker, 176 St Fagans Road, Fairwater  
Application no,19/01802/DCH, 13 Heol-Y-Pavin, Llandaff

### 5 Development Control Applications

- a 19/02020/MNR, 29 Alexandra Road, Canton
- b 19/02075/MNR, Brodawel, Druidstone Road, Old St Mellons
- c 19/02245/MNR, 67 Beda Road, Canton

*This document is available in Welsh / Mae'r ddogfen hon ar gael yn Gymraeg*

- d 19/02512/MNR, Coffee Bunker, 176 St Fagans Road, Fairwater
- e 19/02825/MNR, 217-223 Newport Road, Plasnewydd
- f 19/01802/DCH, 13 Heol-Y-Pavin, Llandaff
- g 19/01521/MJR, Land to the Rear of 14 Queen Anne Square, Cathays Park

**6 Applications decided by Delegated Powers - November 2019**

**7 Urgent Items (if any)**

**8 Date of Next Meeting**

22 January 2020

**Davina Fiore**

**Director Governance & Legal Services**

Date: Thursday, 12 December 2019

Contact: Kate Rees, 029 2087 2427, [krees@cardiff.gov.uk](mailto:krees@cardiff.gov.uk)

## **WEBCASTING**

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**If you make a representation to the meeting you will be deemed to have consented to being filmed. By entering the body of the Chamber you are also consenting to being filmed and to the possible use of those images and sound recordings for webcasting and/or training purposes. If you do not wish to have your image captured you should sit in the public gallery area.**

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PLANNING COMMITTEE

20 NOVEMBER 2019

Present: Councillor Jones(Chairperson)  
Councillors Ahmed, Asghar Ali, Driscoll, Gordon, Jacobsen,  
Jones-Pritchard, Sattar and Stubbs

18 : APOLOGIES FOR ABSENCE

Councillors Hudson, Lay and Parry

19 : MINUTES

The minutes of the meeting held on 16 October 2019 were signed as a correct record.

20 : DECLARATIONS OF INTEREST

COUNCILLOR	ITEM	REASON
Driscoll	19/01917/MNR	Known to applicant and Objectors

21 : PETITIONS

- (i) Application no 19/02126/DCH, 1 The Fairway, Cyncoed
- (ii) Application no 19/02020/MNR, 29 Alexander Road, Canton
- (iii) Application no 19/01867/MNR, 6 Park Road, Whitchurch

In relation to (i) the petitioner spoke and the applicant responded.

22 : DEVELOPMENT CONTROL APPLICATIONS

The Committee considered the schedule of development control applications submitted in accordance with the Town and Country Planning Act 1990:

RESOLVED: That pursuant to this Committee's delegated powers the following development control applications be determined in accordance with the recommendations as set out in the report of the Director of Planning, Transport and Environment subject to any further amendments as detailed below and notification be given of the decisions in accordance with Section 70 of the Town & Country Planning Act 1990 or Section 74 of the Planning (Listed Building & Conservation Act 1990).

## APPLICATIONS GRANTED

19/01917/MNR – LLANDAFF

5A WAUN GRON ROAD

Proposed three bedroom dwelling extension to side of existing main dwelling.

Subject to an additional Condition 8 to read:

'Notwithstanding the details shown on the approved drawings numbered 0868/27 REV A (Proposed rear elevation) 0868/29 REV G (Proposed garden elevation) and 0868/21 REV D (Proposed first floor level plan) the first floor rear bedroom window shall not compromise a patio door opening with Juliet balcony. Prior to the commencement of development, amended details of writing by the Local Planning Authority and the development shall be completed in accordance with the approved details'

19/02126/DCH – CYNCOED

1 THE FAIRWAY

Single storey rear and two storey side extensions.

## **APPLICATIONS GRANTED ON EXECUTION OF A PLANNING OBLIGATION UNDER SECTION 106 OF THE TOWN AND COUNTRY PLANNING ACT 1990**

19/01331/MJR – LISVANE

LOZELLES, CHURCH ROAD

Proposed construction of block of 6 self contained flats.

## **APPLICATIONS DEFERRED**

19/02020/MNR – CANTON

29 ALEXANDRA ROAD

Conversion of two flats into 4 with single storey rear extension and loft conversion.

REASON: In order for a site visit to this location to take place.

## **APPLICATIONS WITHDRAWN**

19/01867/MNR – WHITCHURCH AND TONGWYNLAIS

6 PARK ROAD

Proposed change of use of existing A2 premises to A3 premises: refurbishment and extension of rear of building to provide three apartments and associated works including new flue.

REASON: Withdrawn by the applicant.

23 : APPLICATIONS DECIDED BY DELEGATED POWERS - OCTOBER 2019

Noted

24 : URGENT ITEMS (IF ANY)

None

25 : DATE OF NEXT MEETING - 18 DECEMBER 2019

Noted

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**PETITION, LOCAL MEMBER, MP& AM OBJECTIONS**

COMMITTEE DATE: 18/12/2019

APPLICATION No. **19/02020/MNR**

DATE RECEIVED: 23/07/2019

ED: **CANTON**

APP: TYPE: Full Planning Permission

APPLICANT: Mr Lubin

LOCATION: 29 Alexandra Road, Canton, Cardiff

PROPOSAL: CONVERSION OF TWO FLATS INTO 4 WITH SINGLE STOREY REAR EXTENSION AND LOFT CONVERSION.

**RECOMMENDATION 1:** That planning permission be **GRANTED** subject to the following conditions:

- 1 C01 – Statutory Time Limit
- 2 The development shall be carried out in accordance with the approved drawings numbered 483-P06A, 483-P07, 483-P08, 483-P09 & 483-P10.  
Reason: To ensure satisfactory completion of the development and for the avoidance of doubt in line with the aims of Planning Policy Wales to promote an efficient planning system.
- 3 Prior to occupation of the flats hereby approved, refuse storage shall be provided externally and shall thereafter be retained.  
Reason: To secure an orderly form of development and to protect the amenities of the area in accordance with Policy W2 of the Local Development Plan.
- 4 Prior to occupation of the flats hereby approved undercover and secure cycle storage shall be provided in accordance with drawing numbered 483-P06A and shall thereafter be retained.  
Reason: To ensure that secure and undercover cycle parking facilities are provided to encourage other modes of transport over the private car, in accordance with Policy T5 of the Local Development Plan.

**RECOMMENDATION 2:** Developers of all new residential units are required to purchase the bin provision required for each unit. The bins have to meet the Council's specifications and can be purchased directly by contacting Waste Management's commercial team (029 20717500).

1. **DESCRIPTION OF PROPOSED DEVELOPMENT**

- 1.1 This application was deferred by Planning Committee at its meeting on 20<sup>th</sup> November, in order to undertake a site visit. The visit took place on 9<sup>th</sup> December, and the application is reported back for determination.

- 1.2 A 2.6m long 1.8m wide single storey infill extension is proposed alongside an existing single storey extension, it would have a flat roof 2.9m high and be finished in render.
- 1.3 The application seeks planning permission to convert a two storey end of terrace dwelling from two to four flats and to erect single a storey rear extension.
- 1.4 A 2.6m long 1.8m wide single storey infill extension is proposed alongside an existing single storey extension, it would have a flat roof 2.9m high and be finished in render.
- 1.5 Two roof lights would also be inserted in the rear roof slope of the main roof.
- 1.6 Internally the accommodation would comprise of a flat (30m<sup>2</sup>) within the ground floor front, a flat (32m<sup>2</sup>) within the ground floor rear, a flat (43m<sup>2</sup>) within the first floor front and main roof space and a flat (30m<sup>2</sup>) within the first floor rear. All flats would be accessed via an existing entrance from Alexandra Road.

## 2. **DESCRIPTION OF SITE**

- 2.1 The site comprises a two storey end of terrace building accommodating two flats with an external area at the rear.

## 3. **SITE HISTORY**

- 3.1 10/01316/DCH – planning permission granted for retention of single storey rear extension.

### 3.2 Related History:

02/02748/W – planning permission granted and implemented for conversion of no. 48 Alexandra Road to two flats.

90/00276/W – planning permission granted and implemented for use of no. 53 Alexandra Road as two flats.

12/00037/DCO – planning permission granted and implemented for conversion of no. 66 Alexandra Road to two flats with rear dormer extension single storey extension part first floor and part two storey extension.

16/01363/E – planning permission granted and implemented for conversion of no. 67 Alexandra Road to two flats with single storey side extension to the rear annexe and balcony to rear.

## 4. **POLICY FRAMEWORK**

### 4.1 Relevant National Planning Guidance:

Planning Policy Wales (Edition 10, 2018)

Technical Advice Note 12: Design  
Technical Advice Note 15: Development & Flood Risk

4.2 Relevant Cardiff Local Development Plan (2006-2026) policies:

Policy KP5 (Good Quality and Sustainable Design)  
Policy EN14 (Flood Risk)  
Policy H5 (Subdivision or Conversion of Residential Property)  
Policy T5 (Managing Transport Impacts)  
Policy W2 (Provision for Waste Management Facilities in Development)

4.3 Relevant Supplementary Planning Guidance

Flat Conversions (2019).  
Managing Transportation Impacts (Incorporating Parking Standards) 2018.  
Waste Collection and Storage Facilities (2016).  
Residential Extensions & Alterations (2015).

5. **INTERNAL CONSULTEE RESPONSES**

5.1 Transportation – There would be 4 x secure cycle parking spaces to the rear of the property. The rear amenity space (inc bins & bikes) can only be accessed by all flats from the public highway off Eton Place. There is an existing drive/vehicle access to the rear of the property, but as this is to be removed and fails to provide a dropped kerb access across the footway, this will not need to be rectified. On-street car parking could be available near to the property. The cycle parking numbers are adequate to satisfy the minimum requirements although confirmation that the facility offers covered/sheltered parking is required.

5.2 Waste Management – The proposed storage area has been noted and is acceptable. Each flat will require the following for recycling and waste collections: 1 x 140 litre bin for general waste; 1 x 25 litre kerbside caddy for food waste; Green bags for mixed recycling (equivalent to 140 litres). The storage of which must be sensitively integrated into the design. Please advise the owner/applicant that since 27<sup>th</sup> July 2015, the developers of all new residential units are required to purchase the bin provision required for each unit. The bins have to meet the Council's specifications and can be purchased directly by contacting the Waste Management's commercial team on 029 20717500.

6. **EXTERNAL CONSULTEE RESPONSES**

6.1 Natural Resources Wales – initially requested a Flood Consequences Assessment (FCA) to demonstrate the risks and consequences of flooding can be managed to an acceptable level in accordance with Technical Advice Note 15. The planning application proposes high vulnerable development (2 flats into 4 flats). This represents the retention of a highly vulnerable development. Our Flood Risk Map, which is updated on a quarterly basis, confirms the site to be wholly within Zone C1 of the Development Advice Map (DAM) contained in

TAN15 and the 0.1% (1 in 1000 year) annual probability fluvial flood outlines of the River Taff.

Further to the submission of an FCA, NRW have advised as follows: The submitted FCA (Residential Development at 29 Alexandra Road, Canton, Cardiff, CF5 1NS, Flood Consequence Assessment by W M Clarke Ltd) shows that based on the Finished Floor Levels (FFL) of 8.7868m AOD the site is predicted to be flood free during both the 1% (1 in 100 year) flood event plus climate change allowance (CCA), and during the 0.1% (1 in 1000 year) flood event. We therefore we have no objection to the application as submitted.

## 7. **REPRESENTATIONS**

7.1 The application was publicised by letter. A petition of objection was received, signed by 69 residents with addresses in Cardiff, all of which are within the vicinity who could reasonably be affected by the matter. Full details are viewable online.

7.2 Objections have been received from the following addresses:

- nos. 27, 29, 32 Alexandra Road
- 19, 24, 40A Penllyn Road
- 49, 50 Theobald Road
- 15 Eton Place
- 20 Brecon Street.

Full details are viewable online, their comments are summarised as follows:

- a) *Overdevelopment/Internal Floor Areas of Flats;*
- b) *Parking;*
- c) *Loss of family accommodation;*
- d) *Noise/Sound Insulation;*
- e) *Refuse Storage;*
- f) *Standard of Amenity Space;*
- g) *Strain on Utilities/Sewerage System;*
- h) *Planning permission not gained for Existing Flats;*
- i) *Future use of the flats;*
- j) *Precedent for Further Conversions.*

7.3 Cllrs Stephen Cunnah, Susan Elsmore and Ramesh Patel object to the application, as follows:

*The Canton Councillors are objecting to this application because we consider it an overdevelopment. The proposal is to convert two flats into four with a single-storey extension and loft conversion. We clearly see this an over development for a house of this size. The flats are very small and barely meet the minimum space standards (Policy H5i of the LDP). If this development is approved it will have a detrimental effect on parking in the vicinity, which is already at maximum levels. The street has 75% residents parking. Canton clearly needs more family homes and not very small studio flats and two-bed*

*flats. The SPG states (Section 5.1) that Cardiff has a high shortage and this includes the provision for family accommodation. Converting this property into four flats will necessitate more bins. The bin collections are done from Alexandra Road and not Eton Place, and the front gardens are too small to accommodate all the bins on collection days. This would cause an obstruction on the highway (Alexandra Road). We also have a concern regarding the safety of the tenants and neighbours, as no fire exit has been provided for the upstairs flats or for the loft and there will be a kitchen upstairs. Please take this safety issue as a serious concern from the Canton Councillors. We would be grateful if the Planning Committee would consider refusing this application for the reasons given.*

7.4 Mark Drakeford AM & Kevin Brennan MP have commented as follows:

*We are aware of objections from local residents and local councillors to this planning application. In addition to the points which they will have already made, we are particularly concerned at the quality of accommodation which would result from the proposed intensification of use involved in this application. We hope the planning committee will consider very carefully the desirability of allowing space standards of this sort to become part of redevelopment in what is already one of the most densely populated areas in the whole of Wales.*

## 8. **ANALYSIS**

### 8.1 Policy

Subdivision of residential properties is supported by Policy H5 of the Local Development Plan, the subdivision of a residential building into smaller residential units can be an important source of housing. National Planning policy encourages the provision of additional housing stock within previously developed or existing residential land. Paragraph 4.2.17 of Planning Policy Wales states that *'Maximising the use of suitable previously developed and/or underutilised land for housing development can assist regeneration and at the same time relieve pressure for development on greenfield sites.'* It is noted that other residential dwellings within Alexandra Road have been subdivided to flats, however Policy H5 does not identify a threshold at which subdivision of residential dwellings is considered to reach saturation level. In this case it is noted that building is already in use as two flats. Accordingly, it is not considered that subdivision from two to four flats would adversely affect the general character of the area.

### 8.2 Residential Amenity

It is not considered that the single storey extension would be overbearing or un-neighbourly to justify concern for the Local Planning Authority. The extension would be positioned alongside an existing single storey structure adjacent to the adjoining highway at Eton Place, having no impact upon the amenity of adjoining dwellings.

### 8.3 Design

The scale of the single storey extension is considered acceptable, it would not be out of character to the locality and will provide a subservient addition to the building. The roof lights to the rear roof slope are considered visually acceptable.

### 8.4 Internal/External Space

The internal floor area of all flats satisfies the minimum requirements recommended by the Flat Conversions SPG at an internal height of 1.5m or more, and the outlook from all living areas is considered adequate. An external amenity area of approximately 23 square metres (excluding the cycle store) would be retained at the rear, accessible by all flats.

### 8.5 Waste Management

Refuse storage can be satisfactorily accommodated within the external area to the rear as confirmed by Waste Management, condition 3 is considered necessary to ensure storage is provided prior to beneficial occupation of the flats and thereafter retained.

### 8.6 Transportation

The Managing Transportation Impacts (Incorporating Parking Standards) SPG identifies no requirement for off street parking, and provision of cycle parking at a minimum ratio of 1 space per bedroom. The proposal is therefore considered to be car parking policy compliant with no off street parking spaces. The site is also in a sustainable location near to bus routes. Details of satisfactory secure and sheltered cycle storage have been submitted indicating provision of independently accessible cycle storage externally to the rear. Condition 4 is considered necessary to ensure the cycle storage is provided prior to beneficial occupation and thereafter retained.

### 8.7 Flood Risk

Natural Resources Wales have reviewed the submitted Flood Consequences Assessment and have no objection to the application as the site is predicted to be flood free during both the 1% (1 in 100 year) flood event plus climate change allowance (CCA), and during the 0.1% (1 in 1000 year) flood event. Having regard to the above is not considered that occupants would be subject to unacceptable flood risk, therefore the development is considered justified in accordance with section 6 of TAN15.

### 8.8 Representations

The representations received from the neighbouring residents, Cllrs Stephen Cunnah, Susan Elsmore, Ramesh Patel, Mark Drakeford AM and Kevin Brennan MP are noted. Specific issues are addressed as follows:

- a) Overdevelopment/Internal Floor Areas of Flats. It is considered that the provision of four flats is acceptable as detailed within the above amenity analysis. The proposed floor plans indicate that the internal floor area of all flats satisfies the minimum requirements recommended by the Flat Conversions SPG of 30 square metres for a studio flat and 35 square metres for a one bedroom flat.
- b) Parking. The proposal is compliant with the Access, Circulation and Parking Standards SPG with no off street parking spaces subject to provision of cycle parking as detailed within the transportation analysis.
- c) Loss of family accommodation. There is no planning policy/guidance in terms of thresholds/saturation of flat conversions. Although the Flat Conversions SPG recommends a range of flat sizes including two bedroom flats on the ground floor, this could not be relied on to support a refusal, particularly as in this instance there is an existing one bedroom flat at ground floor
- d) Noise/Sound Insulation. It is not considered that the proposal would result in adverse noise consequences. The proposal does not involve any material change of use as the existing use is residential. Planning Policy Wales Technical Advice Note 11 (Noise) advises that *'The Building Regulations impose requirements for sound insulation between dwellings, and were extended to cover sound insulation between converted flats by including provisions that are as close to new build as is practical. The Government considers that the Building Regulations are the most appropriate means of control for sound insulation in such conversions, and should be used in preference to planning condition'*. There are also statutory powers to control noise under the Control of Pollution Act 1990.
- e) Refuse Storage. It is considered that refuse storage containers could be suitably accommodated within the external area at the rear enclosed behind the boundary as detailed within the waste management analysis. It is also noted that refuse storage containers for the existing flats and other dwellings within this street are accommodated within the front forecourts, enclosed by front boundary walls/railings.
- f) Standard of Amenity Space. It is considered that the proposal would provide an adequate standard of amenity for future occupiers as detailed within the above amenity analysis. A private external amenity area would be retained at the rear, accessible by all flats. The provision of flats with mutual overlooking from their shared external amenity area is not uncommon, and is consistent with the existing scenario at this site.
- g) Strain on Utilities/Sewerage System. Connections would be considered by the building regulations procedure, it is not considered that the proposal would cause 'demonstrable' harm to the infrastructure. There is no policy or specific supplementary planning guidance which the Council could rely on to support a refusal.
- h) Planning permission not gained for Existing Flats. Although formal planning permission was not granted for the existing flats, it was established in 2009 that the flats were lawful due to the passage of time.
- i) Fire Exit from Upper Flats: Means of escape is considered by part M of the Building Regulations. Part M is not applicable where there is no material change of use (subdivision of an existing dwelling to flats is not a material change of use), and in such cases requires that any material alterations

must not make the building less satisfactory than it was before. Therefore, provision of a separate fire escape is not required in this instance.

- j) Future use of the flats. Selling or renting of flats is not a material planning matter, there is no planning control upon the length of a tenancy.
- k) Precedent for Further Conversions. All planning applications are considered on their own merit.

## 8.8 Other Legal Considerations

*Crime and Disorder Act 1998* – Section 17(1) of the Crime and Disorder Act 1998 imposes a duty on the Local Authority to exercise its various functions with due regard to the likely effect of the exercise of those functions on, and the need to do all that it reasonably can to prevent, crime and disorder in its area. This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable increase in crime and disorder as a result of the proposed decision.

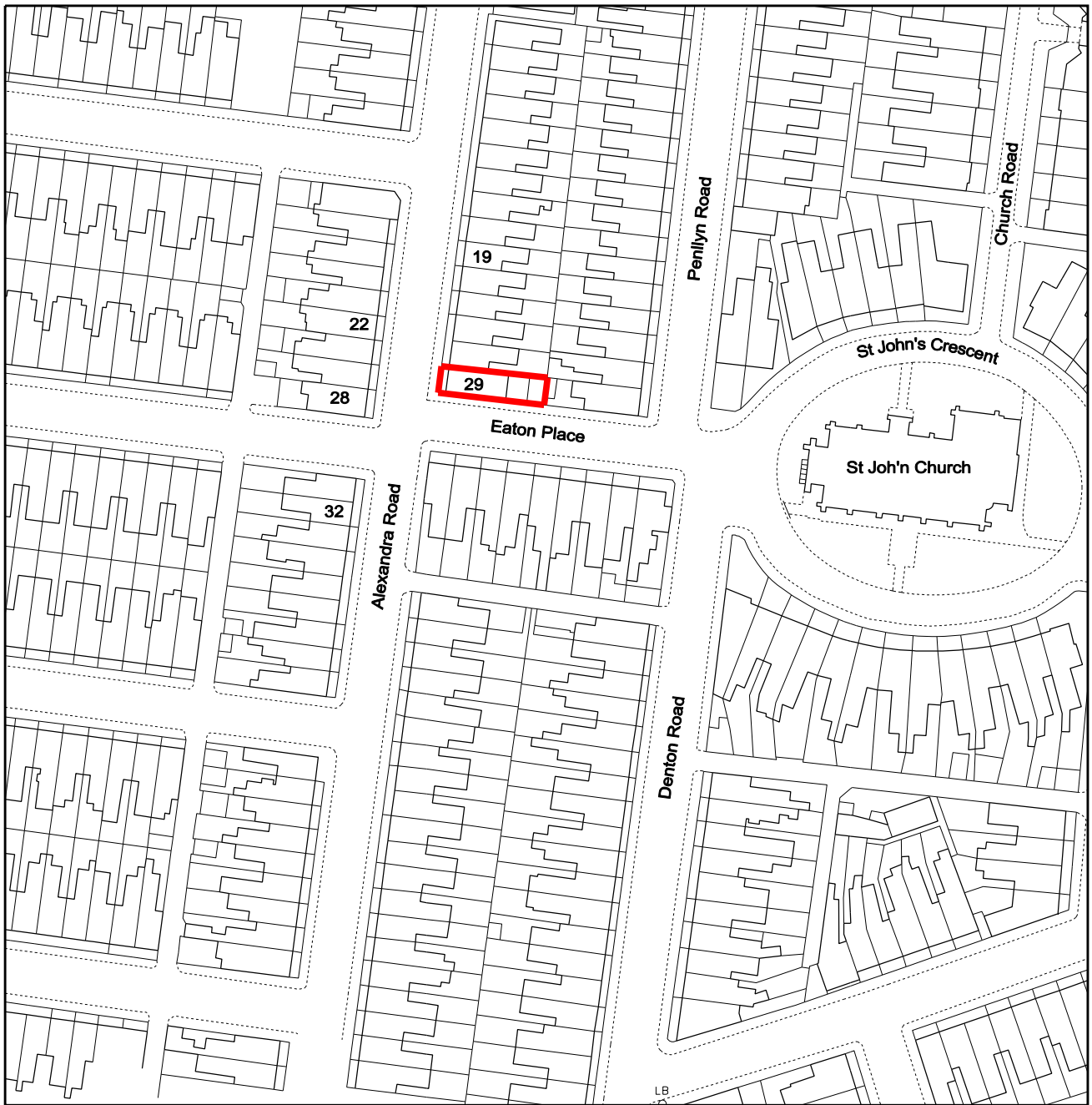
*Equality Act 2010* – The Equality Act 2010 identifies a number of ‘protected characteristics’, namely age; disability; gender reassignment; pregnancy and maternity; race; religion or belief; sex; sexual orientation; marriage and civil partnership. The Council’s duty under the above Act has been given due consideration in the determination of this application. It is considered that the proposed development does not have any significant implications for, or effect on, persons who share a protected characteristic.

*Well-Being of Future Generations Act 2016* – Section 3 of this Act imposes a duty on public bodies to carry out sustainable development in accordance with the sustainable development principle to act in a manner which seeks to ensure that the needs of the present are met without compromising the ability of future generations to meet their own needs (Section 5). This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable impact upon the achievement of wellbeing objectives as a result of the recommended decision.

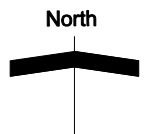
## 8.9 Conclusion

It is concluded that the application is acceptable in accordance with the planning policies listed, and is recommended that planning permission be granted, subject to conditions.





## Location Plan



Site: **29 Alexandra Road,  
Cardiff. CF5 1NS**

Title: **Location Plan**

Scale: 1:1250 @ A4  
Date: July 2019

Revision:

Drawing No: **L 00**



# Proposed Site Plan



**Richard Whitaker Architects Ltd**

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 Email: mail@richardwhitaker.com

Site: **29 Alexandra Road,  
 Cardiff. CF5 1NS**

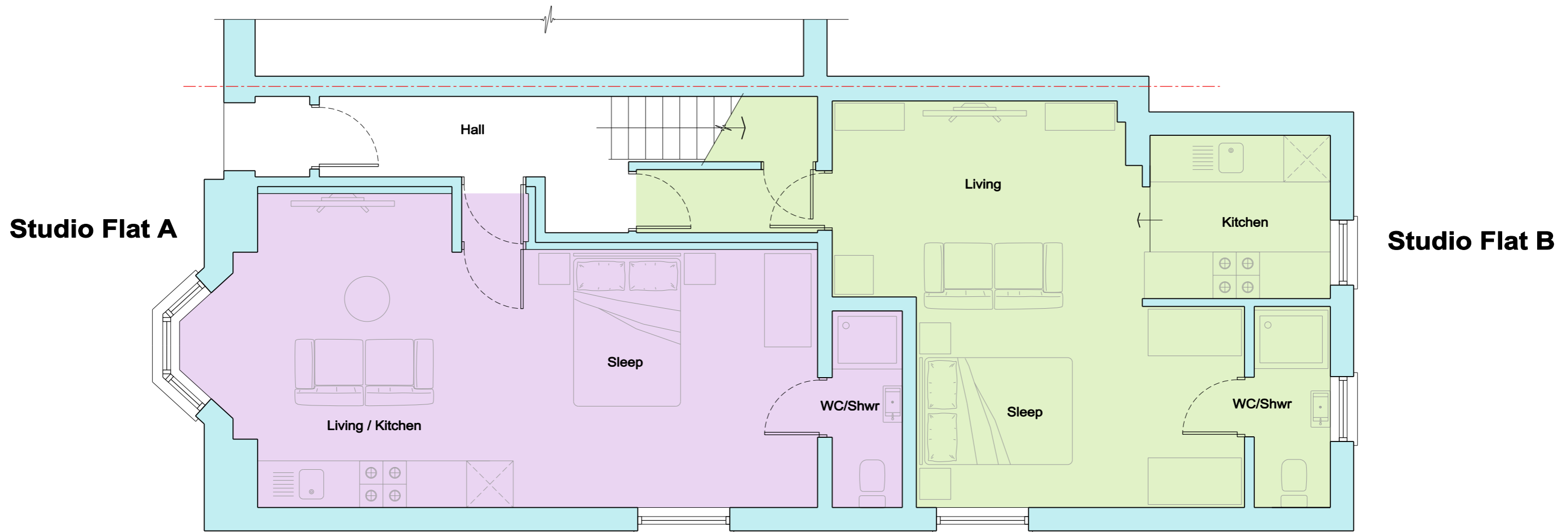
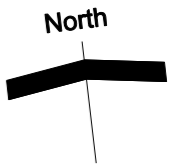
Scale:  
 1:250 @ A3

Date:  
 July 2019

Revision:  
 A

Title: **Proposed Site Plan**

Drawing No: **483 - P 06**



**Proposed Ground Floor Plan**

Unit:	Designation:	GIA: M <sup>2</sup>
<b>A</b>	Studio Flat	30.8
<b>B</b>	Studio Flat	32.8



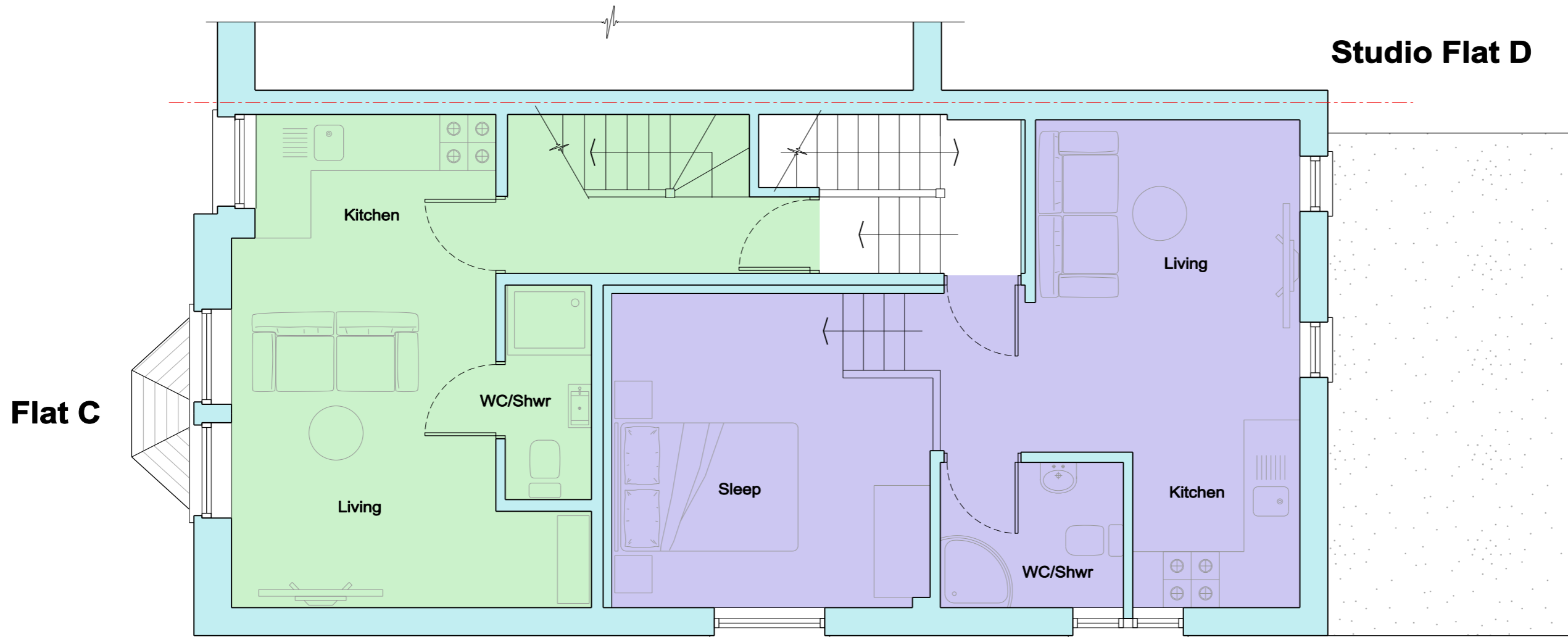
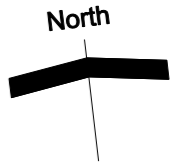
**Richard Whitaker Architects Ltd**  
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Site: **29 Alexandra Road,  
 Cardiff. CF5 1NS**

Scale: 1:50 @ A3  
 Date: June 2019

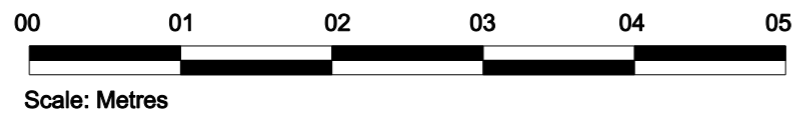
Title: **Proposed Ground  
 Floor Plan**

Drawing No: **483 - P 07**



**Proposed First Floor Plan**

Unit:	Designation:	GIA: M <sup>2</sup>
<b>C</b>	1B Flat	43.5
<b>D</b>	Studio Flat	30.3



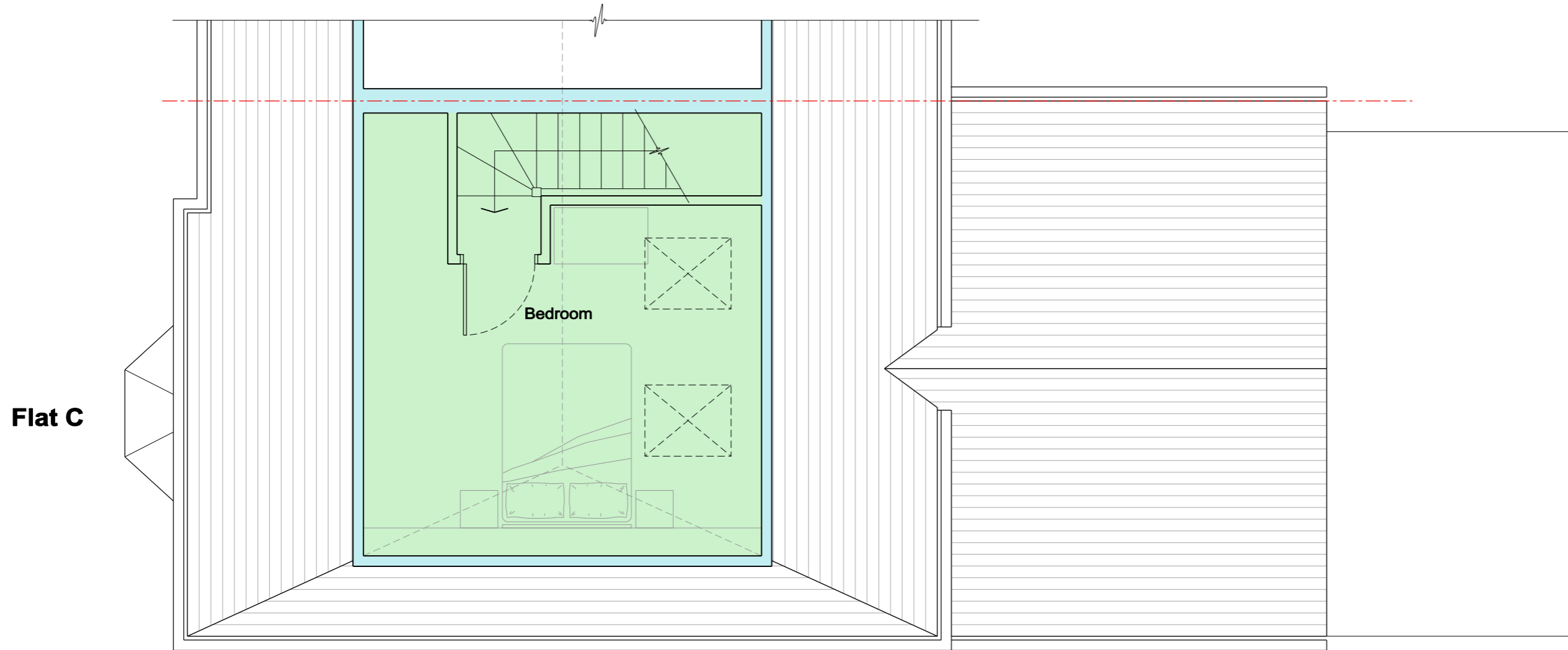
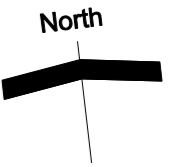
**Richard Whitaker Architects Ltd**  
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Site: **29 Alexandra Road,  
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Scale: 1:50 @ A3  
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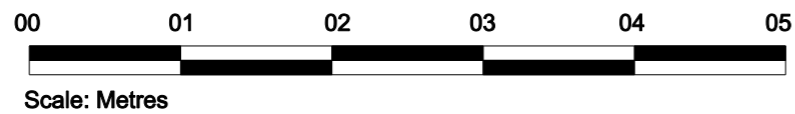
Title: **Proposed First  
 Floor Plan**

Drawing No: **483 - P 08**



### Proposed Second Floor Plan

 = Roof window over



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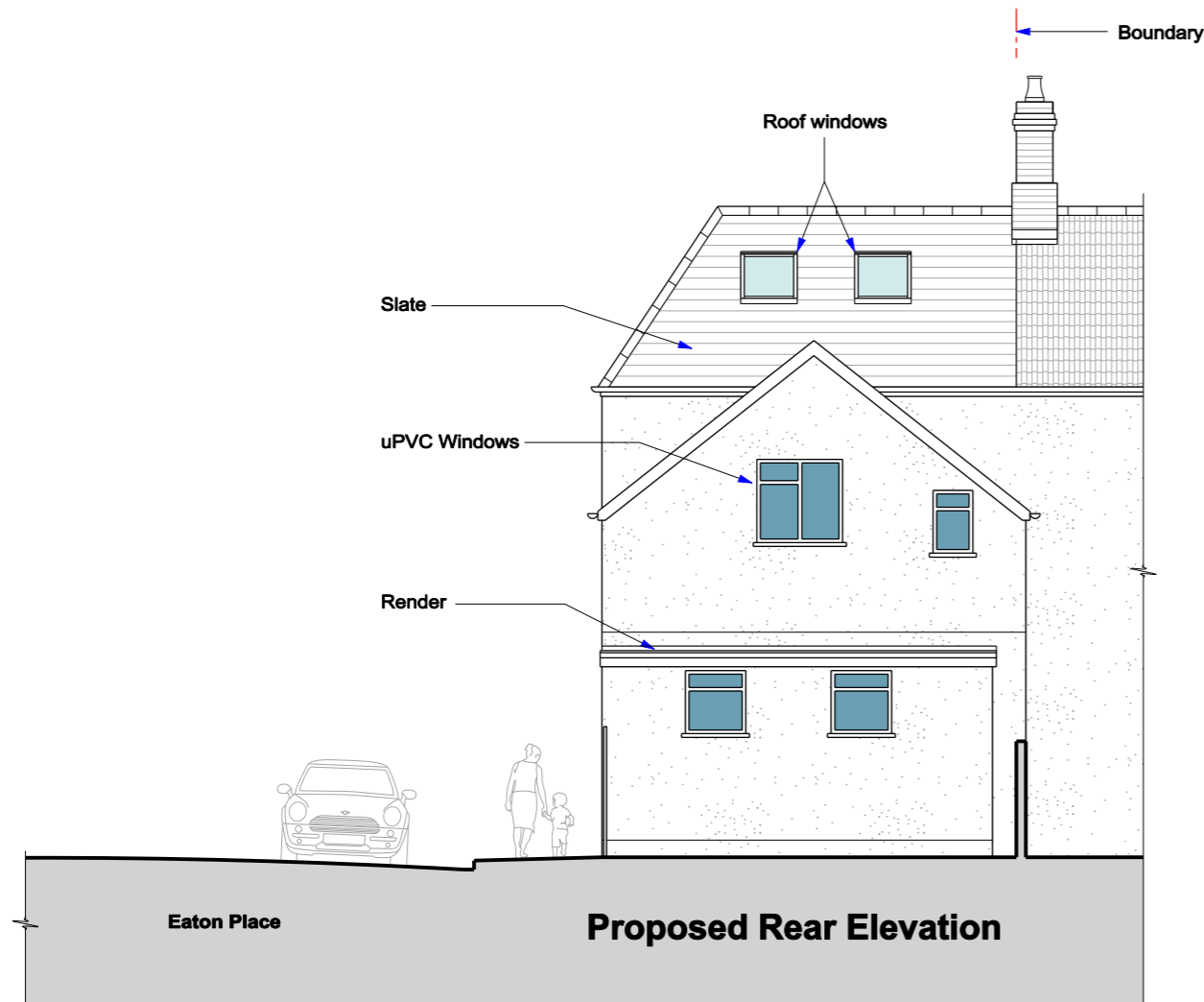
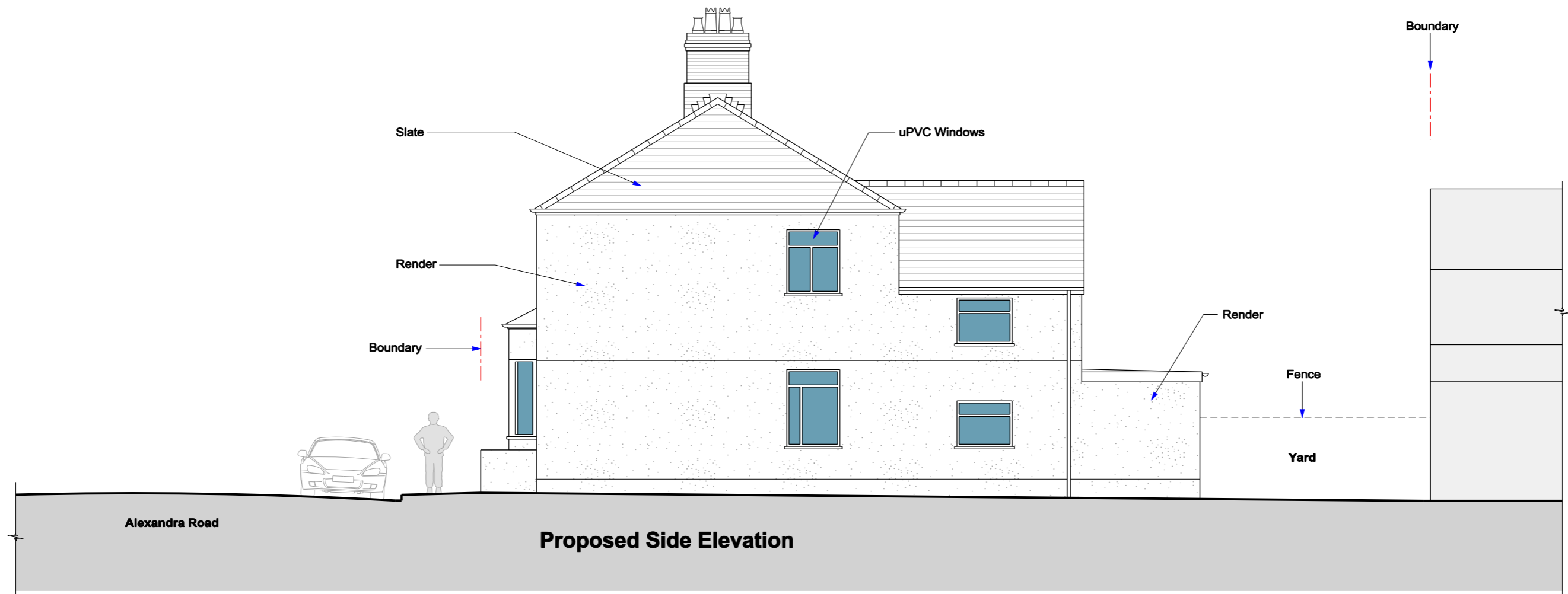
Site: **29 Alexandra Road,  
Cardiff. CF5 1NS**

Scale: 1:50 @ A3  
Date: June 2019

Revision:

Title: **Proposed Second  
Floor Plan**

Drawing No: **483 - P 09**



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Site: **29 Alexandra Road,  
 Cardiff. CF5 1NS**

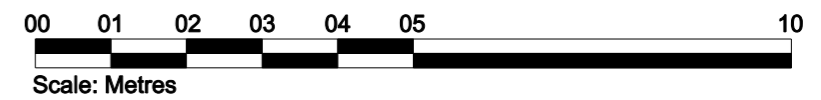
Title: **Proposed Elevations**

Scale:  
 1:100 @ A3

Date:  
 July 2019

Revision:

Drawing No: **P 10**



## PETITION

COMMITTEE DATE: 18/12/2019

APPLICATION No. **19/02075/MNR**

APPLICATION DATE: 30/07/2019

ED: **PONTPRENNAU/ST MELLONS**

APP: TYPE: Full Planning Permission

APPLICANT: Mr Parks

LOCATION: BRODAWEL, DRUIDSTONE ROAD, OLD ST MELLONS,  
CARDIFF, CF3 6XD

PROPOSAL: CONSTRUCTION OF NEW DETACHED DWELLING PLUS  
THE DEMOLITION OF AN EXISTING GARAGE WITH  
CONSTRUCTION OF A NEW GARAGE PLUS ALL  
ASSOCIATED WORKS

**RECOMMENDATION 1** : That planning permission be **GRANTED** subject to the following conditions :

1. C01 Statutory Time Limit
2. The development hereby permitted shall be carried out in accordance with the following approved plans:
  - A002 rev C
  - A003 rev A
  - A004 rev B
  - A005 rev A
  - A006 rev B
  - A007
  - site location plan rev A, dated 30/07/19.

Reason: To ensure satisfactory completion of the development and in line with the aims of Planning Policy Wales to promote an efficient and effective planning system.

3. Prior to the construction of the building above foundation level, details of the external finishing materials shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details.  
Reason: To ensure that the finished appearance of the development is in keeping with the area in accordance with Policy KP5: Good Quality and Sustainable Design of the Cardiff Local Development Plan.
4. Notwithstanding the submitted plans, details of the means of site enclosure

shall be submitted to and approved by the Local Planning Authority. The means of site enclosure shall be constructed in accordance with the approved details prior to the development being put into beneficial use.

Reason: In the interests of visual and residential amenity, in accordance with Policy KP5: Good Quality and Sustainable Design of the Cardiff Local Development Plan.

5. No equipment, plant or materials shall be brought onto the site for the purpose of development until full details of both hard and soft landscape works have been submitted to and approved in writing by the Local Planning Authority. These details shall include proposed finished levels, hard surfacing materials, planting plans (including schedules of plant species, sizes, numbers or densities, and in the case of trees, planting, staking, mulching, protection, soil protection and after care methods) and an implementation programme. The landscaping shall be carried out in accordance with the approved design and implementation programme.

Reason: To maintain and improve the appearance of the area in the interests of visual amenity, in accordance with Policy KP5: Good Quality and Sustainable Design of the Cardiff Local Development Plan.
6. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (Amendment) (Wales) Order 2013 (or any Order amending, revoking or re-enacting that Order) no further windows shall be inserted in the dwelling house hereby approved.

Reason: To ensure that the privacy of adjoining occupiers is protected in accordance with Policy KP5: Good Quality and Sustainable Design of the Cardiff Local Development Plan.
7. In the event that contamination is found at any time when carrying out the approved development that was not previously identified it shall be reported in writing within 2 days to the Local Planning Authority, all associated works shall stop, and no further development shall take place unless otherwise agreed in writing until a scheme to deal with the contamination found has been approved. An investigation and risk assessment shall be undertaken and where remediation is necessary a remediation scheme and verification plan must be prepared and submitted to and approved in writing by the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be submitted to and approved in writing by the Local Planning Authority. The timescale for the above actions shall be agreed with the Local Planning Authority within 2 weeks of the discovery of any unsuspected contamination.

Reason: To ensure that any unacceptable risks from land contamination to the future users of the land, neighbouring land, controlled waters, property and ecological systems are minimised, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other off-site receptors, in accordance with Policy EN13: Air, Noise, Light Pollution and Land Contamination of the Cardiff Local Development Plan.



8. Any topsoil [natural or manufactured], or subsoil, to be imported shall be assessed for chemical or other potential contaminants in accordance with a scheme of investigation which shall be submitted to and approved in writing by the Local Planning Authority in advance of its importation. Only material approved by the Local Planning Authority shall be imported. All measures specified in the approved scheme shall be undertaken in accordance with the relevant Code of Practice and Guidance Notes.

Subject to approval of the above, sampling of the material received at the development site to verify that the imported soil is free from contamination shall be undertaken in accordance with a scheme and timescale to be agreed in writing by the Local Planning Authority.

Reason: To ensure that the safety of future occupiers is not prejudiced, in accordance with policy 2.63 of the Cardiff Unitary Development Plan.

9. Any aggregate (other than virgin quarry stone) or recycled aggregate material to be imported shall be assessed for chemical or other potential contaminants in accordance with a scheme of investigation which shall be submitted to and approved in writing by the Local Planning Authority in advance of its importation. Only material approved by the Local Planning Authority shall be imported. All measures specified in the approved scheme shall be undertaken in accordance with the relevant Code of Practice and Guidance Notes.

Subject to approval of the above, sampling of the material received at the development site to verify that the imported material is free from contamination shall be undertaken in accordance with a scheme and timescale to be agreed in writing by the Local Planning Authority.

Reason: To ensure that the safety of future occupiers is not prejudiced, in accordance with Policy EN13: Air, Noise, Light Pollution and Land Contamination of the Cardiff Local Development Plan.

10. The car parking and manoeuvring spaces shown on the approved plans shall be provided prior to the development being brought into beneficial use and thereafter shall be maintained and shall not be used for any purpose other than the parking of vehicles.

Reason : To ensure that the use of the proposed development does not interfere with the safety and free flow of traffic passing along the highway, in accordance with Policy T5: Managing Transport Impacts of the Cardiff Local Development Plan.

11. The first floor en-suite window on the side elevation serving bedroom 4, the master bathroom window and en-suite window on the rear elevation serving bedroom 1, shall be non-opening below a height of 1.7 metres above internal floor level, glazed with obscure glass and thereafter be so retained. The high level 'slot' window serving the snug/library on the rear elevation of the proposed dwelling shall be constructed such that its sill level is a minimum of 1.7 metres above the internal floor level of the room. The recessed roof terrace in the rear roof plane shall be constructed as shown on drawing A004 rev B and thereafter be so retained.

The 'void' area shown on the first floor plan above the family room shall be retained at all times.

The roof lights on the side and rear elevation of the proposed dwelling shall be constructed such that their lower sill level is a minimum of 1.7 metres above the internal floor level.

Reason: To ensure that the privacy of adjoining occupiers is protected in accordance with Policy KP5: Good Quality and Sustainable Design of the Cardiff Local Development Plan.

12. Further details of the footway to be constructed along the site frontage, as shown on drawing A101 Rev G, shall be submitted to and approved by the Local Planning Authority. The footway shall be constructed in accordance with the approved details prior to the beneficial occupation of the dwelling and thereafter shall be retained at all times.

Reason: In the interests of highway and pedestrian safety, in accordance with Policy T5: Managing Transport Impacts of the Cardiff Local Development Plan.

13. No surface water and/or land drainage shall be allowed to connect directly or indirectly with the public sewerage network

Reason: To decrease the risk of flooding elsewhere and prevent hydraulic overload of the public sewerage system in accordance with Policy EN14: Flood Risk of the Cardiff Local Development Plan.

**RECOMMENDATION 2:** The applicant is informed that Welsh Water/Dwr Cymru has advised the site is crossed by a public sewer and that no operational development should be carried out within 3.0 metres either side of its centre line. For further information please contact [developer.services@dwrwymru.com](mailto:developer.services@dwrwymru.com)

**RECOMMENDATION 3:** To protect the amenities of occupiers of other premises in the vicinity attention is drawn to the provisions of Section 60 of the Control of Pollution Act 1974 in relation to the control of noise from demolition and construction activities. Further to this the applicant is advised that no noise audible outside the site boundary adjacent to the curtilage of residential property shall be created by construction activities in respect of the implementation of this consent outside the hours of 0800-1800 hours Mondays to Fridays and 0800 - 1300 hours on Saturdays or at any time on Sunday or public holidays. The applicant is also advised to seek approval for any proposed piling operations.

**RECOMMENDATION 4:** The contamination assessments and the effects of unstable land are considered on the basis of the best information available to the Planning Authority and are not necessarily exhaustive. The Authority takes due diligence when assessing these impacts, however you are minded that the responsibility for

- (i) determining the extent and effects of such constraints and;
- (ii) ensuring that any imported materials (including, topsoils, subsoils, aggregates and recycled or manufactured aggregates / soils) are chemically suitable for the proposed end use. Under no circumstances should

controlled waste be imported. It is an offence under section 33 of the environmental Protection Act 1990 to deposit controlled waste on a site which does not benefit from an appropriate waste management license. The following must not be imported to a development site:

- Unprocessed / unsorted demolition wastes.
  - Any materials originating from a site confirmed as being contaminated or potentially contaminated by chemical or radioactive substances.
  - Japanese Knotweed stems, leaves and rhizome infested soils. In addition to section 33 above, it is also an offence under the Wildlife and Countryside Act 1981 to spread this invasive weed; and
- (iii) the safe development and secure occupancy of the site rests with the developer.

Proposals for areas of possible land instability should take due account of the physical and chemical constraints and may include action on land reclamation or other remedial action to enable beneficial use of unstable land.

**RECOMMENDATION 6:** The applicant is advised to secure the consent of the Operational Manager, Asset Management (via 'highwaysnetworkmanagement@cardiff.gov.uk') prior to undertaking any works within the adopted highway in relation to the proposed development.

**RECOMMENDATION 7:** The applicant is advised that developers of all new residential units are required to purchase the bin provision required for each unit. The bins have to meet the Council's specifications and can be purchased directly by contacting the Waste Management's commercial team on 029 20717500.

**RECOMMENDATION 9:** On the 7<sup>th</sup> January 2019, Schedule 3 of the Flood and Water Management Act 2010 was enacted. This effects all new developments where the construction area is of 100m<sup>2</sup> or more. Cardiff Council is aware that your application for planning permission was validated after the recent legislative change in which Schedule 3 of the Flood and Water Management Act was enacted and therefore will be subject to surface water drainage proposals under the SAB application process.

It is recommended that the developer engages in consultation with the Cardiff Council SAB team, as the determining SuDS Approval Body (SAB), in relation to their proposals for SuDS features. To arrange discussion regarding this please contact [SAB@cardiff.gov.uk](mailto:SAB@cardiff.gov.uk)

## **1. DESCRIPTION OF PROPOSED DEVELOPMENT**

- 1.1 Planning permission is sought to construct a detached house in the former side garden of Brodawel, Druidstone Road. The application also proposes a new replacement garage for the existing house. A similar application for a detached dwelling and replacement garage at the site was granted on 18<sup>th</sup> October, 2018

under planning application ref.18/01114/MNR. The current application seeks permission for a dwelling of alternative design.

- 1.2 Details of the siting and appearance of the proposed dwelling and garage are shown on the plans submitted with the application. The dwelling comprises a two storey hipped roof structure with a single storey garage element attached to the side. Second floor accommodation is proposed in the roof space. The dwelling will sit broadly on the footprint of the approved dwelling. However, part of the structure will project approximately 2.0 metres closer to the rear boundary than previously approved. The design incorporates a cantilevered gable element to the first floor on the front elevation together with a pitched roof dormer. The cantilevered element projects approximately 1.1 metres forward of the ground floor. A recessed roof terrace is proposed in the rear roof plane. Finished materials comprise a mix of white render and reconstituted stone with fibre cement slates to the roof.
- 1.3 The proposed house rises to a main ridge height of approximately 8.6 metres with the lower element rising to approximately 5.0 metres. This reflects the height of the approved dwelling on the plot.
- 1.4 A separation distance of approximately 3.7 metres will be retained between the proposed dwelling and Brodawel. To the rear, the site backs onto the large garden of a neighbouring property at Pwll Coch. The rear elevation of the proposed dwelling is shown to be sited approximately 8.8 metres from the application site boundary with this property at its south eastern end.
- 1.5 The side elevation of the proposed dwelling will be positioned approximately 6.2 metres from eastern boundary of the site. This boundary comprises a hedge which runs alongside the driveway serving Pwll Coch and a single storey dwelling known as Ty Win. The front garden of Ty Win lies opposite the eastern boundary of the application site on the other side of the lane.
- 1.6 The proposed replacement garage for Brodawel will be of similar scale and position to that approved in 2018. It will be sited forward of the house and adjacent to the boundary with the front garden of the neighbouring property at Bron y Coed. The boundary is enclosed with a 1.8 metre high close boarded fence. The garage comprises a pitched roof structure rising to eaves and ridge heights of 2.1 metres and 4.7 metres respectively.
- 1.7 Vehicular access serving both houses will be from Druidstone Road, positioned mid-way along the site frontage, as existing. A new frontage footway will be provided as part of the development. To facilitate the provision of the footway, it is necessary to remove the boundary hedge. However, a new hedge is to be planted in mitigation. The access proposals reflect the arrangement approved in the previous planning application.
- 1.8 The subdivision of the plot, as proposed, would retain a side and rear garden of approximately 380 sq metres with the existing house and a garden area exceeding 250 sq metres is shown to be provided for the new dwelling.

## 2. **DESCRIPTION OF SITE**

- 2.1 Brodowel is a detached hipped roof property located on the south eastern side of Druidstone Road, approximately 600.0 metres from its junction with Tyr Winch Road.
- 2.2 Although Druidstone Road has the character of a rural lane, Brodowel lies within an established row of houses fronting this section of road. Planning permission has been granted on many occasions in recent years for the development of new houses in the gardens of existing properties at Druidstone Road and several of these developments have been carried out.
- 2.3 To the rear, the site backs onto the large garden of a neighbouring property at Pwll Coch. The eastern boundary of the site comprises a hedge which runs alongside the driveway serving Pwll Coch and a single storey dwelling known as Ty Win. The front garden of Ty Win lies opposite the eastern boundary of the application site on the other side of the lane.
- 2.4 To the west, Brodowel adjoins a large detached property known as Bron y Coed. The boundary with this neighbour is enclosed by a close boarded fence, approximately 1.8 metres in height.
- 2.5 On the opposite side of the road lies another detached house known as The Cottage, with fields on either side.
- 2.6 The application site has been subject to ground clearance works and the removal of vegetation, including a length of hedge adjacent to Druidstone Road. A detached garage has also been removed. A fence has been erected to separate the plot from Brodowel.

## 3. **SITE HISTORY**

- 3.1 18/01114/MNR: Demolition of existing attached garage and erection of new double garage for Brodowel and construction of new detached dwelling: Granted 18th October, 2018.
- 3.2 There is an extant planning permission to demolish the bungalow at the neighbouring Ty Win and the construction of a new two storey house sited forward of its existing footprint (ref:1900281/MNR).

## 4. **POLICY FRAMEWORK**

- 4.1 The Cardiff Local Development Plan 2006-2026 provides the local planning policy framework. Relevant policies include:

KP3 (B): Settlement Boundaries  
KP5: Good Quality and Sustainable Design  
KP8: Sustainable Transport  
KP 15: Climate Change

EN11: Water Sensitive Design  
EN13: Air, Noise, Light Pollution and Land Contamination  
T1: Walking and Cycling  
T5: Managing Transport Impacts  
EN1: Countryside Protection  
EN3: Landscape Protection  
EN7: Priority Habitats and Species  
EN8: Trees, Woodlands and Hedgerows

- 4.2 Supplementary Planning Guidance: Cardiff Infill Sites (2017);  
Supplementary Planning Guidance: Managing Transport Impacts (Incorporating Parking Standards) (2018);  
Supplementary Planning Guidance: Waste and Collection and Storage Facilities (2016);  
Supplementary Planning Guidance: Green Infrastructure (November, 2017);

4.5 **Planning Policy Wales Ed.10 (2018)**

1.17 Legislation secures a presumption in favour of sustainable development in accordance with the development plan unless material considerations indicate otherwise to ensure that social, economic, cultural and environmental issues are balanced and integrated.

2.8 Planning policies, proposals and decisions must seek to promote sustainable development and support the well-being of people and communities across Wales.

3.6 Development proposals must address the issues of inclusivity and accessibility for all.

3.7 Developments should seek to maximise energy efficiency and the efficient use of other resources (including land), maximise sustainable movement, minimise the use of non-renewable resources, encourage decarbonisation and prevent the generation of waste and pollution.

3.9 The layout, form, scale and visual appearance of a proposed development and its relationship to its surroundings are important planning considerations.

3.11 Local authorities are under a legal obligation to consider the need to prevent and reduce crime and disorder in all decisions that they take.

3.56 Development in the countryside should be located within and adjoining those settlements where it can best be accommodated in terms of infrastructure, access, habitat and landscape conservation. Infilling or minor extensions to existing settlements may be acceptable, in particular where they meet a local need for affordable housing or it can be demonstrated that the proposal will increase local economic activity. However, new building in the open countryside away from existing settlements or areas allocated for development in development plans must continue to be strictly controlled. All new development should be of a scale and design that respects the character of the surrounding area.

4.1.34 New development must provide appropriate levels of secure, integrated, convenient and accessible cycle parking and changing facilities.

4.1.52 Planning authorities must require good standards of car parking design, which do not allow vehicles to dominate the street or inconvenience people walking and cycling. Car parking should be overlooked by surrounding properties, to provide natural surveillance.

4.1.53 Parking standards should be applied flexibly and allow for the provision of lower levels of parking and the creation of high quality places.

4.2.22 Planning authorities will need to ensure that in development plans and through the development management process they make the most efficient use of land and buildings in their areas. Higher densities must be encouraged on sites in town centres and other sites which have good walking, cycling and public transport links.

4.2.23 Infill and windfall sites can make a useful contribution to the delivery of housing. Proposals for housing on infill and windfall sites within settlements should be supported where they accord with the national sustainable placemaking outcomes.

5.12.9 Adequate facilities and space for the collection, composting and recycling of waste materials should be incorporated into the design and, where appropriate, layout of any development as well as waste prevention measures at the design, construction and demolition stage.

6.4.22 The presence of a species protected under European or UK legislation, or under Section 7 of the Environment (Wales) Act 2016 is a material consideration when a planning authority is considering a development proposal which, if carried out, would be likely to result in disturbance or harm to the species or its habitat and to ensure that the range and population of the species is sustained. Planning authorities should advise anyone submitting a planning application that they must conform with any statutory species protection provisions affecting the site, and potentially the surrounding area, concerned.

6.4.25 Planning authorities should protect trees, hedgerows, groups of trees and areas of woodland where they have ecological value, contribute to the character or amenity of a particular locality, or perform a beneficial and identified green infrastructure function.

6.6.27 Planning authorities should be aware of the risk of surface water flooding and ensure developments are designed and planned to minimise potential impacts. Development should not cause additional run-off, which can be achieved by controlling surface water as near to the source as possible by the use of SuDS.

## **5. INTERNAL CONSULTEE RESPONSES**

- 5.1 The Operational Manager, Transportation has no objection to the application subject to appropriate conditions. An informative is also requested advising the application of the need to secure the consent of the Operational Manager, Network Management prior to undertaking any works in adopted highway.
- 5.2 The Operational Manager (Drainage Management) advises that the applicant will require SAB approval and recommends that the applicant engages in pre application discussions with Cardiff Council SAB team as the determining SuDS Approval Body (SAB).
- 5.3 Shared Regulatory Services (Contaminated Land) requests the inclusion of conditions and informative statements in accordance with CIEH best practice and to ensure that the safety of future occupiers is not prejudiced in accordance with policy EN13 of the Cardiff Local Development Plan.

- 5.4 The Operational Manager, Waste Management advises that the proposed dwelling will require the following for recycling and waste collections:

1 x 140 litre bin for general waste  
1 x 240 litre bin for garden waste  
1 x 30 litre bin for grass ( to be implemented in early 2020)  
1 x 25 litre kerbside caddy for food waste  
Green bags for mixed recycling (equivalent to 140 litres).

The Officer also advises that the bins have to meet Council's specifications and can be purchased directly by contacting the Waste Management's commercial team.

- 5.5 The Council's Ecologist comments as follows:

*In relation to the suggestion that Dormice may be impacted by these proposals, there is a hedge at the rear of this property and I note from the Proposed Ground Floor Layout drawing (Revision C) that this feature will be retained as part of the proposed development. Dormice are present in this area and if they are using this hedgerow then removing this feature would significantly impact an area of habitat and of habitat connectivity linking the woodland opposite 'Jindabyne' and the network of hedges and mature gardens behind the Druidstone Road properties. However, routine hedgerow management should not impact significantly upon dormice, and any impact upon this species caused by works to the hedgerow outside of the planning system is a matter between the landowner and Natural Resources Wales.*

## 6. **EXTERNAL CONSULTEE RESPONSES**

- 6.1 Welsh Water/ Dwr Cymru advises that the site is crossed by a public sewer. The Company comments that under the Water Industry Act 1991, it has rights of access to its apparatus at all time. If the Council is minded to grant planning permission, the following condition is recommended:

*No surface water and/or land drainage shall be allowed to connect directly or indirectly with the public sewerage network.*

- 6.2 Glamorgan Gwent Archaeological Trust advises that it is unlikely that any archaeological deposits would be encountered during the proposed development.

## 7. **REPRESENTATIONS**

- 7.1 Local members have been notified of the application and have advised that due to a conflict of interest, they will not be commenting on the application.
- 7.2 A 91 signature petition of objection has been submitted formally requesting that the application be determined by Planning Committee and requesting a site visit before determination.



7.3 The occupier of The Cottage objects for the following reasons (summary):

The occupier comments that the proposed mass of the dwelling is larger than the previous proposed dwelling which he believes was rejected.

The occupier states that a third floor will look directly into his property including bedroom windows which is considered to be a complete invasion of privacy.

The occupier comments that the proposed roof line is above that of the current house so that the new dwelling would sit higher than that detailed in the previous application.

The occupier would have expected much greater explanation and justification for the significant increase in the overall footprint of the property contained within the new design.

The occupier comments with regard to ecological considerations that there appears to have been no thought given to providing a replacement habitat for dormice and other wildlife. There is no reference to a bat survey in the new application whereas the previous application referred to a survey having occurred.

The occupier queries the space available on the site to accommodate the footway, replacement hedge and new dwelling.

The occupier states that the house would sit at an angle and would encourage the viewing into The Cottage which the occupier would find unacceptable.

7.4 The occupier of Ty Win objects to the application. The Occupier understands that the previous application was agreed only due to a reduction in the size of the house and a sympathetic design. The occupier considers the new proposal to be vastly larger, dominating the plot. The occupier is also concerned that the rear balcony will have a direct view both into his property including his garden and that of his neighbours resulting in a loss of privacy. He states that the plans do not accurately show or demonstrate the impact of the proposal or how vehicles will be able to access and exit the property in a safe way.

7.5 Old St Mellons Community Council has been notified of the application. No representations have been received.

7.6 Neighbouring occupiers have been notified of amended plans.

8. **ANALYSIS**

8.1 Planning permission is sought to construct a detached house in the former side garden of Brodowel, Druidstone road. The application also proposes a new replacement garage for the existing house. A similar application for a detached dwelling and replacement garage at the site was granted on 18<sup>th</sup> October, 2018 under planning application ref.18/01114/MNR. The current application seeks permission for a dwelling of alternative design

- 8.2 The main planning issues relate to the acceptability of the proposed development on land use policy grounds, the effect of the proposed development on the character and appearance of the area and the general amenities of neighbouring occupiers, the acceptability of the proposal on transportation grounds and the effect on nature conservation.
- 8.3 The site lies outside the settlement boundary as defined on the Proposals Map of the Cardiff Local Development Plan. Policy KP3 (B): Settlement Boundaries seeks to control inappropriate development in the countryside outside the identified settlement boundaries as shown on the Proposals Map
- 8.4 Policy EN1: Countryside Protection provides more guidance on the interpretation of this policy approach stating that: *there will be a presumption against development in the countryside, beyond the settlement boundaries identified of the Proposals Map, except where it can be justified for agricultural and forestry needs or it is essential for facilitating sustainable access to and enjoyment of the countryside including appropriate outdoor recreation and tourism uses.*
- 8.5 The aim of Policy EN1 is to ensure that development within the countryside is strictly controlled to protect and enhance Cardiff's natural heritage and setting.
- 8.6 Policy EN3 Landscape Protection states that: *development will not be permitted that would cause unacceptable harm to the character and quality of the landscape and setting of the City.*
- 8.7 National planning policy guidance is contained in Planning Policy Wales (PPW). Paragraph 3.56 relating to development in the countryside states that:
- Development in the countryside should be located within and adjoining those settlements where it can be best be accommodated in terms of infrastructure, access and habitat and landscape conservation. Infilling or minor extensions to existing settlements may be acceptable, in particular where it meets a local need for affordable housing, but new building in the open countryside away from existing settlements or areas allocated for development in development plans must continue to be strictly controlled. All new development should respect the character of the surrounding area and should be of appropriate scale and design.*
- 8.8 Notwithstanding the location of the application site some distance outside the settlement boundary, it comprises part of the former garden of Brodawel. The proposed dwelling would be located within an established row of houses fronting this section of Druidstone Road. As a consequence, it would not add to unacceptable ribbon development along the road and is considered, in principle, to comply with the guidance outlined in PPW in relation to sensitive filling of small gaps. Whilst Ty Win to the north east, currently comprises a relatively modest single storey dwelling, there is an extant planning permission to demolish the bungalow and construct a new two storey house sited forward of its existing footprint.

- 8.9 The application site benefits from an extant planning permission for a detached dwelling the determination of which took these circumstances into account. They are considered to remain applicable to the assessment of the current application and on land use policy grounds, the revised application remains acceptable.
- 8.10 Policy KP5: Good Quality and Sustainable Design of the Cardiff Local Development Plan states that... *all new development will be required to be of a high quality, sustainable design and make a positive contribution to the creation of distinctive communities, places and spaces by (inter alia):*  
*(i) responding to the local character and context of the built and landscape setting so that layout, scale, form, massing, height, density, colour, materials, detailing and impact on the built and natural heritage are all addressed within development proposals;*  
*(x) ensuring no undue effect on the amenity of neighbouring occupiers and connecting positively to surrounding communities;*
- 8.11 Planning Policy Wales at paragraph 4.11.9 states that *'visual appearance, scale and relationship to surroundings and context are material planning considerations.*
- 8.12 The Residential Infill sites SPG advises at paragraph 2.13 that:  
  
*'It is important to strike a balance between maintaining the established positive character of a residential street and introducing additional housing. To avoid a 'town cramming' effect, any proposals must:*  
  
*Maintain a useable amenity space or garden for new as well as any existing dwellings/occupiers.*  
*Maintain an established spacing between buildings that respects the pattern of layout in the vicinity of the site.*  
*Maintain appropriate scale and massing which respects buildings in the vicinity of the site.*  
*Respect the frontage building line and respond to the existing street scene.*
- 8.13 At paragraph 3.8 the SPG states that:  
  
*Infill development needs to be sensitive to its context. It is important that in residential areas where there is a clear existing pattern and form of development, that new buildings, landscaping and boundary treatments (e.g. gates, railings, walls and hedges) complement the character of the surroundings.*
- 8.14 At paragraph 4.11 the SPG states that:  
  
*To safeguard the amenity of existing residents, proposals must not result in unacceptable harm regarding the level of overbearing, overshadowing or overlooking of neighbouring properties.*
- 8.15 The dwelling comprises a two storey hipped roof structure with a single storey garage element attached to the side. It mainly differs in appearance from the approved dwelling in that second floor accommodation is proposed in the roof

space and the structure incorporates a cantilevered gable element to the first floor together with a pitched roof dormer. The dwelling will sit on a similar footprint to the approved dwelling all be it that part of the structure will project approximately 2.0 metres closer to the rear site boundary than previously approved. It will be of similar ridge height and comparable to the ridge height of the existing Brodowel, as shown on the submitted plans.

- 8.16 Given the evident variety in house design in Druidstone Road, it is not considered that the appearance and scale of proposed dwelling would cause unacceptable harm to the character of the area. The proposed design remains essentially traditional in form but incorporates contemporary design features. Whilst accommodation is proposed at second floor level, this is contained within the roof space so that the overall height of the house reflects the previously approved scheme.
- 8.17 Finished materials comprise a mix of white render and reconstituted stone with fibre cement slates to the roof. Rendered elevations are common along Druidstone Road. A stone finish is evident on the front enclosure at The Cottage opposite. Since the previous planning application was determined, Brodowel has itself been refurbished with sections of brickwork on the building having been rendered.
- 8.18 A distance of approximately 3.7 metres would be retained between Brodowel and the proposed dwelling which is considered sufficient to ensure a reasonable visual separation between them and to safeguard the living conditions of the neighbouring occupiers.
- 8.19 A frontage footway and replacement hedge is proposed. The layout plan shows sufficient space to accommodate this new planting, the details of which would be addressed by condition. This aspect of the scheme reflects the arrangement approved in the previous planning application.
- 8.20 The proposed replacement garage for the existing house will be of similar scale and siting to that approved in 2018; a distance of approximately 11.0 metres would be retained between the structure and the front of the site which is considered sufficient to ensure that its siting would not appear incongruous in the street scene.
- 8.21 The subdivision of the plot, as proposed, retains a side and rear garden of approximately 385 sq. metres with the existing house and a garden exceeding approximately 250 sq. metres would be provided for the new dwelling. Consequently, the provision of private amenity areas significantly exceeds the Council's guidelines as set out in the Infill Sites SPG.
- 8.22 The likely effect of the proposed development on the living conditions of neighbouring occupiers has been carefully considered.
- 8.23 The application site backs onto the garden of the neighbouring property at Pwll Coch and is screened by a hedge/vegetation. The rear elevation of the proposed dwelling at its south eastern end is shown to be sited approximately 8.8 metres

from the application site boundary. The position of this boundary has been disputed by the owners of Pwll Coch.

8.24 The footprint of the proposed dwelling has been previously set out on site by the applicant to establish its position in relation to the existing dwelling at Brodawel, Druidstone Road and existing boundary hedges.

8.25 Where the position of the rear boundary remains in dispute, this is primarily a land ownership matter between the neighbouring parties. However, it raises the following issues:

i) A possible reduction in the amenity/garden space when compared to that shown on the submitted plans. Notwithstanding this possibility, it is considered that adequate garden space would remain with the dwelling, significantly exceeding the guidelines set out in the Infill sites SPG.

ii) Overlooking/Privacy. As proposed, first floor windows in the rear elevation serve a bathroom and en-suite to be obscurely glazed, a snug/study where the window will be positioned at high level and an internal double height void area. The neighbouring garden would not be unacceptably overlooked from these windows notwithstanding the disputed position of the boundary. A recessed roof terrace is proposed in the rear roof plane. The front of this roof terrace has been revised and is shown on the submitted plans to be at a distance of 10.5 metres from the application site boundary. The proposed window accessing the roof terrace would be sited approximately 2.0 metres further back into the roof recess therefore positioning it further away from the boundary hedge.

Having regard to the angle of view and the separation distances between the proposed terrace and the dwellings at Pwll Coch/Ty Win, it is not considered that there would be material harm to the living conditions of the adjoining residents such that the application should be refused on this ground.

iii) The initial details submitted with the application showed a possible location for a soakaway in the rear eastern corner of the application site. However, a drainage scheme has yet to be agreed for the development. The Council's Drainage Officer has advised that the scheme will require SAB approval and recommends that the applicant engages in pre application discussions with Cardiff Council SAB team as the determining SuDS Approval Body (SAB).

8.26 Notwithstanding the precise position of the legal boundary, it is considered that this should not prevent the determination of the application on its planning merits.

8.27 The proposed dwelling will be sited at least 40.0 metres from the front elevation of The Cottage located on the opposite side of Druidstone Road. Having regard to this distance, it is not considered that it would be overbearing result in an unacceptable loss of privacy having regard to the Council's guidelines.

8.28 The side elevation of the proposed dwelling would be positioned 6.2 metres from eastern boundary of the site, as previously approved. This boundary comprises a

hedge which runs alongside the driveway serving Pwll Coch and a single storey dwelling known as Ty Win. The front garden of Ty Win lies opposite the eastern boundary of the application site on the other side of the lane. It is not considered that the proposed house would have an unduly overbearing or dominating effect on this neighbouring property (or on the approved replacement two storey dwelling on the site) or would impact unacceptably on privacy or light to habitable rooms having regard to the distance between them and the Council's guidelines on these matters.

- 8.29 The proposed replacement garage for Brodawel will be of similar scale and siting to that approved in 2018. It will be sited forward of the house and adjacent to the boundary with the front garden of the neighbouring property at Bron y Coed. The boundary is enclosed with a 1.8 metre high close boarded fence. The garage comprises a pitched roof structure rising to an eaves and ridge height of 2.1 metres and 4.7 metres respectively. It is not considered that it would impact on the living conditions of the adjoining occupiers to the extent that it would constitute an unneighbourly form of development.
- 8.30 The Operational Manager, Transportation has raised no objections to the application on parking, highway safety subject to the provision of adequate off-street parking/manoeuvring and a 1.8 metre wide frontage footway, as advised in relation to the previous planning application.
- 8.28 Connectivity of the site with existing neighbourhoods by means of travel other than the car would be restricted having regard to the distance from a regular bus service (stop). However, numerous planning permissions have been granted in recent years for dwellings within gardens of properties along Druidstone Road. Although Druidstone Road has limited footway provision, it is illuminated. In considering the transportation impact of such applications, the main highway consideration is that in the absence of footways, the visibility for vehicles emerging onto the road is often poor. The Operational Manager, Transportation has taken the view that, on balance, the benefit achieved in terms of highway safety, by securing improved visibility for existing accesses outweighs the limited increase in the numbers of vehicle/pedestrian movements which would accrue from the new dwellings and that a further consequential benefit would be the incremental improvement that would be achieved to pedestrian facilities generally along the road. It is also material that the site has an extant planning permission for a dwelling.
- 8.29 With regards to the issue of dormice, the Council's ecologist's advice on this matter is set out in paragraph 5.5 of this report.
- 8.30 The application is considered acceptable on planning grounds and approval is recommended subject to the attached conditions.

## 9. **OTHER CONSIDERATIONS**

### 9.1 *Equality Act 2010*

The Equality Act 2010 identifies a number of 'protected characteristics', namely age; disability; gender reassignment; pregnancy and maternity; race; religion or

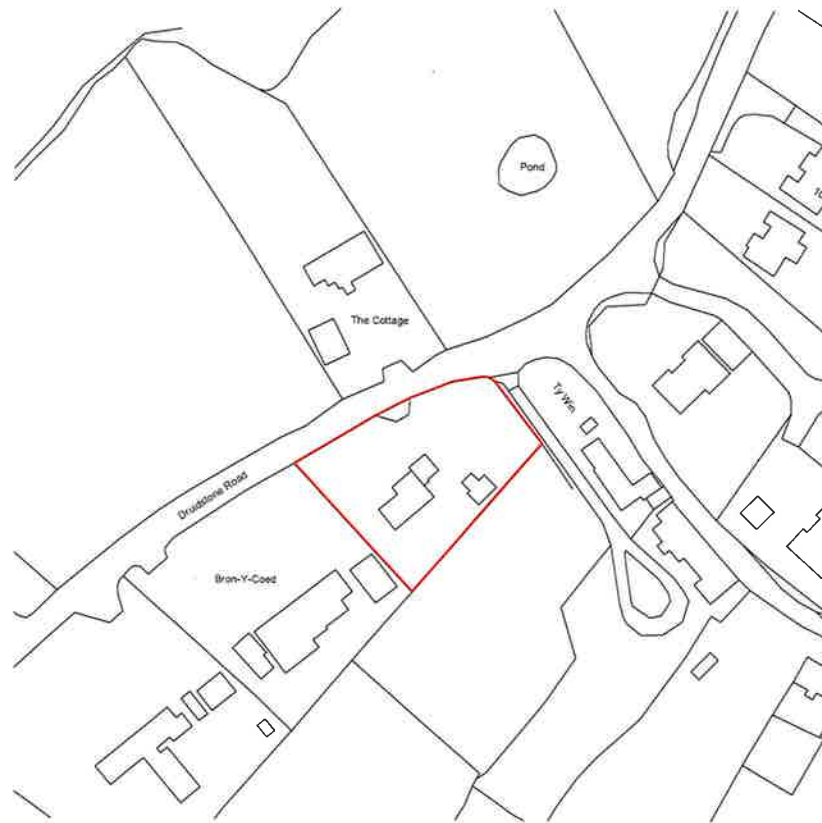
belief; sex; sexual orientation; marriage and civil partnership. The Council's duty under the above Act has been given due consideration in the determination of this application. It is considered that the proposed development does not have any significant implications for, or effect on, persons who share a protected characteristic, over and above any other person.

#### 9.2 *Well-being of Future Generations (Wales) Act 2015*

Section 3 of this Act imposes a duty on public bodies to carry out sustainable development in accordance with the sustainable development principle to act in a manner which seeks to ensure that the needs of the present are met without compromising the ability of future generations to meet their own needs (Section 5). This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable impact upon the achievement of wellbeing objectives as a result of the recommended decision.

#### 9.3 *Environment (Wales) Act 2016*

The Environment (Wales) Act 2016 imposes a duty on the Local Authority to seek to maintain and enhance biodiversity in the proper exercise of its functions and in doing so to promote the resilience of ecosystems. It is considered that the proposed development does not have any significant implications for, or effect on, biodiversity.



Site location  
1 : 1250



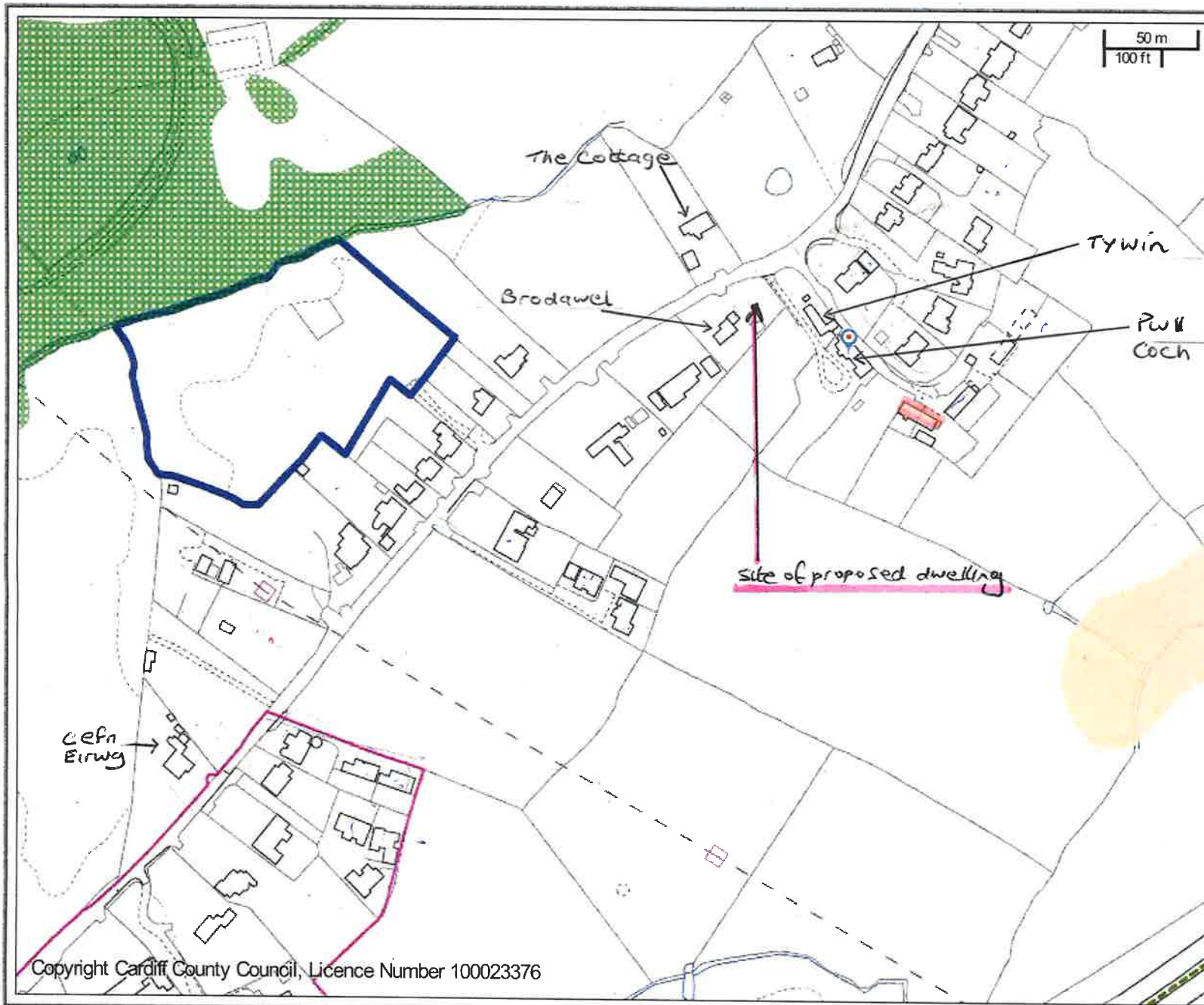
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REV	DATE	DESCRIPTION
A	30.07.2019	Redline updated

PROJECT	DRAWING	SCALE	DATE	STATUS	REVISION
Druadhine Road / Old St Mellons	Site location plan	1 : 1250 @ A3	11.03.2019	PLANNING	A





**City of Cardiff Council**  
**Cyngor Dinas Caerdydd**



CHIEF EXECUTIVE  
 Paul Orders  
 County Hall  
 Atlantic Wharf  
 Cardiff CF10 4UW  
 Tel: 029 20872000

**Title**  
 Scale: 1:3000  
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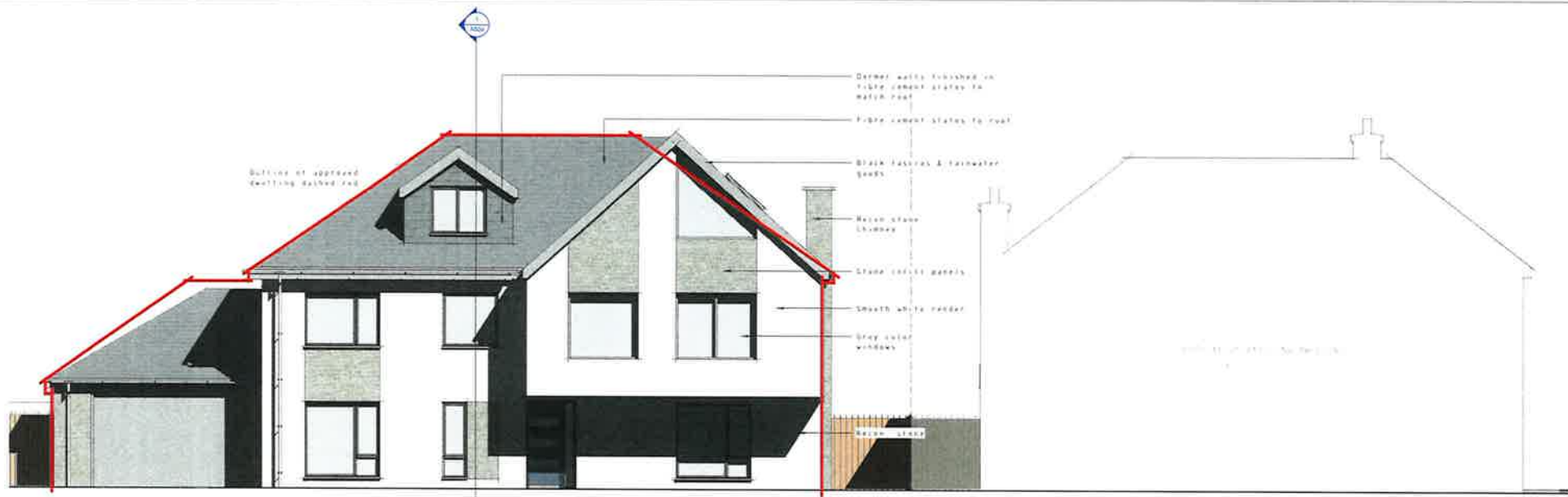
Ground floor layout  
1:200



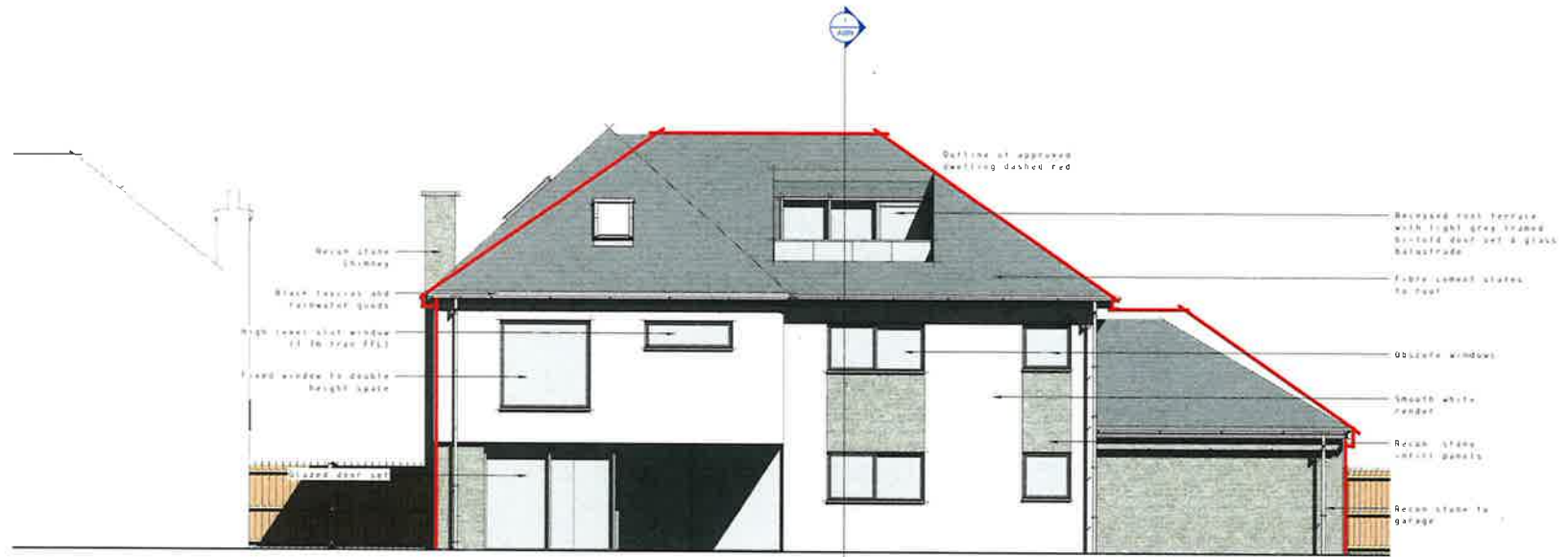
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REV	DATE	DESCRIPTION
A	16.09.2019	Outline of approved plot updated
B	06.11.2019	Parting boundary updated
C	25.11.2019	Updated following comments from planning

PROJECT	DRAWING	SCALE	DATE	STATUS	REVISION
Draclore Road   Old St Melons	Proposed ground floor layout	1:200 @ A1	23.07.2019	PLANNING	0



Proposed front elevation  
1:50

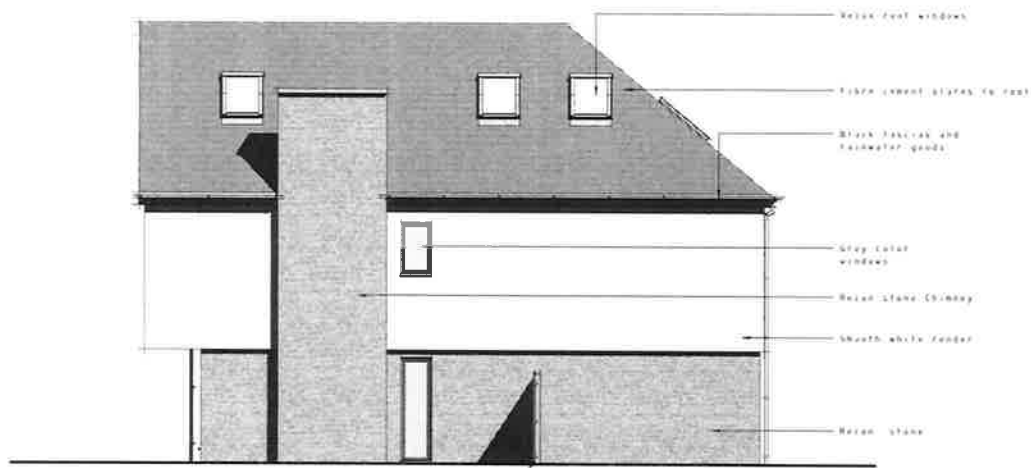


Proposed rear elevation  
1:50

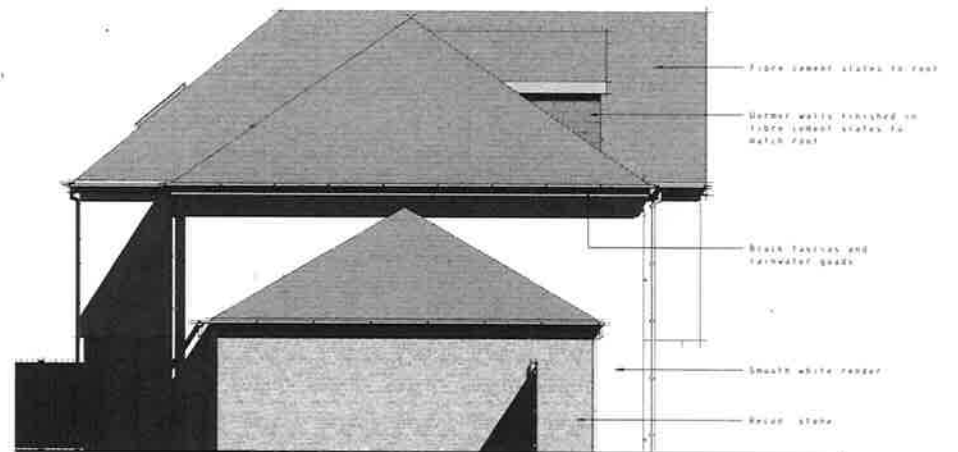
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REV	DATE	DESCRIPTION
A	26/09/2019	Outline of approved plot updated

PROJECT	DRAWING	SCALE	DATE	STATUS	REVISION
Drumline Road   Old St Mellons	Physical Planning	1:50 (g.4)	23.02.2019	PLANNING	A



Side elevation 1  
1:50

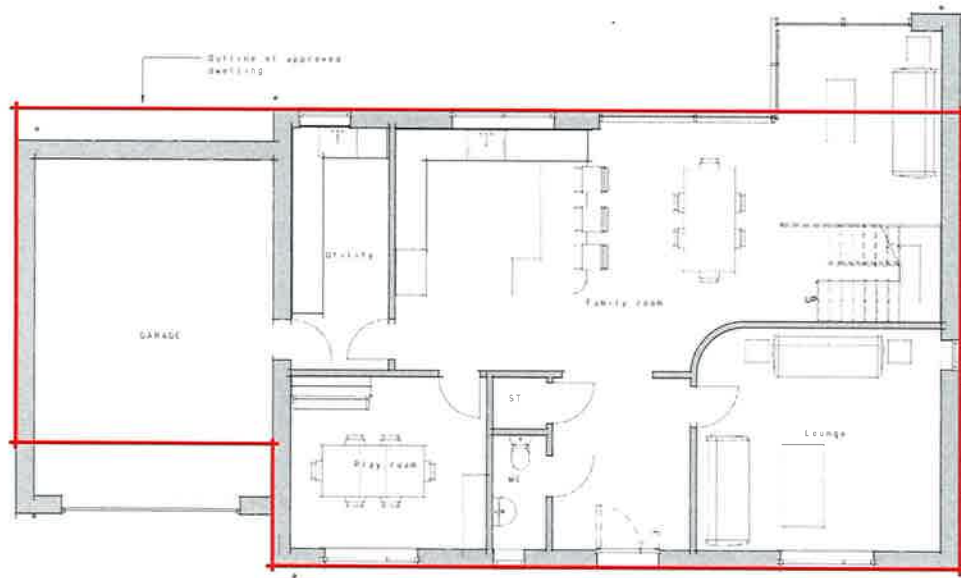


Side elevation 2  
1:50

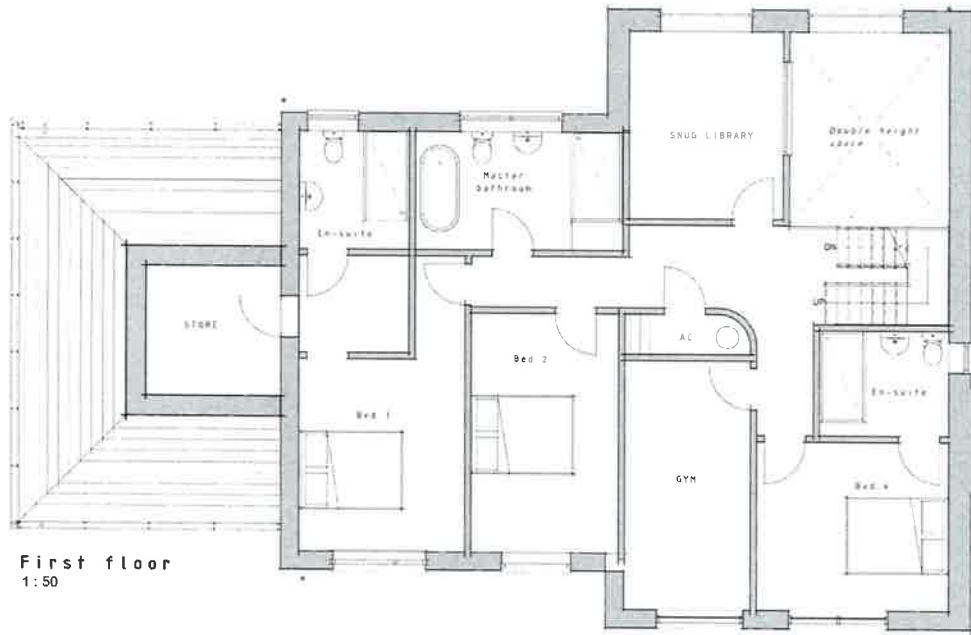
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REV	DATE	DESCRIPTION
A	25.10.2019	Outline of approved site location
B	25.11.2019	Updated following comments from planning

PROJECT	DRAWING	SCALE	DATE	STATUS	REVISION
Durdstone Road   Old St Mellons	Proposed elevation 2	1:50 @ A1	23.07.2019	PLANNING	8



**Ground floor**  
1:50

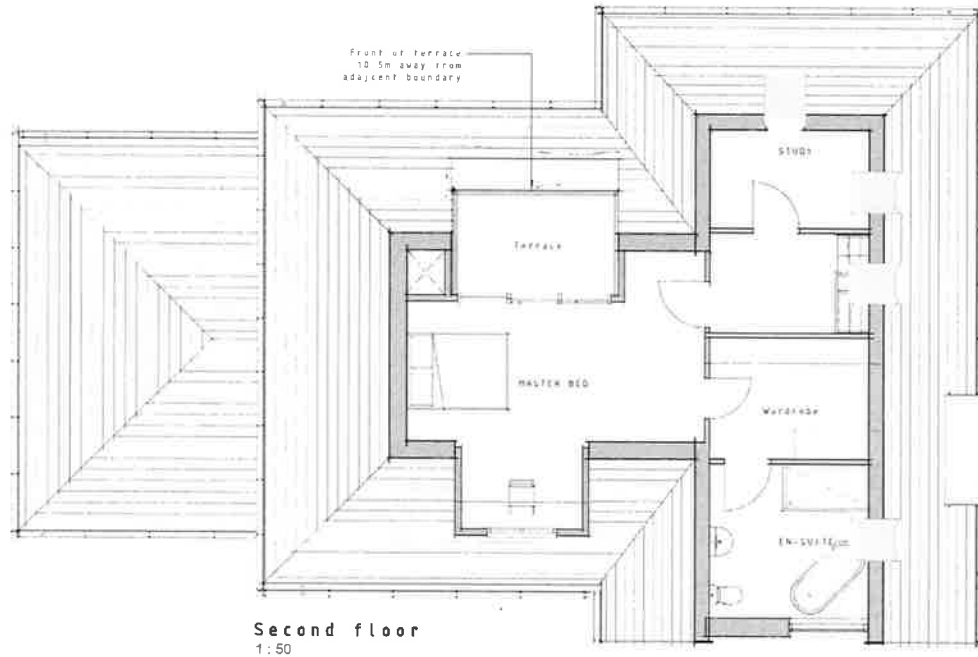


**First floor**  
1:50

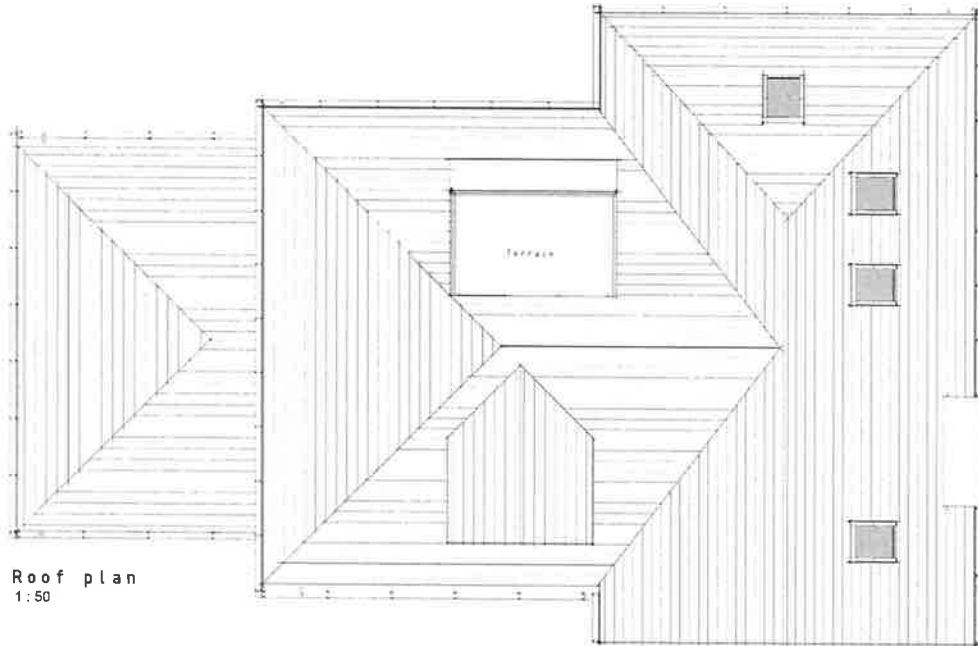
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REV	DATE	DESCRIPTION
1	20.02.2019	Outline of approved plan updates

PROJECT	DRAWING	SCALE	DATE	STATUS	REVISION
Drucklone Road   36 St Mellons	Floor plans sheet 1	1:50 @ A1	20.07.2019	PLANNING	



**Second floor**  
1:50



**Roof plan**  
1:50

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REV	DATE	DESCRIPTION
A	10.09.2019	Outline of approval plan updated
B	08.11.2019	Roof terrace adjusted

PROJECT	DRAWING	SCALE	DATE	STATUS	REVISION
Druidstone Road   Old St Mellons	Floor plans sheet 2	1:50 @ A1	23.07.2019	PLANNING	8



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**LOCAL MEMBER OBJECTION**

COMMITTEE DATE 18/12/2019

APPLICATION No. **19/02245/MNR** DATE RECEIVED: 13/8/2018

ED: **CANTON**

APP: TYPE: Full Planning Permission

APPLICANT: Neo Property Projects

LOCATION: 67 Beda Road, Canton, Cardiff CF5 1LX

PROPOSAL: CHANGE OF USE FROM RESIDENTIAL (CLASS C3) TO 6 BED HOUSE IN MULTIPLE OCCUPATION (HMO)(CLASS C4) TOGETHER WITH A DORMER ROOF EXTENSION

**RECOMMENDATION** 1: That planning permission be **GRANTED** subject to the following conditions:

- 1 C01 – Statutory Time limit
- 2 The development shall be carried out in accordance with the following approved plans:
  - 208-102
  - 208-103
  - 208-104
  - Cycle parking plan

Reason: To ensure satisfactory completion of the development and for the avoidance of doubt in line with the aims of Planning Policy Wales to promote an efficient planning system.

- 3 Prior to the beneficial use of the property as a C4 HMO 6 undercover and secured cycle parking spaces, as indicated on the approved site layout plan, shall be provided within the curtilage of the property and shall thereafter be retained and maintained for as long as the use hereby approved remains in existence.  
Reason: To ensure that secure cycle parking facilities are provided to encourage other modes of transport over the private car.

- 4 Prior to the beneficial use of the property as a C4 HMO a refuse storage area, as indicated on the approved site layout plan, shall be provided within the curtilage of the property. The refuse storage area shall thereafter be

retained and maintained at all times for as long as the use hereby approved remains in existence.

Reason: To secure an orderly form of development and to protect the amenities of the area in accordance with Policy W2 of the Cardiff Local Development Plan 2006-2026.

**RECOMMENDATION 2:** The applicant be advised that the property may now be licensable under Part 2 of the Housing Act 2004 and in this respect they should contact Shared Regulatory Services on 0300 123 6696 to confirm if a license is required.

**RECOMMENDATION 3:** As the site is located within a flood risk area future occupants/owners are advised to sign up to the Environment Agency's Flood Warning service. Additional guidance can be found on the following website: <https://naturalresources.wales/flooding/flood-warning-service/?lang=en>

**RECOMMENDATION 4:** The owner be advised that they are responsible to ensure the full bin provision is present at the property. If more bins are required in order to meet the full bin provision, the owner should contact Connect 2 Cardiff on 029 2087 2087.

## 1. **DESCRIPTION OF PROPOSED DEVELOPMENT**

- 1.1 This application seeks planning permission to erect a dormer roof extension to facilitate the change of use of the property to a C4 HMO. The submitted drawings/details provide information regarding the principal matters for consideration as set out in the relevant Supplementary Planning Guidance.
- 1.2 Internally the property accommodates two bedrooms and a lounge/kitchen on the ground floor; two en-suite bedrooms, a bedroom and a bathroom on the first floor; and one en-suite bedroom in the second floor roofspace.
- 1.3 Externally an amenity space of approximately 37 square metres is provided.

## 2. **DESCRIPTION OF SITE**

- 2.1 The site comprises a two storey property located within a terrace of two storey properties within the Canton Ward of Cardiff. The lawful use of the property is a C3 residential dwelling.

## 3. **SITE HISTORY**

None

#### 4. **POLICY FRAMEWORK**

4.1 The site lies within a residential area as defined by the proposals map of the Cardiff Local Development Plan 2016.

#### 4.2 Relevant National Planning Guidance:

Planning Policy Wales (Edition 10, 2018)  
Planning Policy Wales TAN 12: Design  
Planning Policy Wales TAN 21: Waste

#### 4.3 Relevant Cardiff Local Development Plan Policies:

Policy KP5 : Good Quality and Sustainable Design  
Policy KP8 : Sustainable Transport  
Policy KP13 : Responding to Evidenced Social Needs  
Policy H5 : Sub-Division or Conversion of Residential Properties  
Policy T5 : Managing Transport Impacts  
Policy W2 : Provision for Waste Management Facilities in Development

#### 4.5 Relevant Supplementary Planning Guidance:

Managing Transport Impacts (Incorporating Parking Standards) (2018)  
Houses in Multiple Occupation (HMO's) (2016)  
Waste Collection and Storage Facilities (2016)

#### 5. **INTERNAL CONSULTEE RESPONSES**

5.1 Waste Management have been consulted and have advised that the current waste storage arrangements within the frontage is acceptable and need to be retained for future use. As there will be at least 6 occupants, this property will be entitled to:

General waste:	1 x 240 litre bin
Food waste:	2 x 25 litre food caddies
Dry recyclables:	Green bags equivalent to 240 litres
Garden Waste	1 x 240 litre bin

They have also advised that the owner is responsible to ensure the full bin provision is present at the property and that if more bins need to be provided in order to meet the full bin provision, they should contact Connect 2 Cardiff on 02920872087.

5.2 Traffic and Transportation have been consulted and have advised that the Council's Managing Transport Impacts (including Parking Standards) SPG applies a maximum vehicle parking standard. Therefore the provision of no off-street parking in this location is acceptable.

The SPG requires the provision of secure and covered cycle parking spaces for each bedroom within the HMO with a minimum requirement to provide at least 1 secure covered cycle parking space per bedroom. In this instance as the proposal is for a 6 bedroom HMO then a minimum of 6 secure covered cycle spaces should be provided.

Details of cycle parking facilities to cater for 6 cycles have been submitted and these are considered satisfactory.

## 6. **EXTERNAL CONSULTEE RESPONSES**

- 6.1 South Wales Police South Wales Police have advised that as there are no HMO's within 50m of the application site they have no objection to the application.
- 6.2 Natural Resources Wales have advised that they have no objection to the proposed development as submitted and provide the following advice.

The planning application proposes change of use from residential (Class C3) to a 6-bed house in multiple occupation (HMO) (highly vulnerable development) in Zone C1 of the Development Advice Map (DAM) contained in TAN15. Our Flood Map information, which is updated on a quarterly basis, confirms the site to be at risk from the 0.1% (1 in 1000 year) annual probability fluvial flood outlines of the River Taff, which is a designated main river.

Section 6 of TAN15 requires the Local Planning Authority to determine whether the development at this location is justified. Therefore, we refer you to the tests set out in section 6.2 of TAN15. If you consider the proposal meets the tests set out in criteria (i) to (iii), then the final test (iv) is for the applicant to demonstrate through the submission of an FCA that the potential consequences of flooding can be managed to an acceptable level. We consider the risk of tidal flooding to the proposed development is negligible, as the site benefits from the presence of the Cardiff Bay Barrage. This operates in a flood risk capacity, providing significant protection to Cardiff from tidal flood risk. Therefore, we have no further comment regarding tidal flood risk in this instance.

With respect to fluvial flood risk we have reviewed the following information submitted in support of this application:

- Flood Consequences Assessment, Residential Development at 67 Beda Road, Canton, Cardiff, CF5 1LX, prepared by W M Clarke LTD Consulting Engineers. The FCA states that the ground floor level of the existing property has recently been established at 8.571m AOD and that the approximate road level outside the property is 8.4m AOD.

We confirm that the site is predicted to be flood free in the 1 in 100 year plus 25% climate change allowance and 1 in 1000-year fluvial flood event from the River Taff. In consideration of the above, we are satisfied that the FCA shows that the risks and

consequences of flooding are manageable to an acceptable level. Therefore, we have no objection to the proposals.

## 7. REPRESENTATIONS

7.1 Neighbours have been consulted and objections have been received from the occupiers of 6, 70, 74 and 76 Beda Road; 114 Lansdowne Road and 1 Thurston Street. The owner of 72 Beda Road has objected and also claims her objection is on behalf of 4 other occupiers of Beda Road. The objections can be summarised as follows:

- Car parking issues;
- How will the rear access be controlled as the owner does not have access to the rear lane;
- Contrary to the Council's SPG on HMO's;
- Overcrowding of the property and whether the rooms will meet current size standards;
- The intentions of the owner that the development is being undertaken for profit with no care for the community;
- Waste storage facilities;
- Cycle storage facilities

7.2 An objection has also been submitted from the Ward Councillors Cunnah, Elsemore and Patel. Their objection is as follows:

*"We believe this proposal needs to be revisited to take into account our concerns as set out below. If the officers of the local planning authority are minded to recommend this application to be approved, we would like the application to be put in front of a full meeting of the Planning Committee, and for the members of the Committee to undertake a site visit so they can fully understand the context and the concerns we have raised.*

*Our objection is specifically based on our view that the application fails to adhere to:*

- *Policy H5 of the Cardiff Local Development Plan 2006-2026 (hereafter **Policy H5 of the LDP**);*
- *Cardiff's Houses in Multiple Occupation (HMO) supplementary planning guidance, October 2016 (**the SPG**), and;*
- *Cardiff's HMO Licensing Fire Safety and Amenity Standards (**the HMO standards**).*

### **The character of the area**

*Our first point is a general point, and is in relation to point iii) of Policy H5 of the LDP, which states that "The cumulative impact of such conversions will not adversely affect the amenity and/or character of the area."*

*We note that in Appendix E of the SPG, relating to Housing Tenure, it is demonstrated that Canton has consistently, since the sixties, had the highest proportion of occupier- owned housing compared to other inner city wards (67% in 2011). It also has the highest proportion of children amongst inner city wards, which is an indicator that homes are predominantly used here by single family units.*

*Therefore it stands to reason that HMO applications in Canton – included the application under consideration – represent a more significant, and in our view damaging, change to the long-term demographic pattern here compared to other wards.*

### **Car parking**

*Point iv) of Policy H5 of the LDP states that HMO applications should “not have an adverse effect on local parking provision.” This point is expanded on in the SPG, including at 6.4 where the guidance is that “car parking provision should be effectively incorporated into the design of the development” and “the merits and circumstances of each planning application need to be taken into account when applying parking standards; including [...] any factors relating to location and context”.*

*We note at this location there is a history of problem on-street parking, for a variety of reasons, including:*

- The application site is next to a popular pub which does not have off-street parking for its customers, who therefore park on Beda Road.*
- It is next to the junction with the busy thoroughfare Lansdowne Road, and there is no parking at all for existing residents on long stretches of Lansdowne Road, some of whom therefore park on Beda Road.*
- In common with other nearby streets in this part of Canton, Beda Road attracts a large increase in on-street parking when events are held at the nearby Cardiff City Stadium, an acute pressure which is well known to the Council and its enforcement officers.*

*We believe this context is significant and should be considered in relation to this application, which proposes accommodation for six separate individuals, who along with any visitors can be expected to require parking and to generate many vehicle trips.*

### **Cycling**

*Section 6.5.3 of the SPG states that:*

*“Outside cycle storage should be secure, sheltered and adequately lit, with **easy access to the street** [our emphasis]. Cycle parking and storage provision should be considered into the design of an HMO from the outset and shown in any plans”*

*In the plans submitted, cycle storage is proposed at the rear of the property. We note that there is no rear access to this property. Therefore, in order to use a cycle which is stored there, each resident will need to move the cycle through the entire ground floor of the building, including through the kitchen which we would expect should have a heavy, auto-closing fire door.*

*Clearly, this is not providing easy access to the street. In our view, if residents are regular cyclists and the proposed storage solution does not discourage them, it would be far too impractical to transport a bicycle through the house twice a day. We would anticipate that the bikes – up to six – would be kept in the hall. In our view up to six individual bikes stored in the hall would then in turn cause a practical storage problem and a fire hazard.*

### **Waste**

*We note and agree with the SPG's assertion that areas with HMOs record substantially greater numbers of waste concerns.*

*Section 6.5.3 of the SPG states that "Planning applications that cannot demonstrate suitable, ideally covered, storage space, for recycling and waste will be refused."*

*We are concerned that the application has identified for waste storage the relatively small forecourt at the front of the property, which we doubt will be adequate or appropriate for six individuals. There is not enough space to sort waste when discarded, or manoeuvre specific collected waste from back to front for example, ready to present for collection on the correct dates. The waste storage space will not be covered either, which we anticipate could cause visual problems on the street.*

### **Overlooking**

*Section 6.3.3 of the SPG states that:*

*"All residents within an HMO should have access to amenity space without impacting upon other residents within the HMO, i.e. access should be through communal space, not a private bedroom. The **amenity space should also not overlook occupied bedrooms.**"*

*Contrary to the guidance, we note that the only exterior window from the rear ground floor bedroom is overlooked from the garden. We believe an inevitable result of this is that the resident in that bedroom will suffer an invasion of privacy, or alternatively will have a perpetually drawn curtain and not have access to natural light.*

## **Fire risk**

*The HMO Standards state that a 3-4 storey HMO must have, among other things:  
a) 30 minute protected route with FD30 Fire Doors (no smoke seals)  
b) 30 mins separation to walls/ceilings between units of accommodation.*

*We can see no evidence supporting these measures in the application. In the event of a fire, we are particular concerned that a resident in the third storey converted attic would be left with a single route to exit the building down two flights of stairs.”*

## **8. ANALYSIS**

- 8.1 This application seeks planning permission to change the use of 67 Beda Road from a C3 residential dwelling to a C4 House in Multiple Occupation's (HMO). In order to facilitate the change of use a dormer roof extension is proposed to the rear of the property.

As Use Class C4 allows for tenanted living accommodation occupied by up to six people, who are not related and who share one or more basic amenities as their only or main residence, the main issue for this application is the impact the change of use to a six person HMO will have on the character of the area, the community and the living conditions of future occupiers of the property together with the impact of the dormer roof extension.

- 8.2 **Policy Considerations** - In respect of the conversion of the property to a C4 HMO for up to 6 occupants Policy H5 of the adopted LDP is considered relevant. Further guidance can also be found in the adopted HMO SPG

- 8.3 Policy H5 of the LDP is considered to be a prescriptive policy whereby as long as the relevant criteria is met there is unlikely to be any objection to such proposals. It advises that:

*“Proposals for any conversion to flats or Houses in Multiple Occupation will be permitted where:*

*i. The property is of a size whereby the layout, room sizes, range of facilities and external amenity space of the resulting property would ensure an adequate standard of residential amenity for future occupiers.*

*ii. There would be no material harm to the amenity of existing, nearby residents by virtue of general disturbance, noise or overlooking.*

*iii. The cumulative impact of such conversions will not adversely affect the amenity and/or the character of the area.*

*iv. Does not have an adverse effect on local parking provision.”*

- 8.4 The approved Supplementary Planning Guidance on HMO's further expands on this Policy and aims to provide background information on, and provide a rationale for how the Council will assess applications for planning permission to create new



C4 and *Sui Generis* HMOs. It aims to identify the threshold at which it is deemed that the concentration of HMOs in an area has reached a level considered to adversely impact upon the community. It is recognised that HMOs can provide an important source of housing and it is recognised that demographic change has driven many of the changes that have seen traditional family homes become HMOs. HMOs are a popular accommodation source for many groups, including students, young professionals, migrant workers and often people on lower incomes.

However, concentrations of HMOs clustered in small geographical areas can detract from the character of the area and actively contribute towards a number of perceived problems, including, but not limited to:

- Increased population density, leading to greater demand for infrastructure, such as waste collections and on-street parking.
- Higher proportion of transient residents, potentially leading to less community cohesion, undermining existing community facilities
- Areas of higher HMO concentrations becoming less popular with local residents, with many properties taken out of the owner-occupier market.
- A proliferation of properties vacant at certain points of the year
- Subsequent impact on crime, local centre viability, as a result of the number of properties temporarily vacant for long periods.

It is considered that this may conflict with policy KP13 of the LDP which aims to improve the quality of life for all.

Having identified some of the issues caused by HMOs the Council considered it was necessary to determine a threshold at which new HMOs may cause harm to a local area. This threshold will resist further HMOs in communities that already have a concentration above this limit, while also controlling the growth of HMOs in communities below this threshold. A two-tier threshold will therefore be applied to determine when an area has reached the point at which further HMOs would cause harm. In Cathays and Plasnewydd the figure of 20% is to be applied and in all other wards, the figure of 10% is to be applied.

This means that within Cathays or Plasnewydd, if more than 20% of the dwellings within a 50m radius of the proposed HMO are already established HMOs (i.e. either C4 or *sui generis* in Planning terms) then this development would be considered unacceptable. In other wards the figure would be 10%.

Having regard to the “cumulative impact” of such conversions in respect of this application, an analysis has been made on the extent of HMO’s (including those defined as such under Sections 254 to 259 of the Housing Act 2004 and those covered under the Additional Licensing Scheme which operates within the Cathays and Plasnewydd Wards of Cardiff) against the threshold limits identified above. As the application site is located within the Canton Ward of Cardiff a 10% threshold

limit will be relevant and having undertaken such checks within 50m of the application site it was found that there were no registered HMO's within 50m of the application site which equates to 0%. This is below the 10% limit which would trigger the active consideration of negative cumulative impact consequences.

- 8.5 **Room Sizes** – The Cardiff HMO Licensing Fire & Safety Standards (updated in 2014) sets standards in terms of amenity, space standards and facilities which must be adhered to in order to obtain a License from the Council. From a planning perspective, paragraph 6.1.1 of the adopted HMO SPG identifies that this would be the minimum that would be expected to be achieved for all applications for both C4 HMO's and larger sui generis HMO's. Having had regard to this criteria the submitted plans indicate that these standards would be met.
- 8.6 **Waste** – Policy W2 of the Cardiff Local Development Plan seeks to ensure that adequate provision is made for waste management facilities within new developments, in order to aid the Council in meeting the challenging waste recycling targets set by European and National targets. Facilities provided should be secure, unobtrusive and easily assessable.

The adopted Waste Collection and Storage Facilities SPG supplements policies adopted in the LDP relating to the provision of waste management facilities in new development. Paragraph 4.12 of the approved SPG on Waste Collection and Storage Facilities advises that for HMO's the recommended bin allocation for between 6 & 8 residents is as follows :-

*1 x 240L bin for general waste*  
*1 x 240L bin for garden waste (if required)*  
*2 x 25L bins for food waste*  
*Green bags for recycling.*

This application seeks the change of use of the property to a C4 HMO (6 occupants). Waste Management has been consulted and has advised that the proposed area for the storage of waste and recycling in the front garden identified on the submitted plan has been noted and is acceptable, refuse storage must thereafter be retained for future use. Condition 4 has been imposed to ensure the bin storage area is retained.

- 8.7 **Transportation** - Policy KP8 of the LDP seeks to reduce travel demand and dependence on the car. It identifies that to accommodate the planned growth levels predicted for the city, existing and future residents will need to be far less reliant on the private car and seeks to ensure that more everyday journeys are undertaken by sustainable modes of transport. Policy T5 of the LDP also identifies that all new development for which planning permission is required will contribute to reducing reliance on the private car, in line with national planning policies and the strategic transport objectives of the LDP. The Council's approved Supplementary Planning Guidance on Managing Transportation Impacts

(Incorporating Parking Standards) supplements the LDP in this respect and sets out the Councils approach to assessing and managing the transport impacts of developments within the City. Section 6 of the SPG refers to parking standards and is therefore an important tool to be used in managing demand for travel by car and encouraging a shift to sustainable transport modes.

In respect of car parking the SPG identifies that a C4 HMO does not need to provide any off street car parking spaces.

With respect to cycle parking the SPG identifies that a C4 HMO will be required to provide 1 undercover and secure cycle parking space per bedroom. As this application seeks the change of use to a 6 bedroom HMO then 6 undercover and secure cycle parking spaces will need to be provided. The applicant has submitted plans which indicate that 6 undercover and secure cycle parking spaces can be provided within the rear garden which is considered acceptable. Condition 3 has been imposed to ensure such facilities are provided and retained.

- 8.8 **Amenity Space** – Criterion i) of Policy H5 of the LDP advises that planning permission will be granted where *“The property is of a size whereby the ...external amenity space of the resulting property would ensure an adequate standard of residential amenity for future occupiers.”* This is further reinforced by the HMO SPG which advises that amenity space is important in retaining a quality of life for people living within the dwelling. Paragraph 6.3.2 of the SPG states *“The City of Cardiff Council has typically used the figure of 25m<sup>2</sup> as the minimum expected external useable amenity space for C3 dwellings, i.e. for those dwellings up to 6 persons. This level should also apply to C4 properties. Each additional person would be expected to have 2.5m<sup>2</sup>. As such, for example, the minimum expected for a 7 bed HMO would be 27.5m<sup>2</sup> of external amenity space. Each additional person should result in a corresponding increase of 2.5m<sup>2</sup>. Useable amenity space is considered to be at least 1.4m wide, enabling storage and access.”*

In respect of amenity space as the application seeks permission for 6 occupants to reside at each property then 25 square metres will be required for the property. Having undertaken an assessment of the property an amenity space of approximately 37 square metres will be available for occupiers to use. However when omitting the space required for waste and cycle storage facilities (8 square metres) this will be reduced to 29 square metres. As the minimum amenity space requirement as specified in the HMO SPG is 25 square metres the proposal is therefore considered acceptable when considered against the HMO SPG.

- 8.9 **Rear dormer roof extension** – The dormer is to be set up the roof slope and finished in hanging slate to match the existing roof covering in line with advice contained within the Residential Extensions and Alterations SPG. It is not considered that the dormer is overbearing or un-neighbourly which would justify concern for the Local Planning Authority. It should also be noted that a dormer of this size could presently be built using existing Permitted Development rights i.e.

Welsh Government consider a dormer of this size does not require the benefit of planning permission. The only reason the dormer was included in this planning application was due to bedroom 6 of the proposed HMO being shown in roofspace that does not exist without the erection of the dormer. If planning permission was refused the dormer could still be built using existing Permitted Development rights.

- 8.10 **Flood Issues** – It is noted that the property is situated within a C1 flood zone. The applicant has commissioned a Flood Consequences Assessment which has identified that the risks and consequences of flooding are manageable to an acceptable level. Natural Resources Wales were consulted on the FCA and confirmed that they have no objection to the proposals.

Due to the site's location in the flood zone Recommendation 3 has also been attached to ensure future occupants are made aware of the issue.

- 8.11 In respect of the objections received following neighbour consultations the following should be noted:

- The proposal is fully compliant with the Council's approved Managing Transport impacts (Incorporating Parking Standards) Supplementary Planning Guidance 2018 in respect of car parking and cycle parking facilities;
- The rear access to the property is not the primary access, this is from Beda Road. If it is found that the owner does not have access to the rear of the property from the rear lane this would be a civil matter. Access to the rear garden can still be provided through the main property;
- The proposal is fully in compliance with the Council's approved Supplementary Planning Guidance on HMO's;
- Use class C4 allows for up to 6 residents to reside at the property. If it is found that the property is being occupied by more than 6 residents then this would require the benefit of planning permission as 7 or more occupants would fall within a different Use Class;
- The intentions of the owner to use the development for monetary purposes is not a planning matter;
- Waste storage facilities have been checked by Waste Management and they are satisfied with the arrangements proposed;

- 8.12 In respect of the concerns raised by the local Ward Members the following should be noted:

- **The character of the area** - The proposal is fully in compliance with the Council's approved Supplementary Planning Guidance on HMO's – see paragraph 8.4 of this report. The SPG identified some of the issues caused by HMOs and set a threshold which, if exceeded, may cause harm to a local area. This threshold will resist further HMOs in communities that already have a concentration above this limit, while also controlling the growth of HMOs in communities below this threshold. In respect of this application it

was identified that there were no registered HMO's within 50m of the application site and therefore the proposal does not go against advice contained within the HMO SPG.

- **Car parking** – The issue of car parking is covered in paragraph 8.7 of this report. The Managing Transport impacts (Incorporating Parking Standards) Supplementary Planning Guidance 2018 identifies that no car parking spaces are required for this development.
- **Cycling** - The issue of cycle parking is covered in paragraph 8.7 of this report. The Managing Transport impacts (Incorporating Parking Standards) Supplementary Planning Guidance 2018 identifies that 1 cycle parking space will need to be provided per bedroom. 6 spaces are proposed and this is considered acceptable. It is noted that the property is a terrace and as such the cycles will have to be transported through the property, however this is not uncommon for terraced property. The Council has also previously resisted an application on this basis. When the Council's refusal to grant planning permission was challenged on appeal the Planning Inspector who determined the appeal concluded "I do not consider this to be unreasonable, and I conclude that transporting bicycles through the property would not cause unacceptable harm to the living conditions of future occupiers".
- **Waste** – The proposal is fully in compliance with the Council's approved Supplementary Planning Guidance on Waste Collection and Storage Facilities 2016 as identified in paragraph 8.6 of this report. Waste management have also advised that the receptacles required will be no different to other residential properties within the street.
- **Overlooking** – In respect of concerns that occupiers of the ground floor rear bedroom would be overlooked from the amenity space, the main useable amenity space is situated to the rear of the annex and it is not envisaged that such an arrangement would give rise to privacy issues which would justify concern for the Council.
- **Fire risk** – Issues over fire risk are not a matter for Planning but would be a Building Regulation matter.

## 9. **RECOMMENDATION**

- 9.1 The Council is mindful of the current climate with respect to the amount of HMO's within the City and that there are concerns that a proliferation of such uses can undermine the character of an area to the detriment of local residents. In respect of this application the proposal complies with both local and national planning policy and the SPG on HMO's.

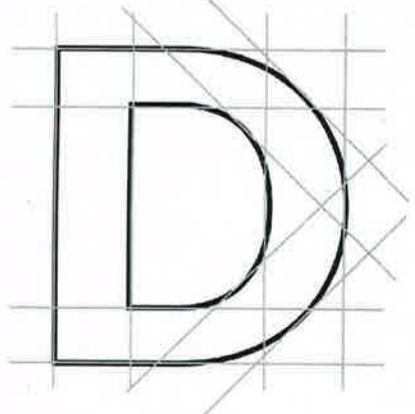
Having taken all of the relevant factors into consideration there are no grounds to

justify a refusal of this application and it is therefore recommended that planning permission be granted, subject to conditions.



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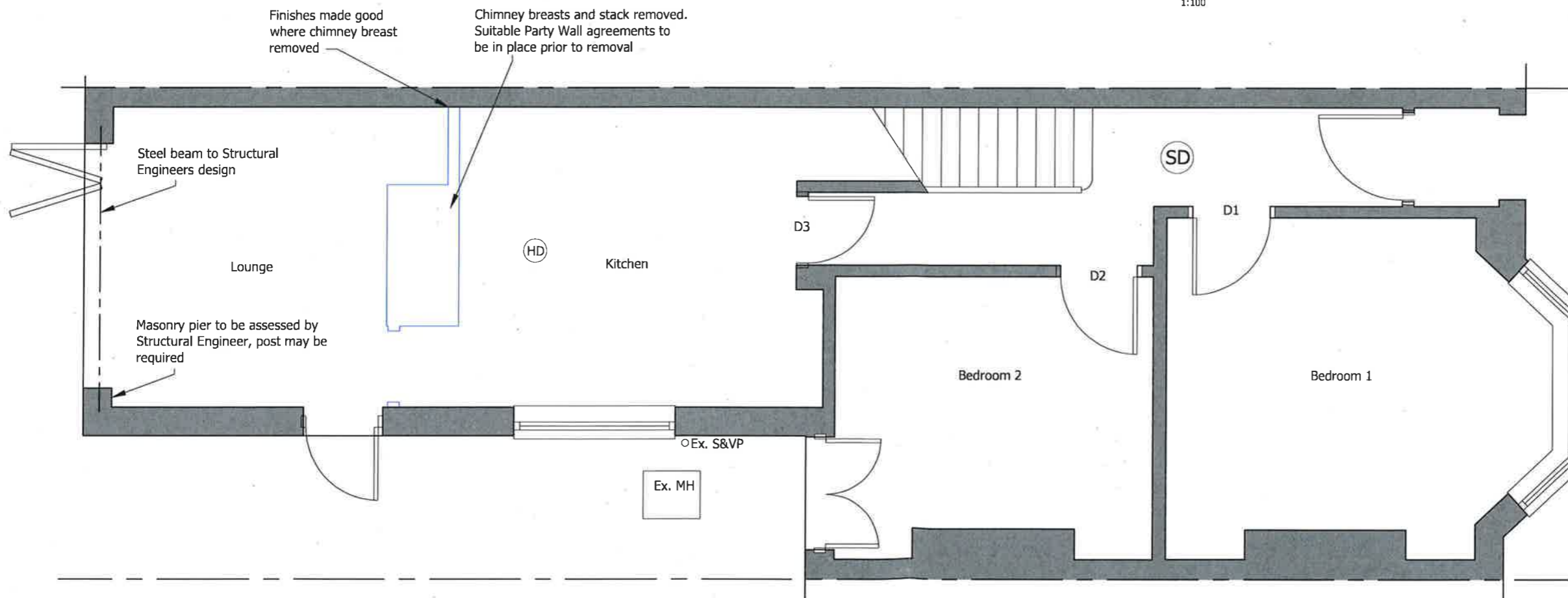
REV	REVISION	REV DATE

PROPOSED LOFT CONVERSION  
67 BEDA ROAD - SITE PLAN

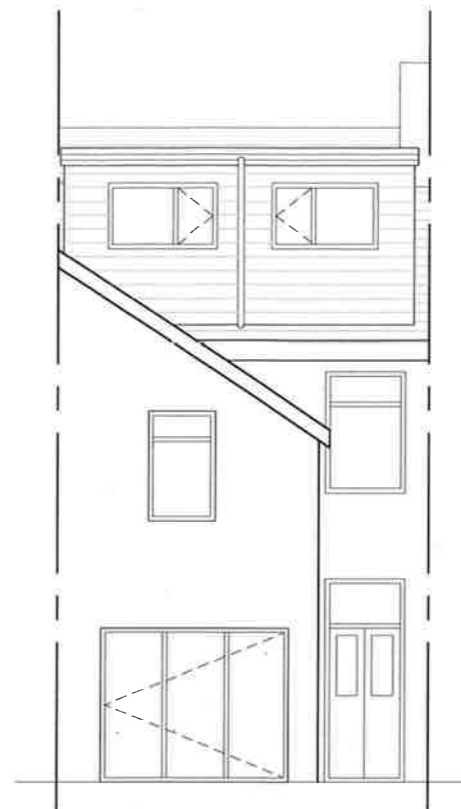
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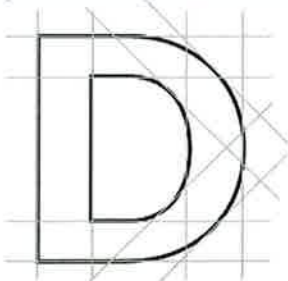


Proposed Ground Floor Plan  
1:50



Proposed Elevation  
1:100

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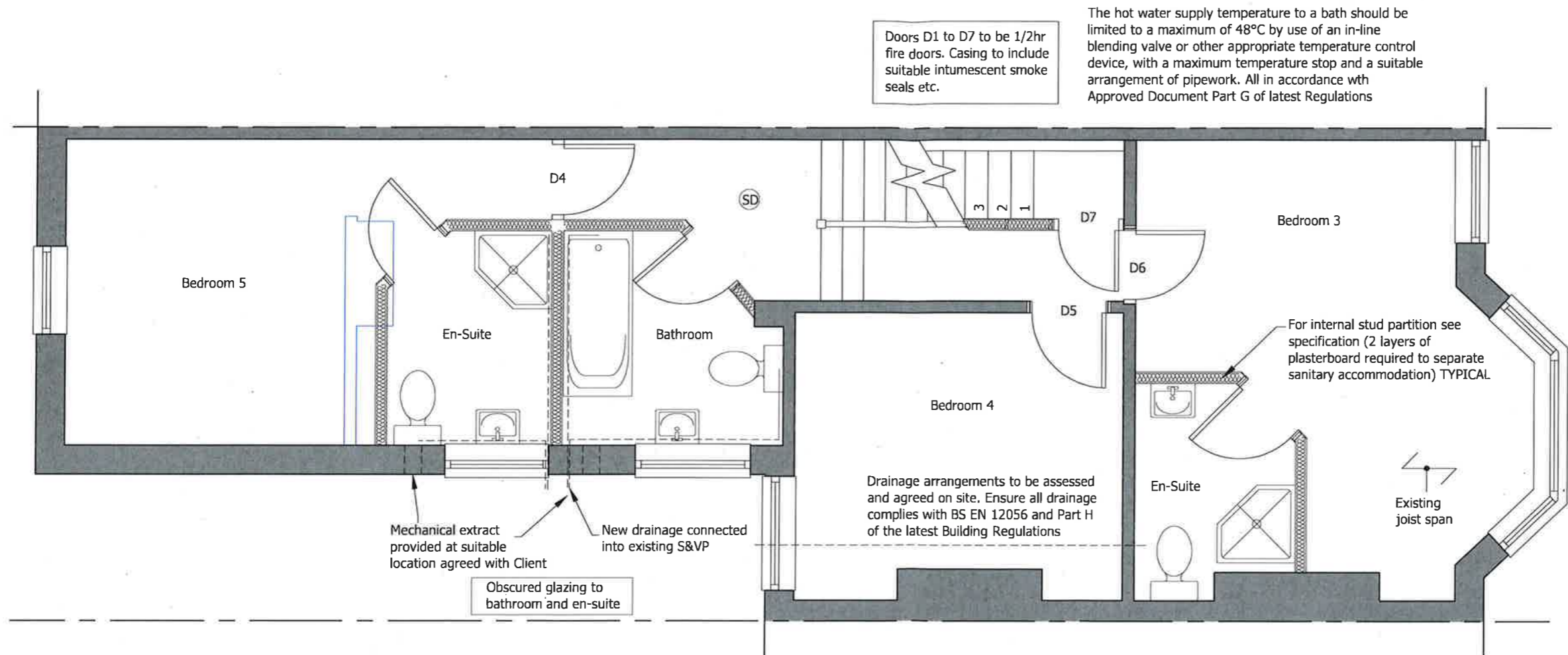


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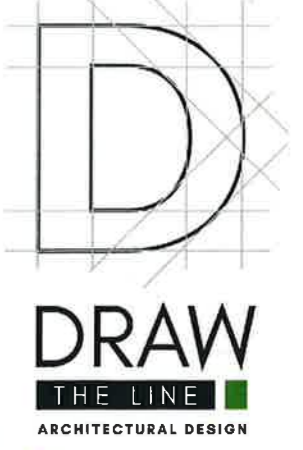


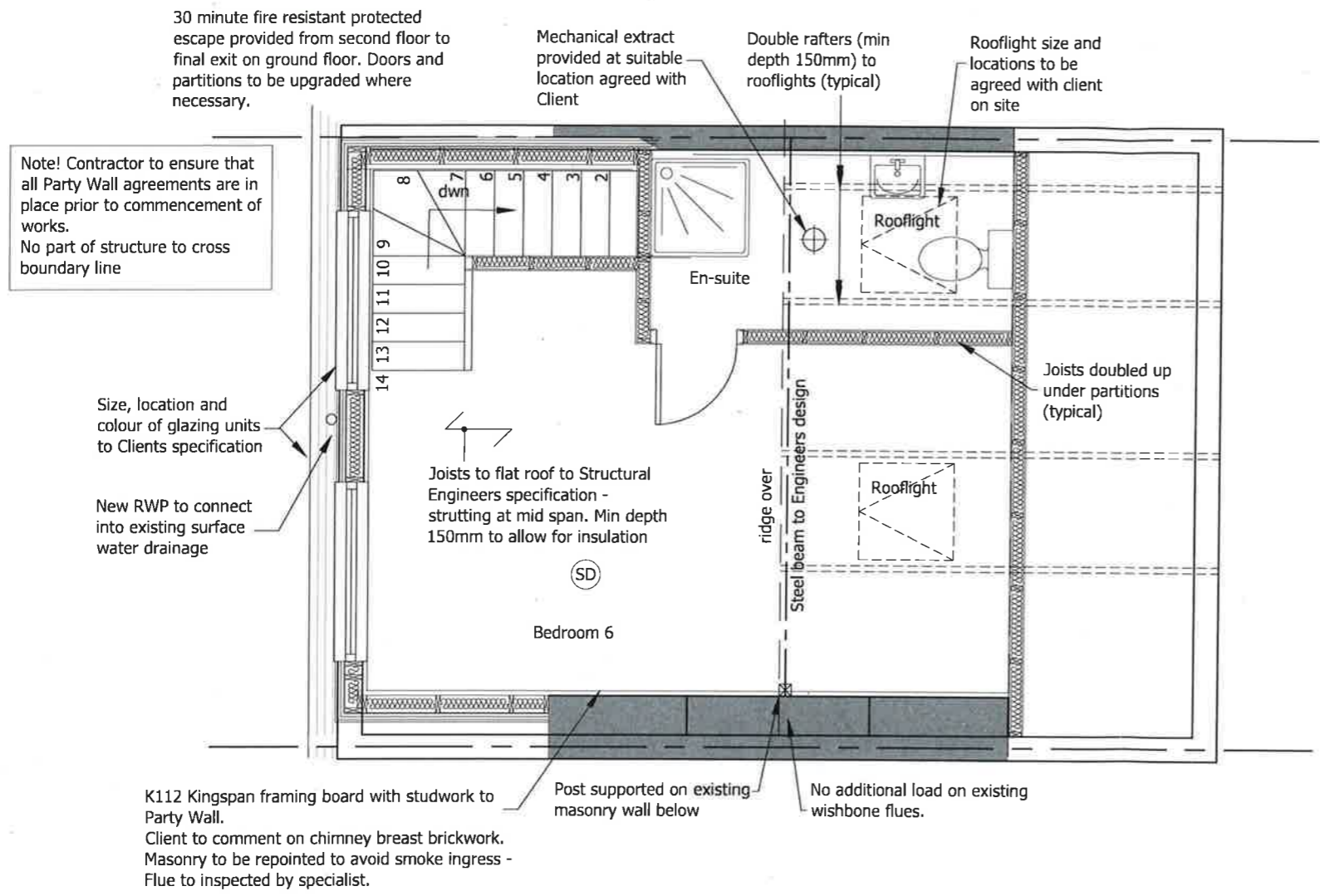


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Proposed First Floor Plan  
1:50

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Proposed Second Floor Plan  
1:50

Smoke alarm system to be introduced

Mains operated self contained smoke alarm system (SC) to be installed to BS 5446:Pt 1 in circulation areas in ground & first floor/storey, and positioned near to kitchen and living room (within 7m) and near to bedroom doors (within 3m). SC smoke alarms to be interconnected to enable smoke detection by 1 unit to operate alarms on all of them. SC smoke alarms to be permanently wired to separate fused circuit via the distribution board, and wiring to conform to wiring regulations. If SC smoke alarms fixed to ceiling - to be at least 300mm away from any wall/ceiling light fittings, and if fixed to walls to be between 150-300mm below ceiling. SC smoke alarms may be wholly mains operated, or mains operated with a secondary supply such as batteries (smoke alarms operated by primary batteries are not acceptable.)  
SD - denotes smoke detector  
HD - denotes heat detector

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## PETITION

COMMITTEE DATE: 18/12/2019

APPLICATION No. **19/02512/MNR** APPLICATION DATE: 13/09/2019

ED: **FAIRWATER**

APP: TYPE: Removal of condition(s)

APPLICANT: MR COFFI (BUNKER STATION) LTD

LOCATION: COFFEE BUNKER, 176 ST FAGANS ROAD, FAIRWATER, CARDIFF, CF5 3EW

PROPOSAL: REMOVAL OF CONDITION 3 OF PLANNING PERMISSION REF. 18/01979/MNR WHICH ALLOWED THE TEMPORARY VARIATION OF CONDITIONS 4 & 20 OF PLANNING PERMISSION REF. 13/02323/MNR, UNTIL 1st NOVEMBER 2019, SUCH THAT THE PREMISES MAY PERMANENTLY OPEN BETWEEN THE HOURS OF 07:00 & 21:30 MONDAYS TO FRIDAYS, 08:00 & 21:30 ON SATURDAYS AND 08:00 TO 18:30 ON SUNDAYS AND SERVE HOT FOOD

**RECOMMENDATION:** That planning permission be **GRANTED**.

1. The development shall be carried out in accordance with the following approved plans:
  - Specification/Data sheet
  - Accompanying letter dated 20th August 2018
  - Email dated 19/10/2018.

Reason: To ensure satisfactory completion of the development and for the avoidance of doubt in line with the aims of Planning Policy Wales to promote an efficient planning system.

2. The premises shall be used only be used as a restaurant, café or coffee shop and for no other purpose (including any other purpose in Class A3 of the Schedule to the Town and Country Planning (Use Classes) Order 1987 or in any provision equivalent to that Class in any statutory instrument amending, revoking or re-enacting that Order).

Reason: The use of the premises for other purposes within Class A3 could detract from the amenities of nearby occupiers.

3. Members of the public shall only be admitted to or allowed to remain on the premises between the hours of 07:00 and 21:00 Monday to Friday, 08:00 and 21.00 on Saturdays and 08:00 and 18:30 on Sundays.

Reason: To ensure that the amenities of other premises in the vicinity are protected, in accordance with policy KP5 of the Local Development Plan.

4. No cooking/heating of food shall take place from the premises other than in the following appliances:

- Buffalo double contact grill
- Flatbed grill
- Bain Marie
- Heavy duty microwave
- High Speed oven
- Commercial toaster

No additional appliances, other than those listed, shall be used for the cooking of food unless otherwise agreed in writing by the LPA.

Reason: The use of particular appliances may, in the absence of an appropriate fume extraction system, have a detrimental impact upon neighbouring occupiers in terms of noise and smell nuisance contrary to Policies KP5 and EN13 of the Cardiff Local Development Plan 2006-2026 and the proposal is only considered acceptable due to the limited extent of hot food to be cooked or heated.

## 1. **DESCRIPTION OF PROPOSED DEVELOPMENT**

1.1 In 2013 the café was originally granted planning permission subject to conditions that sought to control hours of operation and the type of food sold. In 2018 a revised planning permission was granted that allowed for an extension of the hours of operation, and subject to strict control, the sale of hot food. The 2018 permission was granted on a temporary basis which would allow for the further consideration of the matters were there any issues to be experienced.

This application seeks permission to permanently allow for the extended hours of operation and the cooking and sale of hot food from pre-determined cooking appliances.

1.2 For members information the details of the relevant conditions are as follows:

- The original planning permission ref: 13/02323/DCO restricted the opening hours to 08:00-18:30 on any day and also the type of hot food that could be served.

1.3 Condition 4 of the planning permission regarding hours of operation stipulated that;

*‘No member of the public shall be admitted to or allowed to remain in the ground floor café premises between the hours of 18.30 and 08.00 on any day.*

*Reason: To ensure that the amenity of occupiers of other premises in the vicinity are protected in accordance with information received from the agent via email on the 16<sup>th</sup> April 2009; and in accordance with Policy 2.24 of the deposit Cardiff Unitary Development Plan.*

- 1.4 Condition 20 of the planning permission regarding the sale of hot food stipulated that;

*'Notwithstanding the provisions of Class A3 of the Schedule of the Town and Country Planning (Use Classes) Order 1987, or in any provision equivalent to the class in any statutory instrument revoking and re-enacting that order (with or without modification), the ground floor use hereby permitted shall be limited to the sale of drinks, cakes and cold snack foods, re-heating of toasties, paninis and sandwiches or such other foods as shall be agreed in writing by the Local Planning Authority. No other cooking or re-heating of food shall be allowed. Any equipment required shall be installed prior to the permitted use commencing. The equipment shall thereafter be maintained in accordance with the manufacturers guidelines, such guidelines having been agreed by the Local Planning Authority in writing.*

Reason: In order to retain control of the use of the premises.

- 1.5 The application seeks to allow the premises to be open between the hours of 07:00 and 21:00 Monday to Friday, 08:00 and 21:00 on Saturday, 08:00 and 18:30 on Sunday and to serve hot food as prescribed in the temporary granting of those hours and the sale of hot food by planning permission ref:18/01970/MNR.

## 2. **DESCRIPTION OF SITE**

- 2.1 The site comprises a three storey building that incorporates a ground floor A3 café and 2no, two bed flats above. The site is located adjacent to Fairwater Green, on the edge of the Fairwater Green Local Centre. The predominant shopping role of the Local Centre is located to the north-east and south-east of the development site. A series of garages are located to the north of the site with residential dwellings being found beyond. A variety of residential dwellings are also found in a southerly and easterly direction.

## 3. **SITE HISTORY**

- 3.1 09/00335/DCO – Planning permission refused for the erection of a three storey mixed use development comprising A3 café use at ground floor and two residential units above.
- 3.2 Appeal APP/Z6815/A/10/2134523 was considered by the appointed Planning Inspector against the refusal of planning permission 09/00335/DCO and was allowed.
- 3.3 13/02323/DCO - Planning permission granted for a three storey mixed-use development comprising Class A3 café at ground floor and two residential units above, with associated works. The scheme whilst being very similar to that approved by 09/00335/DCO included some design amendments.
- 3.4 16/02842/MNR – Planning permission refused for the construction of a detached A1/A2 unit measuring approximately 54 square metres sited on land

adjacent to 176 St Fagans Road, Fairwater.

- 3.5 18/01979/MNR – Planning permission granted for the variation of condition 4 (to allow hours of opening from 07:00 Monday to Friday and closing 21:30 Monday to Saturday) and condition 20 (to allow the sale of hot food) of 13/02323/DCO.

#### **4. POLICY FRAMEWORK**

##### **National Planning Policy**

- Planning Policy Wales (10<sup>th</sup> Ed) 2016
- Technical Advice Note 12: Design
- Development Management Manual

##### **Cardiff Local Development Plan 2006-2026 (2016)**

- Policy EN13 (Air, Noise, Light Pollution and Land Contamination)
- Policy R5 (Local Centres)
- Policy R8 (Food and Drink uses)
- Policy T5 (Managing Transport Impacts)
- Policy W2 (Provision for Waste Management Facilities in Development)

##### **Supplementary Planning Guidance**

- Managing Transportation Impacts (Incorporating Parking Standards) SPG 2018
- Food, Drink and Leisure Uses SPG 2017
- Waste Collection & Storage Facilities (October 2016)

#### **5. INTERNAL CONSULTEE RESPONSES**

- 5.1 The Operational Manager, Environment (Pollution Control) – no objection
- 5.2 The Operational Manager (Transportation) – no response

#### **6. REPRESENTATIONS**

- 6.1 A petition has been received, which has been signed by 78 people who object to the proposal on the following grounds:

- The impact of the proposal on existing businesses within the area.
- The impact on parking and highway safety which is already a problem within the area.

- 6.2 Objections or representations expressing objections or concerns with respect to the proposal have been received from the following addresses:

- 60 Everswell Road, 178, 181, St Fagans Road, and No's 2, 3, 4, 5, 6 & 7 The Drive.

Full details are available online, however, the comments can be summarised

as follows:

- The impact on existing businesses within the area.
- Traffic congestion and insufficient parking.
- The cooking of hot food on the premises will require new extraction equipment which will impact upon the amenities of neighbouring occupiers.
- The removal of the conditions is just a way of adding value to the property.
- Noise and disturbance caused by customers and deliveries.

## 7. ANALYSIS

### Land Use Policy Considerations

The site is located within the settlement boundary as defined by the LDP Proposals map. Policy R8 identifies that food and drink uses are most appropriately located in the City Centre, the Bay and District and Local Centres subject to amenity considerations, highways matters and crime and fear of crime considerations and where they do not cause unacceptable harm to the shopping character role and character of designated centres. Whilst the premises are located adjacent but outside the Fairwater Green Local Centre the application proposes to extend the opening hours and allow the sale of hot food from an existing A3 commercial premises and no change of use of the unit will take place. In light of this the application raises no land use policy concerns.

### Residential Amenity Considerations.

The premises are located adjacent to the Fairwater Green Local Centre which comprises a mixed residential and commercial setting. There is residential accommodation within the upper floors of the application site and the surrounding buildings, and there are other commercial premises within the vicinity with similar operating hours to the ones proposed. With regard to the loss of neighbours amenity caused by noise disturbance paragraph 5.5 of the SPG states *'it should be acknowledged that residents living within designated centres cannot expect the same standards of amenity as those living in a wholly residential area'*. Members should also be aware that the Councils Shared Regulatory Service (Pollution Control) has noted that there has been no specific complaints regarding noise and other issues from neighbouring occupiers relating to the extended hours of use of these premises and therefore has no objection to the premises operating permanently during these hours. Regard however, has been given to the fact that the premises include an external seating area, and therefore, it is considered necessary to impose a condition restricting the hours of use to 9.00pm in-line with the advice contained in paragraph 5.26 of the SPG 'Food, Drink and Leisure Uses' (November 2017). Having regard to this context and subject to a condition restricting hours of use it is considered that extended opening hours would not have an unreasonable amenity impact.

With regard to allowing hot food to be served on the premises and the potential impact on the neighbouring occupiers, it should be noted that the ground floor

of the building is already in use as an A3 café and the reheating of toasties, paninis and hot sandwiches is already taking place on the premises and was allowed on appeal under planning permission 09/00335/DCO. This application seeks to vary condition 20 of the planning permission so that foods other than those specified in the condition can be sold from the premises using the similar methods of cooking/heating. It is not considered that the use of the premises to sell food other than those specified in the condition using similar methods of reheating to those that are currently used would not cause a harm to the amenities of the neighbours different to one caused by the permitted use of the premises granted under planning permission 13/02323/DCO.

### Transport Considerations

With regard to highway safety and parking, the premises are sited on the edge of the Local Centre in a highly sustainable location within walking distance of nearby residential areas and close to public transport links. Having regard to this context it is not envisaged that the premises would attract significant traffic movements above and beyond those that could be expected if the authorised use was to continue.

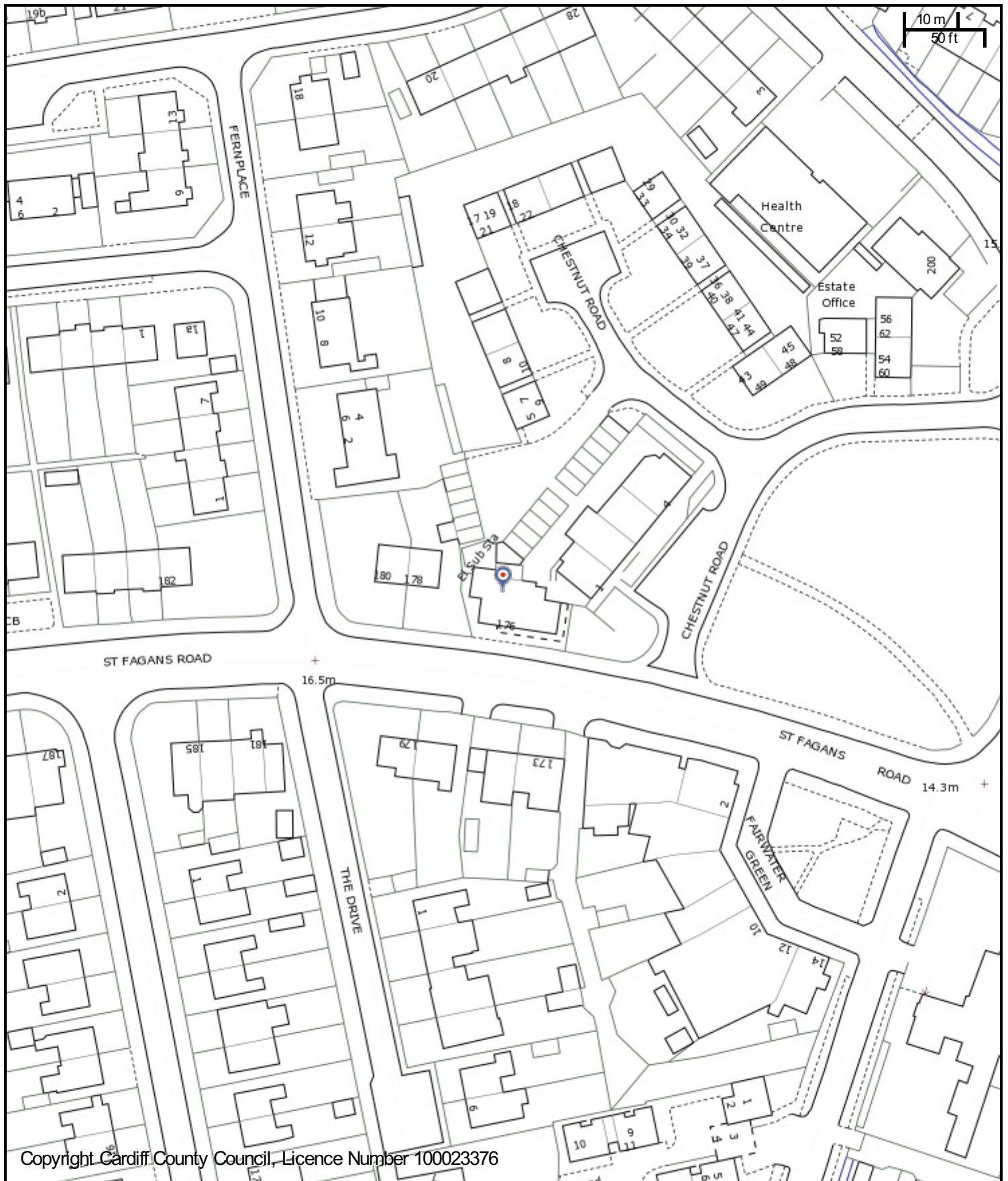
### Other considerations

In regard to comments made by neighbours which are not covered above members should note that Planning Policy Wales makes it clear that it is no part of the planning system to interfere with normal competition between commercial activities.

### Conclusion

It is concluded that the application is acceptable in accordance with the planning policies listed, and it is recommended that planning permission be granted, subject to conditions.





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CHIEF EXECUTIVE

Neuadd y Sir, Glanfa'r Iwerydd  
 CAERDYDD CF10 4UW  
 Tel: 029 20872088

County Hall, Atlantic Wharf  
 CARDIFF CF10 4UW  
 Tel: 029 20872087

**Cyngor Caerdydd**

**Cardiff Council**



**Title**

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**LOCAL MEMBERS, A.M. AND M.P. OBJECTIONS**

COMMITTEE DATE: 18/12/2019

APPLICATION No. **19/02825/MNR** APPLICATION DATE: 24/10/2019

ED: **PLASNEWYDD**

APP: TYPE: Full Planning Permission

APPLICANT: WPI Estates

LOCATION: 217-223 NEWPORT ROAD, ROATH, CARDIFF

PROPOSAL: PROPOSED DEMOLITION OF EXISTING REAR  
OUTBUILDINGS TO 217-223 NEWPORT ROAD AND  
CONSTRUCTION OF TWO 15 BED HOUSES OF MULTIPLE  
OCCUPANCY WITH ON-SITE AMENITY, PARKING, CYCLE &  
REFUSE STORES

**RECOMMENDATION 1:** That planning permission be **GRANTED** subject to the following conditions:

1. C01
2. The development shall be carried out in accordance with the following approved plans and documents:
  - AL(01)02 - Plots A & B ground floor plan
  - AL(01)03 - Plots A & B upper floor plans
  - AL(01)10 - Proposed elevations
  - AL(090)01 - Location plan

Reason: For the avoidance of doubt as to the extent of the permission.

3. The internal layout of the development shall be constructed strictly in accordance with the approved plans and no future alteration shall be made to the internal layout, including the conversion of any rooms to self-contained flats.

Reason: To protect the amenities of future occupiers and the surrounding area, to ensure that car/cycle parking facilities and amenity space are appropriate to the development, and to ensure that any additional need for contributions under Section 106 of the Town and Country Planning Act 1990 that may arise as a result of such changes are properly assessed, in accordance with policies KP5, KP6, KP7, H3 and T5 of the Cardiff Local Development Plan.

4. Only foul water from the development site shall be allowed discharge to the public sewerage system and this discharge shall be made to the 225mm combined sewer between manhole reference number ST19777503 and ST19777401 as indicated on the extract of the Sewerage Network Plan attached to this decision notice.

Reason: To prevent hydraulic overloading of the public sewerage system, to protect the health and safety of existing residents and ensure no pollution of or detriment to the environment, in accordance with policy EN10 of the Cardiff Local Development Plan.
5. No surface water and/or land drainage shall be allowed to connect directly or indirectly with the public sewerage network.

Reason: To prevent hydraulic overloading of the public sewerage system, to protect the health and safety of existing residents and ensure no pollution of or detriment to the environment, in accordance with policy EN10 of the Cardiff Local Development Plan.
6. Prior to the construction of the buildings above foundation level, samples of the external finishing materials shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the finished appearance of the development is in keeping with the area in accordance with policy KP5 of the Cardiff Local Development Plan.
7. No development, including demolition works, shall take place until details of tree protection boxes for the protection of the street trees adjacent to the development on Roath Court Road have been submitted to and approved in writing by the Local Planning Authority. The approved tree protection boxes shall be erected prior to the commencement of development and shall be retained in situ for the full course of demolition and construction. Any excavations / demolition close to the protected trees shall be undertaken under arboricultural supervision and tree roots shall be dealt with in accordance with section 7.2 of BS 5837:2012.

Reason: In the interests of visual amenity and tree protection, in accordance with policies KP5 and EN8 of the Cardiff Local Development Plan.
8. Prior to the construction of the buildings above foundation level, a plan indicating the positions, height, design, materials and type of boundary treatment to be erected shall be submitted to and approved by the local planning authority. The submitted details shall include, but not be limited to, proposals for walls with railings and/or hedges along the Roath Court Road frontage and lockable gates to prevent access to the rear of the site by unauthorised persons. The installation of the boundary treatment and gates shall be completed as approved before the buildings are occupied.

Reason: In the interests of visual and residential amenity and site security, in accordance with policies KP5 and C3 of the Cardiff Local Development Plan.

9. No equipment, plant or materials shall be brought onto the site for the purpose of development until full details of both hard and soft landscape works have been submitted to and approved in writing by the Local Planning Authority. These details shall include:
- A landscaping implementation programme.
  - Proposed finished levels;
  - Hard surfacing materials;
  - Proposed and existing services, lighting, CCTV and drainage above and below ground level to demonstrate there will be no conflict with proposed landscaping;
  - Scaled planting plans prepared by a qualified landscape architect;
  - Schedules of plant species, sizes, numbers and densities, prepared by a qualified landscape architect;
  - Scaled tree pit sectional and plan drawings prepared by a qualified landscape architect that show the Root Available Soil Volume for each tree;
  - Topsoil and subsoil specification for all planting types, including full details of soil assessment, protection, stripping, storage, handling, amelioration and placement to ensure it is fit for purpose. Where imported planting soils are proposed, full specification details shall be provided including the parameters for all imported planting soils, a soil scientists interpretive report demonstrating that the planting soil is appropriate for the landscaping type proposed and not only meets British Standards, but exceeds them in terms of suitability for the proposed end use. The specification shall be supported by a methodology for handling, amelioration and placement;
  - Planting methodology and post-planting aftercare methodology prepared by a qualified landscape architect, including full details of how the landscape architect will oversee landscaping implementation and report to the Local Planning Authority to confirm compliance with the approved plans and specifications.

The details shall be consistent with other plans submitted in support of the application and the landscaping shall be carried out in accordance with the approved design and implementation programme.

Reason: To maintain and improve the appearance of the area in the interests of visual amenity, to mitigate against the effects of climate change and to protect and conserve green infrastructure, in accordance with policies KP5, KP15 and KP16 of the Cardiff Local Development Plan.

10. Any trees, plants, or hedgerows included in the approved landscaping scheme which, within a period of five years from the completion of the

development, die, are removed, become seriously damaged or diseased, or become (in the opinion of the Local Planning Authority) otherwise defective, shall be replaced in the current planting season or the first two months of the next planting season, whichever is the sooner, unless the Local Planning Authority gives written consent to any variation.

Reason: To maintain and improve the appearance of the area in the interests of visual amenity, to mitigate against the effects of climate change and to protect and conserve green infrastructure, in accordance with policies KP5, KP15 and KP16 of the Cardiff Local Development Plan.

11. No development shall commence, including any works of demolition, until a Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved statement shall be adhered to throughout the construction period. The statement shall provide for:

- i) the parking of vehicles of site operatives and visitors;
- ii) loading and unloading of plant and materials;
- iii) storage of plant and materials used in constructing the development;
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate;
- v) wheel washing facilities;
- vi) measures to control the emission of dust and dirt during demolition and construction; and
- vii) a scheme for recycling/disposing of waste resulting from demolition and construction works.

Reason: In the interests of highway safety and public amenity, in accordance with policies T5, T6 and EN13 of the Cardiff Local Development Plan.

12. Notwithstanding the details shown on the approved plans, prior to the commencement of development plans showing details of secure and covered cycle parking spaces at a rate of at least one space per bedroom shall be submitted to and approved in writing by the local planning authority. The approved details shall be implemented prior to the development being put into beneficial use. Thereafter the cycle parking spaces shall be maintained and shall not be used for any other purpose.

Reason: To ensure that adequate provision is made for the secure parking of cycles in accordance with policies KP5 and T5 of the Cardiff Local Development Plan.

13. Notwithstanding the details shown on the approved plans, prior to the commencement of development, plans showing details of facilities for the storage of refuse and recycling containers within the site shall be submitted

to and approved in writing by the Local Planning Authority, and thereafter refuse and recycling containers shall only be stored in accordance with the approved details.

The storage area shall be large enough to accommodate the following –

Dry Recyclables:	2 x 1100 litre bulk bins
General waste:	2 x 1100 litre bulk bins

and shall also include a designated area for the storage of bulky waste.

**RECOMMENDATION 2:** The applicant is advised that to operate a House in Multiple Occupation (HMO) in this location, and of this size, they will need both planning permission and a licence. Both disciplines have different requirements, and the applicant should contact Private Sector Housing on 02920 871762 or [privatesectorhousing@cardiff.gov.uk](mailto:privatesectorhousing@cardiff.gov.uk) to discuss licence requirements at the first available opportunity.

**RECOMMENDATION 3:** To protect the amenities of occupiers of other premises in the vicinity attention is drawn to the provisions of Section 60 of the Control of Pollution Act 1974 in relation to the control of noise from demolition and construction activities. Further to this the applicant is advised that no noise audible outside the site boundary adjacent to the curtilage of residential property shall be created by construction activities in respect of the implementation of this consent outside the hours of 0800-1800 hours Mondays to Fridays and 0800 - 1300 hours on Saturdays or at any time on Sunday or public holidays. The applicant is also advised to seek approval for any proposed piling operations.

**RECOMMENDATION 4:** The developer is advised that the site is crossed by a public sewer, with the approximate position being marked on the Statutory Public Sewer Record. The position of the sewer must be accurately located and marked out on site before works commence and no operational development should be carried out within 3 metres either side of its centre line.

**RECOMMENDATION 5:** The applicant may need to apply to Dwr Cymru / Welsh Water for any connection to the public sewer under S106 of the Water industry Act 1991. If the connection to the public sewer network is either via a lateral drain (i.e. a drain which extends beyond the connecting property boundary) or via a new sewer (i.e. serves more than one property), it is now a mandatory requirement to first enter into a Section 104 Adoption Agreement (Water Industry Act 1991). The design of the sewers and lateral drains must also conform to the Welsh Ministers Standards for Gravity Foul Sewers and Lateral Drains, and conform with the publication "Sewers for Adoption"- 7th Edition. Further information can be obtained via the Developer Services pages of [www.dwrcymru.com](http://www.dwrcymru.com) The applicant is also advised that some public sewers and lateral drains may not be recorded on maps of public sewers because they were originally privately owned and were

transferred into public ownership by nature of the Water Industry (Schemes for Adoption of Private Sewers) Regulations 2011. Under the Water Industry Act 1991 Dwr Cymru Welsh Water has rights of access to its apparatus at all times.

**RECOMMENDATION 6:** The applicant is advised that a designated area for the storage of bulky waste is now a compulsory element of all communal bin stores. This area must have appropriate access to allow Council collection crews to remove the items. Waste Management do not accept keys to bin stores therefore either the gate would need to be left open on collection day or an access code would need to be provided. Communal bin stores should have double doors that open outward. Surfaces should be smooth and impervious and the floor must be laid to create suitable drainage. Adequate lighting must be provided - natural or artificial - and good natural ventilation if completely enclosed. It should also be noted that the collection method utilised by Cardiff Council will likely change in the future which could result in different/more receptacles being required for the storage of waste between collections. In light of this, the bin store areas should allow a degree of flexibility in order to be able to adapt to potential collection changes. As bulk containers are specified for this development, access paths to the kerbside for collection should be at least 1.5 metres wide, clear of obstruction, of a smooth surface with no steps. Dropped kerbs should also be provided to ensure safe handling of bulk bins to the collection vehicle. Bulk containers must be provided by the developer/other appropriate agent, to the Councils' specification (steel containers are required where capacity exceeds 240 litres) as determined by S46 of the Environment Protection Act 1990 and these can be purchased directly from the Council. Please contact Waste Management's commercial department for further information on 02920 717504. Further relevant information can be found in the Cardiff Council Supplementary Planning Guidance "Waste Collection and Storage Facilities.

**RECOMMENDATION 7:** In the interests of security it is recommended that:

- Access should be denied to the rear of the development by unauthorised persons by installing lockable gates to the front building lines of each block.
- Audio and visual access control systems should be installed on each communal entrance door.
- PAS24 2016 compliant entrance/exit doors should be used for the residential blocks.
- PAS24 2016 compliant ground floor windows should be installed.
- Every individual dwelling unit door should be PAS24 2016 compliant.
- An adequate fire risk assessment should be in place in relation to fire prevention, detection and evacuation.
- A premises management plan should be put in place.
- The premises should meet "Secured by Design" standards and be inspected by the police (to minimise crime risk).

**RECOMMENDATION 8:** The developer is advised that bats often roost in houses and other buildings, and work on these buildings may disturb a bat roost. All bats



and their roosts are protected against disturbance under UK and European legislation. If works are planned on a building in which bats are known or found to be roosting, Natural Resources Wales (NRW) must be contacted for advice.

If work has already commenced and bats are found, or if any evidence that bats are using the site as a roost is found during works, work should cease and NRW should be contacted immediately.

Where there is a likelihood that bats are present, or where bats are found to be present, a suitably qualified and experienced ecological consultant should be contracted to provide an assessment of the impact of the proposed works, and undertake bat surveys if necessary.

Where bats or their roosts are present, no works of site clearance, demolition or construction should take place unless a licence to disturb these species and/or their roosts has been granted in accordance with the relevant legislation. Otherwise, a prosecution may result..

NRW can be contacted at:-

Natural Resources Wales, Tŷ Cambria, 29 Newport Road, Cardiff CF24  
0TP, 0300 065 3000

Bat Conservation Trust can be contacted at:-

5th Floor, Quadrant House, 250 Kennington Lane, London, SE11 5DR, 0845  
1300228

**RECOMMENDATION 9:** The developer is advised that nesting birds are protected under the Wildlife and Countryside Act 1981: Part 1, 1(1)(b), and it is an offence to intentionally take, damage or destroy the nest of any wild bird while that nest is in use or being built. Any work involving the alteration or removal of trees or hedges should ensure that no nesting birds are disturbed. Any such works should be avoided between the months of March and September if possible; however, if the works are essential then any vegetation to be cleared should be inspected by a competent person prior to operations to identify the location of any active bird nests. If active nests are encountered, these should be protected by a 5m ring of vegetation around the nest. Work should not recommence within this exclusion area until the young have fledged and the nest vacated. Birds also use buildings and other structures as nesting sites and some species breed all year.

**RECOMMENDATION 10:** Contamination assessments and the effects of unstable land are considered on the basis of best information available to the Planning Authority and are not necessarily exhaustive. The Authority takes due diligence when assessing these impacts; however you are reminded that the responsibility for –

- i) determining the extent and effects of such constraints and

- ii) safe development and secure occupancy of the site rests with the developer.

Proposals for areas of possible land instability should take due account of the physical and chemical constraints and may include action on land reclamation or other remedial action to enable beneficial use of unstable land.

The Local Planning Authority has determined the application on the basis of the information available to it, but this does not mean that the land can be considered free from contamination.

**RECOMMENDATION 11:** On the 7<sup>th</sup> January 2019 Schedule 3 of the Flood and Water Management Act 2010 was enacted. This affects all new developments where the construction area is of 100 square metres or more. Cardiff Council is aware that your application for planning permission was validated after the recent legislative change in which Schedule 3 of the Flood and Water Management Act was enacted and therefore may be subject to surface water drainage proposals under the SAB application process. It is recommended that the developer engage in consultation with the Cardiff Council SAB team as the determining SuDS Approval Body (SAB) in relation to their proposals for SuDS features. Cardiff Council are aware that this is new legislation and as such we are offering a free pre-application service for the first year. To arrange discussion regarding this please contact [SAB@cardiff.gov.uk](mailto:SAB@cardiff.gov.uk)

In the meantime if you require further information please review our website: <https://www.cardiff.gov.uk/ENG/resident/planning-and-suds/suds-approval-body/>

Or, alternatively you can review the legislation set by Welsh Government here: <https://gweddill.gov.wales/topics/environmentcountryside/epq/flooding/drainage/>

## 1. **DESCRIPTION OF PROPOSED DEVELOPMENT**

- 1.1 The application seeks full planning permission for the demolition of existing garages and outbuildings at the rear of 217 to 223 Newport Road and the construction of two detached 3 storey Houses in Multiple Occupation, each containing 15 bedrooms, on the site to the rear of 221 and 223 Newport Road, fronting onto Roath Court Road.
- 1.2 The two blocks will each have 3 floors containing 5 kitchen/bed/living rooms and shared bathroom facilities. Access into the buildings will be via the Roath Court Road frontage. There will also be gated access for residents to the rear of the site where there will be approximately 112 sq.m of outdoor amenity space (including clothes drying areas), a 9 space car park, a bin store and a bicycle store, accessed via the adopted lane that runs along the rear of 187 to 223 Newport Road linking Roath Court Road and Oakfield Street.

- 1.3 The buildings will be set back from the highway behind a wall and a narrow strip of planting and will be finished in red brickwork and painted render with grey synthetic slate pitched roofs. There will be two flat roofed 2 storey projecting bays on the front elevation of each building (finished in brick), located either side of the central entrance door, and three roof gables. Windows and doors will be grey UPVC double glazed units and fascias, bargeboards and downpipes will be coloured UPVC.

## 2. **DESCRIPTION OF SITE**

- 2.1 The application site covers 0.09 hectares and is situated on the corner of Newport Road and Roath Court Road, extending behind 217, 219, 221 and 223 Newport Road as far as the rear access lane which links Roath Court Road and Oakfield Street.
- 2.2 223 Newport Road is a detached, stone faced, gabled and slate roofed Victorian building with stone bays to the front elevation. 217 to 221 Newport Road are terraced buildings of the same design, which have been converted to flats/HMOs. The side and rear walls and the rear annexes are faced in concrete render. Each has a substantial rear garden and there are also a number of single storey garage outbuildings which adjoin the rear lane.
- 2.3 Further to the west and on the opposite side of Newport Road there are more semi-detached and terraced Victorian villas, many of which have been converted to such uses as flats, offices, care homes and guest houses. To the east the houses on Newport Road are of a smaller scale, finished in red brick and render, and date from the early 20<sup>th</sup> century. To the north and east of the site along Roath Court Road are two storey hipped roof semi-detached and terraced dwellings faced in red brick, set back from the highway behind low red brick boundary walls.
- 2.4 The site to the rear of 217, 219, 221 and 223 Newport Road is overgrown and largely unused, and is bounded by a tall concrete block wall. There is a gap in the wall and an existing vehicular access onto Roath Court Road.

## 3. **SITE HISTORY**

- 3.1 18/01669/MJR – Proposed demolition of existing annex to 223 and rear outbuildings to 217-223 Newport Road, construction of two 15 bed HMO's & one 6 bed HMO with onsite amenity, parking, cycle & refuse stores. Refused – the site area met the threshold for the provision of affordable housing but the proposal did not include appropriate affordable housing on site, an acceptable scheme for alternative off-site provision, confirmation from the relevant parties that they were willing to provide a financial contribution in lieu of such provision, or a satisfactory viability appraisal setting out how the developer was unable to meet the full policy requirements.

- 3.2 08/01551/C – Demolition of all buildings on site. construction of 47 self contained apartments with on site car and cycle parking, bin stores and amenity facilities. Permission granted 21/09/2011. Development commenced September 2016.
- 3.3 03/262R – Permission granted for conversion of 217 Newport Road from 2 to 5 flats.
- 3.4 93/1875R – Permission refused for a change of use of the site to a drug dependency unit.

#### 4. **POLICY FRAMEWORK**

- 4.1 *Cardiff Local Development Plan 2006-2021:*
  - KP5 (Good Quality and Sustainable Design);
  - KP13 (Responding to Evidenced Social Needs);
  - KP15 (Climate Change);
  - KP16 (Green Infrastructure);
  - EN8 (Trees, Woodlands and Hedgerows);
  - EN10 (Water Sensitive Design);
  - EN13 (Air, Noise, Light Pollution and Land Contamination);
  - T5 (Managing Transport Impacts);
  - C3 (Community Safety/Creating Safe Environments);
  - W2 (Provision for Waste Management Facilities in Development).
- 4.2 *Supplementary Planning Guidance:*
  - Houses in Multiple Occupation (October 2016).
  - Waste Collection and Storage Facilities (October 2016).
  - Cardiff Residential Design Guide (January 2017).
  - Cardiff Infill Sites (November 2017).
  - Green Infrastructure (November 2017).
  - Managing Transportation Impacts (Incorporating Parking Standards) (2018).
- 4.3 *Planning Policy Wales (Edition 10 – December 2018):*
  - 2.2 All development decisions, either through development plans policy choices or individual development management decisions should seek to contribute towards the making of sustainable places and improved well-being.
  - 2.8 Planning policies, proposals and decisions must seek to promote sustainable development and support the well-being of people and communities across Wales.
  - 3.3 Good design is fundamental to creating sustainable places where people want to live, work and socialise. To achieve sustainable development, design must go beyond aesthetics and include the social, economic, environmental, cultural aspects of the development, including how space is used, how buildings and the public realm support this use, as well as its construction, operation, management, and its relationship with the surrounding area.
  - 3.4 Meeting the objectives of good design should be the aim of all those involved in the development process and applied to all development proposals, at all scales.

3.6 Development proposals must address the issues of inclusivity and accessibility for all.

3.7 Developments should seek to maximise energy efficiency and the efficient use of other resources (including land), maximise sustainable movement, minimise the use of non-renewable resources, encourage decarbonisation and prevent the generation of waste and pollution.

3.8 Good design can help to ensure high environmental quality. Landscape and green infrastructure considerations are an integral part of the design process.

3.9 The layout, form, scale and visual appearance of a proposed development and its relationship to its surroundings are important planning considerations.

3.11 Local authorities are under a legal obligation to consider the need to prevent and reduce crime and disorder in all decisions that they take.

3.12 Good design is about avoiding the creation of car-based developments. It contributes to minimising the need to travel and reliance on the car, whilst maximising opportunities for people to make sustainable and healthy travel choices for their daily journeys.

3.47 Higher densities should be encouraged in urban centres and near major public transport nodes or interchanges, to generate a critical mass of people to support services such as public transport, local shops and schools

3.51 Previously developed (also referred to as brownfield) land should, wherever possible, be used in preference to greenfield sites where it is suitable for development. In settlements, such land should generally be considered suitable for appropriate development where its re-use will promote sustainability principles and any constraints can be overcome. It is recognised, however, that not all previously developed land is suitable for development. This may be, for example, because of its unsustainable location, the presence of protected species or valuable habitats or industrial heritage, or because it is highly contaminated.

4.1.8 The Welsh Government is committed to reducing reliance on the private car and supporting a modal shift to walking, cycling and public transport. Delivering this objective will make an important contribution to decarbonisation, improving air quality, increasing physical activity, improving the health of the nation and realising the goals of the Well-being of Future Generations Act.

4.1.11 It is Welsh Government policy to require the use of a sustainable transport hierarchy in relation to new development, which prioritises walking, cycling and public transport ahead of the private motor vehicles.

4.1.13 The sustainable transport hierarchy must be a key principle in the preparation of development plans, including site allocations, and when considering and determining planning applications.

4.1.30 Planning authorities must support active travel by ensuring new development is fully accessible by walking and cycling.

4.1.33 In determining planning applications, planning authorities must ensure development proposals, through their design and supporting infrastructure, prioritise provision for access and movement by walking and cycling and, in doing so, maximise their contribution to the objectives of the Active Travel Act.

4.1.34 New development must provide appropriate levels of secure, integrated, convenient and accessible cycle parking and changing facilities. As well as

providing cycle parking near destinations, consideration must also be given to where people will leave their bike at home.

4.1.36 Planning authorities must direct development to locations most accessible by public transport. They should ensure that development sites which are well served by public transport are used for travel intensive uses, such as housing, jobs, shopping, leisure and services, reallocating their use if necessary.

4.1.51 A design-led approach to the provision of car parking should be taken, which ensures an appropriate level of car parking is integrated in a way which does not dominate the development. Parking provision should be informed by the local context, including public transport accessibility, urban design principles and the objective of reducing reliance on the private car and supporting a modal shift to walking, cycling and public transport. Planning authorities must support schemes which keep parking levels down, especially off-street parking, when well designed. The needs of disabled people must be recognised and adequate parking provided for them.

4.1.52 Planning authorities must require good standards of car parking design, which do not allow vehicles to dominate the street or inconvenience people walking and cycling. Car parking should be overlooked by surrounding properties, to provide natural surveillance.

4.1.53 Parking standards should be applied flexibly and allow for the provision of lower levels of parking and the creation of high quality places. 4.2.22 Planning authorities will need to ensure that in development plans and through the development management process they make the most efficient use of land and buildings in their areas. Higher densities must be encouraged on sites in town centres and other sites which have good walking, cycling and public transport links. 4.2.23 Infill and windfall sites can make a useful contribution to the delivery of housing. Proposals for housing on infill and windfall sites within settlements should be supported where they accord with the national sustainable placemaking outcomes.

5.12.9 Adequate facilities and space for the collection, composting and recycling of waste materials should be incorporated into the design and, where appropriate, layout of any development as well as waste prevention measures at the design, construction and demolition stage.

6.2.5 The quality of the built environment should be enhanced by integrating green infrastructure into development through appropriate site selection and use of creative design.

6.6.17 New developments of more than one dwelling or where the area covered by construction work equals or exceeds 100 square metres also require approval from the SuDS Approval Body (SAB) before construction can commence. Adoption and management arrangements, including a funding mechanism for maintenance of SuDS infrastructure and all drainage elements are to be agreed by the SAB as part of this approval. This will ensure that SuDS infrastructure is properly maintained and functions effectively for its design life.

6.6.18 The provision of SuDS must be considered as an integral part of the design of new development and considered at the earliest possible stage when formulating proposals for new development.

4.4 *Technical Advice Note 12 - Design (March 2016).*

## 5. **INTERNAL CONSULTEE RESPONSES**

5.1 *Transportation:* The proposal is for 9 car parking spaces in the rear area (same number as for the refused application) and whilst there are now proposed to be 6 fewer HMO's and thus technically car parking spaces should reduce pro-rata, given the existing situation at the rear where cars could park, I would accept 9 car spaces with this application.

5.2 A 30 bed HMO development should have a minimum of 30 cycle parking spaces, whilst 56 are shown on the submitted plan and 'over 70' referenced in the DAS. It is not clear if the proposed cycle store is to also cover provision for the existing units at the front of 217-223 or for the remaining future redevelopment of those units (allowed under 08/01551). However, if the latter, the additional cycle parking proposed (above the 30 required for this application) would need to conform (as a minimum) with the details set out in 16/01701. In any case, with regards this application, the proposed cycle stands need to have minimum horizontal spacings of 1m and minimum aisle widths of 1.8m. In addition there needs to be a door/gate for the cycle store to allow appropriate security. Further cycle plan details should be supplied.

5.3 *Waste Strategy & Minimisation Officer:* The refuse storage area, shown in the site plans, has been noted and is acceptable, however Waste Management do not accept keys to bin stores therefore either the gate would need to be left open on collection day or an access code would need to be provided. The refuse storage area must be large enough to accommodate the following recommended provisions for 30 bedrooms: 2 x 1100 litre bulk bins for Dry Recyclables; 2 x 1100 litre bulk bins for general waste; 1 x 240 litre bin for food waste. Please note, the collection method utilised by Cardiff Council will likely change in the future which could result in different/more receptacles being required for the storage of waste between collections. In light of this, the bin store areas should allow a degree of flexibility in order to be able to adapt to potential collection changes. A designated area for the storage of bulky waste is now a compulsory element of all communal bin stores. This area will prevent unwanted waste being left in the communal bin store or other areas, thereby improving the aesthetics of the site. Communal bin stores should have double doors that open outward. Surfaces should be smooth and impervious to permit cleaning and the floor must be laid to create suitable drainage. Adequate lighting must be provided- natural or artificial, and good natural ventilation if completely enclosed. The developer is advised; as bulk containers are specified for this development, access paths to the kerbside for collection should be at least 1.5 metres wide, clear of obstruction, of a smooth surface with no steps. Dropped kerbs should also be provided to ensure safe handling of bulk bins to the collection vehicle. Bulk containers must be provided by the developer/other appropriate agent, to the Councils' specification (steel containers are required

where capacity exceeds 240 litres) as determined by S46 of the Environment Protection Act 1990. Refuse storage, once implemented, must be retained for future use

## 6. **EXTERNAL CONSULTEES RESPONSES**

- 6.1 *Welsh Water*: Request conditions to ensure there is no detriment to the public sewerage system - only foul water from the development site shall be allowed discharge to the public sewerage system and this discharge shall be made to the 225mm combined sewer between manhole reference number ST19777503 and ST19777401 (as indicated on the extract of the Sewerage Network Plan provided), and no surface water and/or land drainage shall be allowed to connect directly or indirectly with the public sewerage network. Records also indicate that the proposed site is crossed by a public sewer. Under the Water Industry Act 1991 Dwr Cymru Welsh Water has rights of access to its apparatus at all times. The required protection zone of 3 metres either side of the centre line of the sewer may impact upon new build dwellings and it is recommended that this is given appropriate consideration during the design and determination process.
- 6.2 *Police Crime Prevention Design Advisor*: The plans for the new build at 217 -233 Newport Road will potentially increase the number of vehicles in the street and potentially have an adverse effect on other residents already living in the area. The plans show a cycle store. As incidents of theft of bicycles are high within the Cardiff area, if a cycle store is to be installed it needs to be a secure store which can be locked and adequately lit.
- 6.3 On the basis of the above evidence South Wales Police would not object to this application. However high density of HMO'S are a significant contributory factor to the high levels of crime and disorder and as Plasnewydd does have a high number of HMO, an increase in capacity in this area is likely to exasperate the existing problems. However, if the Authority is mindful to grant this application South Wales Police would recommend the following:
- PAS 24: 2016 ground floor windows
  - PAS 24:2106 external entrance doors
  - An access control system on the main entrance door
  - Lighting above the main entrance door
  - Secure rear garden with a minimum of 1.8 m fencing/ walling/gating
  - Secure cycle storage facility which is overlooked.
  - Each individual room should have lockable doors
  - Appropriate Fire Risk Assessments are in place and include adequate fire warning and prevention measures.

South Wales Police would ask that the above recommendations be considered by way of condition.



## 7. **REPRESENTATIONS**

7.1 The application has been advertised by press notice, site notice and neighbour notification. 24 individual objections have been received. The grounds for objection are summarised as follows:

1. The neighbourhood already has too high a density of HMOs. The development will cater for a transient population and will change the character of the area which is currently made up of family homes. There will be an adverse impact on community cohesion.
2. The residents will create disturbance and noise nuisance.
3. Loss of privacy to existing properties.
4. Concern about the type of tenants the accommodation will be offered to. The risks of crime and anti-social behaviour are likely to increase. There will be a danger to surrounding residents.
5. There will be inadequate outdoor amenity space for the residents and outdoor amenity space for the existing buildings will be lost.
6. Poor quality of accommodation - the rooms will be too small, individuals will have little opportunity to socialise, they will have shared bathroom facilities which could lead to disagreements between residents, it is not clear how clothes washing and drying will be managed or how food waste will be disposed of when the collection caddy is normally kept outside by most households. There is also little attention to the 'green design' of the space outside.
7. The current property, especially its garden, is badly maintained. It is unlikely that the proposed development will be properly maintained once built.
8. Inadequate parking facilities. Inconsiderate parking and competition for parking spaces in Roath Court Road already causes problems. There is no evidence that an assessment of the parking impact has been made.
9. The proposed development is of poor quality. The design is out of keeping architecturally with the existing properties. The development does not respect the character of the area as it introduces three storey development onto a street characterised by traditional two-storey residences and does not follow the building line of frontages of houses on the street. The buildings will be too close together and too tall.
10. The development will place significant strain on existing local services and infrastructure.
11. There will be an increase in traffic using the rear lane and surrounding streets, which will adversely affect the safety of pedestrians and current road users.
12. There will be increased congestion on the surrounding roads, leading to gridlock at peak times.
13. During the development works there will be problems with construction vehicles blocking access etc.
14. The floor areas given on the plans are inaccurate and misleading.

15. The application wrongly states that no trees are affected. There are trees and hedges on site which would need to be removed, and building too far forward would compromise the health of the street trees.
16. There will be an increase in air pollution caused by the extra traffic.
17. The value of existing properties in the area will be affected.
18. There are insufficient school places in the area for any children accommodated in the development.
19. If the development is intended for students, there is already too much student accommodation in the area.
20. The site should be kept for parking and bin storage for the existing properties.
21. The site should be developed for family houses.
22. A precedent has been set for refusing development that would be out of character with Roath Court Road (an extension to 40 Princes Street).
23. The developer has made no effort to contact or speak to immediate neighbours about this new application. Their behaviour suggests a disregard for the feelings and amenity of local residents.
24. The applicant submitted then withdrew an appeal against the refusal of the previous application at the same time that they submitted this new application. This was probably done to create confusion amongst local residents.
25. The application does not comply with the Cardiff Residential Design Guide in that the drawings do not show the scale of the proposal in comparison with the existing neighbouring housing on Roath Court Rd. The context has not been addressed.
26. The application states the scale and massing is in line with the permission granted in 08/01551/C in 2011. However, this predates the council's infill policy which was approved in November 2017. Up to date policy should be used to assess this application. There are significant differences between this proposal and the previous scheme – the density is higher, the application is for HMOS rather than flats and the context is different – in early 2016 a major new development was completed at 158-164 Newport Rd containing 37 flats, directly opposite 217-223 Newport Rd, which has changed the parking and traffic levels in the surrounding vicinity.
27. The developer claims that planning permission granted on the site in 2011 (case 08/01551/C) has established the scale and principle of development on the site. This was for an entirely different scheme and involved the demolition of all buildings on the site and the construction of 47 new self-contained apartments. Moreover, in accordance with the provisions of Section 91 of the Town and Country Planning Act 1990, that permission was only applicable for five years. It was given on 21 September 2011 and thus expired in 2016. An application was made to remove the five-year limit in 2016 (16/00742/MJR) but it was withdrawn.
28. The building should be designed to be carbon neutral. Even if this is not yet enshrined in Building Regulations, a city that has declared a climate emergency should be mindful of this aspect.

29. Students have caused trouble in this area due to antisocial behaviour, parking and issues with refuse and rubbish dumped in gardens and the YMCA on Oakfield Road has already proved to be an issue at times.
- 7.2 Ward Councillors Peter Wong, Dan De'Ath, Mary McGarry and Sue Lent have submitted the following objections on behalf of a number of residents living in the surrounding area.:

Overdevelopment and change of character to the area.

The proposal is for the construction of two 15 bed HMOs. This is a 30 bed HMO development in a very small site accessed through a quiet residential street in the Plasnewydd area. Plasnewydd already suffers from a saturation of HMOs in the area including Newport Road, Oakfield Street, Connaught Road, Claude Road. This development is a clear over-intensification, breaching the saturation point guidance within the SPG on Houses of Multiple Occupancy.

Concentrations of HMOs clustered in small geographical areas detract from the character of those areas and actively contribute towards a reduction of community cohesion. This massive development, on the back of planning permission for multiple flats, will have a significant cumulative impact that will adversely affect the amenity and character of the area.

Additionally as per KP5 of the LDP, none of the proposed development's scale, form, massing, height, density, colour, materials, detailing could be described as in keeping with the distinctive Victorian residential houses in the area, and certainly not in keeping with the local character and context of the built and landscape setting. The proposed development bears no resemblance to the distinctive local Victorian architecture, heritage or fabric of the area.

Impact on Neighbours

It can be clearly seen from the submitted plans that the development faces both side on to houses on Roath Park Road, and the back gardens of Princes Avenue. The sheer size of the development will also have a detrimental impact on the privacy of the residential gardens on both Roath Park Road and Princes Avenue: the proposed flats on the higher levels will have a clear view into neighbour's gardens and rooms.

Paragraph 4.3 of the SPG on Houses of Multiple Occupancy notes that HMOs can lead to "Increased population density, leading to greater demand for infrastructure, such as waste collections and on-street parking." This is a massive HMO development in a residential area, and will create significant waste issues for the surrounding neighbours, and the lane next to it.

Paragraph 4.10 of the HMO SPG notes that "the concentration of HMOs as exists in parts of the city does not come without social or amenity costs. While any individual new HMO may not directly impact upon this, collectively, their concentration is something that the Council can justifiably feel may need management for the benefit of the community at large. In this respect, a concentration of HMOs may conflict with parts of policy KP13."

This massive 30 bed HMO development, on the annexe of an already huge development will directly impact on both the social and amenity of the local community.

We also note that the previous planning application 18/01669/MJR, was refused due to the failure of the applicant to include “appropriate affordable housing on site, an acceptable scheme for alternative off-site provision, confirmation from the relevant parties that they are willing to provide a financial contribution in lieu of such provision, or a satisfactory viability appraisal setting out how the developer is unable to meet the full policy requirements.”

This latest proposed development is identical to the previous application, with the exception of losing 6 bedrooms, and therefore also meets the affordable housing provision threshold set out in policy H3 of the Cardiff Local Development Plan. The plans do not indicate any provision of appropriate affordable housing on site or offsite or a financial contribution in lieu of such provision, and should also contravene policies KP6, KP7 and H3 of the Cardiff Local Development Plan and Cardiff Supplementary Planning Guidance “Planning Obligations.” The proposal should continue to be refused on these grounds.

7.3 Jenny Rathbone A.M. also wishes to object to this application as follows:

“I echo the comments that have been made by local residents in their objections.

The proposed development would increase the saturation of HMOs in the local area to a point that would breach the saturation point guidance of the Supplementary Planning Guidance on Houses of Multiple Occupancy.

This development, taken in combination with other planning permission granted in the area, would fundamentally change the character of the surrounding area. I note that this would mean the development would contravene the policies of the Local Development Plan.

Additionally, the parking provision provided for these flats is inadequate to the total number of houses. I believe that having only 9 parking spaces for what is effectively a 30-bed development is insufficient. When taken into account with the already severely stretched parking provision in the area, the proposed parking facilities would only exacerbate the current problem.

For these reasons, I believe that this application should be rejected.”

7.4 Jo Stevens MP objects to the application as follows:

“Overdevelopment and change of character to the area

The above proposal is to construct two 15 bed HMOs – equivalent to a 30 bed HMO on a small site, accessed through quiet residential streets in Plasnewydd. This local area already suffers from a saturation of HMOs, especially in the areas

surrounding Newport Road, Oakfield Street, Claude Road and Connaught Road. This development would breach the saturation point guidance within the Supplementary Planning guidance on Houses of multiple occupancy.

Policy H5 of the Local Development Plan also states: 'Proposals for any conversion to flats or Houses in Multiple occupation will be permitted where:

- ii. There would be no material harm to the amenity of existing, nearby residents by virtue of general disturbance, noise or overlooking.
- iii. The cumulative impact of such conversions will not adversely affect the amenity and/or the character of the area.
- iv. Does not have an adverse effect on local parking provision.

I believe this development, on the back of planning permission for multiple flats will have a significant impact that will only serve to adversely affect the character of the surrounding area.

#### Car parking and amenity space

The development provides for a mere 9 dedicated parking spaces. For a development of 30 bedrooms, this is clearly substandard and will only have a detrimental impact on local parking provision, which is already at a premium. I also have concerns about the lack of amenity space contained within the plans for a development of this size and scale.

#### Impact on neighbours

It is likely this development will have an impact on neighbours due to it facing on both sides onto Roath Park Road and the back gardens of properties on Princes Avenue. We must consider that the size of the development will result in a detrimental impact on the privacy of gardens on Roath Park Road and on Princes Avenue also.

For the reasons above, I write to object to the application”

## 8. **ANALYSIS**

8.1 A recent application for full planning permission for demolition of the annex to 223 and rear outbuildings to 217-223 Newport Road and the construction of two 15 bed HMO's and one 6 bed HMO on this site was refused on 18<sup>th</sup> April 2019 because the site area (more than 0.1 hectares) was large enough to trigger a requirement for the provision of affordable housing under policy H3 of the Cardiff LDP, and no such provision was offered, nor was the applicant willing to submit a viability appraisal setting out how the developer was unable to meet the full policy requirements.

8.2 The current application relates to a smaller site (0.09 hectares), the existing

- buildings, including the annex of 223 Newport Road that was to have been demolished, having been omitted from the proposals, and the site area no longer meets the affordable housing policy threshold. The single reason for refusal of the previous application was the developer's failure to comply with the policy requirements relating to affordable housing.
- 8.3 It is for the Planning Committee to determine whether these revised proposals are acceptable, taking into consideration the design of the development and the impact on visual amenity and the street scene, the impact on the amenities of neighbouring occupiers and whether the development will provide a satisfactory standard of accommodation for future occupiers, which were considered previously.
- 8.4 In design terms the proposals are identical to the 2018 application except that the rear annex of 223 Newport Road will now be retained and two rather than three detached 3 storey blocks will be constructed. The 2018 application was considered to be an improvement on the previously-approved scheme for 47 apartments (08/01551/C, granted in 2011 and commenced in 2016) as the development now consisted of separate "villas" with the existing buildings on Newport Road retained. The 3-storey scale of the buildings is one storey higher than other buildings on Roath Court Road but the third storey is contained partly within the roof space and the height (approximately 10.5m to the ridge of the roof) is therefore little different to that of the nearby houses, is typical of similar Victorian villas and is similar in scale to the 2011 development on the Roath Court Road frontage.
- 8.5 The architectural style of the buildings is considered to be acceptable. The blocks make reference to Victorian villas with their characteristic symmetry around an entrance, gables and bays. They lack the richness of detailing of many of the nearby original homes (such as door and windows surrounds, banding, leaded lights and mock timbering) but it is not unusual for new homes to lack such details. The site is not within a conservation area or close to any listed buildings that might justify closer adherence to traditional detailing. The external materials of the development (brick and render) will reflect the materials used in neighbouring buildings and a planning condition can be used to ensure that these are of an appropriate quality and are suitable for the area.
- 8.6 The existing street is also characterised by front gardens with boundary walls and hedging. The proposed development will also be set back behind a front boundary, although this will not be as deep as the existing front gardens in the area, and a planning condition will be required to ensure that the front boundary treatment matches the local character of wall and hedge as far as possible as these details are not shown on the submitted plans. The existing mature street trees will be retained on the site frontage, which will also help to minimise the visual impact of the development.
- 8.7 The scheme currently lacks detailed proposals for landscaping. An area of shared

amenity space is depicted on the plans but its treatment and usability is unclear. A planning condition is therefore required to ensure that suitable details of landscaping are submitted, including planting methodology, post-planting aftercare methodology and an implementation programme.

- 8.8 With regard to the impact on the amenities of neighbouring residents, there will be a distance of at least 21m between the habitable room windows of the new development and those of existing properties and more than 10.5m between the windows of the new development and the private outdoor amenity space of neighbouring houses. These distances are normally accepted as appropriate for the maintenance of privacy. The buildings will not be so tall and bulky that they will appear unacceptably overbearing and they will not overshadow the rear gardens of existing houses. Therefore it is not considered that there will be such an adverse effect on amenity as to justify the refusal of planning permission.
- 8.9 Regarding the living conditions of future occupiers of the development, the buildings are not typical HMOs both in terms of their design (with kitchens in the rooms rather than in a communal space) and size (having 15 bedrooms in each block) and there would be some concerns over what the maintenance and management arrangements would be. However, to operate an HMO in this location, and of this size, the applicant will need both planning permission and a licence, and matters such as management arrangements and the details of internal space standards would be dealt with by the Licensing system. HMO licensing also lays down fire safety requirements, applies standards relating to kitchen and bathroom facilities and sets maximum occupancy limits.
- 8.10 The room sizes would be unacceptably small if this were a development consisting of separate self-contained flats rather than HMOs. However, planning permission would be needed to convert the development to flats. For the avoidance of doubt, and to ensure that the standard of accommodation remains acceptable, a planning condition preventing changes to the internal layout is recommended.
- 8.11 The amount of outdoor amenity space – approximately 112 square metres - is acceptable for a development of this size. The Supplementary Planning Guidance “Houses in Multiple Occupation” (October 2016) requires a minimum of 95 square metres for this number of rooms. The amenity space would be shared between the 2 HMOs: ideally each would have its own separate amenity space but in this case, as the HMOs are within a self-contained development, this is considered acceptable. The amenity space will be south facing and will contain a drying area, and landscaping can be used to enhance its amenity value. It will be easily accessible to all residents. It should be noted that no Section 106 contributions relating to Public Open Space can be sought for this development as it relates to 2 individual residential units and therefore current policy does not permit the Council to request an open space contribution.
- 8.12 Adequate facilities for cycle parking can be provided for residents, although

amended plans are required via a condition as the number of proposed stands exceeds the minimum requirement and is acceptable but the horizontal spacings between the stands as shown on the current plans are not adequate, and also lockable doors would be required on the storage building for security purposes.

- 8.13 The number of car parking spaces proposed (9) is the same as for the previous, refused application. This exceeds the maximum standard set out in the SPG “Managing Transportation Impacts (Incorporating Parking Standards)” (2018), which, for a development of two HMOs, would be 2 spaces. However, 9 spaces was considered acceptable for the previous proposal for three HMOs and is acceptable for a development of two for the same reasons - the car parking facilities would be shared with the existing units (i.e. the 3 HMOs and 3 flats at 217 to 223 Newport Road) which would by themselves have a maximum car parking allowance of around 7 spaces, there are existing garages on the site that could already be providing parking facilities, and the existing 2011 consent, which remains extant, has a larger number of proposed on-site spaces. Whilst the current proposal is for a smaller development and therefore the number of car parking spaces ought to be reduced, Transportation officers are satisfied that the number of parking spaces proposed for this development is acceptable.
- 8.14 The concentration of HMOs in the area must also be considered. Concentrations of HMOs clustered in small geographical areas can detract from the character of those areas and actively contribute towards a number of perceived problems such as greater demand for infrastructure, less community cohesion and higher crime levels. This issue was considered when the previous application was determined, and Members did not consider the increase in the concentration of HMOs in the area to be unacceptable. The current application is for a smaller development, therefore it would be unreasonable to refuse planning permission on these grounds. The development will increase the concentration of HMOs in the immediate area above the 20% threshold set out in the HMOs SPG but developments of HMOs on brownfield sites, or developments of new HMOs that do not result in the loss of an existing C3 property, are viewed more favourably and in this case there will be no loss of any family dwellings, the development will be adjacent to existing HMOs and it will increase the housing stock in a sustainable location within a part of the city that already contains a mixture of uses such as flats, offices and care homes, as well as family houses. As such the principle of developing this site for HMOs remains acceptable.
- 8.15 With regard to the objections raised by local residents (as set out at paragraph 7.1 above):
1. The issue of the concentration of HMOs is discussed above. The character of Roath Court Road will remain as one of predominantly family homes. The development will not reduce the number of such properties and will be confined to the area within the curtilage of the existing HMOs close to the junction with Newport Road, where there is a wider mix of accommodation types.



2. Any noise generated by the residents of the properties would be domestic noise that would not be out of place in a residential area such as this. It cannot be assumed that the residents would generate high levels of noise and in any case excessive noise, causing a nuisance to others, is controlled under Environmental Health legislation and is not a planning matter.

3. This issue is discussed above. There will be adequate distances between existing and proposed windows and no overlooking of existing gardens.

4. It is not inevitable that residents will engage in crime or anti-social behaviour. Any such behaviour would be a matter for the police and would be dealt with under other legislation. South Wales Police have not objected to this application and had no objections to the previous proposals. The police have made several recommendations regarding security, and these have been incorporated into the suggested planning conditions and informative notes to the applicant.

5. The submitted plans demonstrate that an appropriate amount of outdoor amenity space will be provided for the new development. Little useable outdoor amenity space will be lost from the existing properties and 217 to 221 Newport Road will have direct access to the outdoor amenity space at the rear of the proposed HMOs as well as the new car parking, cycle storage and refuse storage areas, which will improve their amenities.

6. Internal space standards and bathroom facilities are controlled by the licensing regime. There will be no shared space for socialising other than the outdoor amenity space but this would also be true of a block of one-bedroom flats and does not constitute grounds for refusal of the application. It will be for the owner and residents to determine how clothes washing is managed, and there will be an outdoor clothes drying area within the shared amenity space. The food waste disposal needs would be the same as for a block of self-contained flats. Adequate facilities for the storage of waste and recycling containers will be provided and waste management officers have raised no concerns in this respect. Landscaping of the outdoor areas can be controlled via a planning condition to ensure that there will be "green space".

7. It cannot be assumed that the development will not be properly maintained. The licensing system will control issues around the management of the property.

8. The proposed development is not of a type or scale that requires the submission of a Transport Assessment or Travel Plan. The proposed parking facilities comply with the requirements of policy T5 of the Cardiff Local Development Plan, the adopted SPG and Planning Policy Wales (edition 10) which have no minimum standards for numbers of car parking spaces. The site is within the central area of the city, as defined in the SPG, where limiting the provision of parking is necessary to manage the demand for travel by car to central Cardiff and encourage travel by public transport, walking and cycling in order to achieve the Local Development Plan target of a 50:50 modal split. The SPG recognises that the availability of parking spaces and their location can influence travel choices and excessive provision can serve to stimulate demand for car travel and perpetuate reliance on the car. The application of parking standards to new developments is therefore an important tool in managing demand for travel by car and encouraging a shift to sustainable transport modes. Planning Policy Wales (2018) advises that "good

design is about avoiding the creation of car-based developments. It contributes to minimising the need to travel and reliance on the car, whilst maximising opportunities for people to make sustainable and healthy travel choices for their daily journeys” and it is Welsh Government policy to require the use of a sustainable transport hierarchy in relation to new development, which prioritises walking, cycling and public transport ahead of the private motor vehicles. Planning Policy Wales advises that the sustainable transport hierarchy must be a key principle when considering and determining planning applications and planning authorities must ensure development proposals, through their design and supporting infrastructure, prioritise provision for access and movement by walking and cycling and, in doing so, maximise their contribution to the objectives of the Active Travel Act.

9. Issues of design are addressed earlier in this report. It is not considered that the design and scale are so out of keeping that refusal of the application would be justified. The design, siting and scale of the buildings remain the same as for the previous application, when these aspects of the development were considered acceptable, and there will now be only two buildings rather than three, which further reduces visual impact.

10. It would be unreasonable to refuse planning permission for a development on the grounds that the new residents will use the existing facilities and services in the area. If there is increased demand for services, this should be managed by the service-providers. Waste management officers have raised no concerns relating to waste collections, Welsh Water have no concerns regarding water and sewerage infrastructure and highways/transportation officers have no concerns regarding parking or increased use of the highway network.

11. Highways/transportation officers have raised no concerns regarding traffic or pedestrian and highway safety.

12. Highways/transportation officers have raised no concerns regarding traffic congestion. The site is in a sustainable, central location where it will not be necessary for residents to use private cars. Any future increase in private car usage could not be directly attributable to this development.

13. A construction management scheme can be required, which would minimise disruption caused by building works. It would be unreasonable to refuse planning permission on the grounds of the temporary disturbance that would be caused during construction.

14. The annotation on the floor plans indicates that all the rooms would have a floor area of 20.5 square metres. This appears to be reasonably accurate when the scaled plans are measured.

15. There are no trees or hedgerows on the site of a quality that would justify their retention. The Tree Preservation Officer has confirmed that the street trees on the Roath Court Road frontage will not suffer unacceptable harm subject to protection with tree protection boxes erected for the full course of demolition and construction, and any excavations/demolition close to them being undertaken under arboricultural supervision and roots dealt with in accordance with section 7.2 of BS 5837:2012.

16. The development will not increase the amount or nature of traffic in the area to such an extent that air quality would be affected. The site is not within or close to any existing Air Quality Management Area.

17. The impact of development on the value of other property is not a material planning consideration and cannot be taken into consideration in the determination of this application.

18. It is highly unlikely that any children will live in this development.

19. The application is not specifically for student accommodation, it is for Houses in Multiple Occupation, therefore the accommodation will be available to the general population. As with any other HMOs, there would be nothing to prevent students becoming residents.

20. The Local Planning Authority must consider the acceptability of the development that has been applied for, irrespective of whether others may prefer an alternative scheme. The site is privately owned and the Council cannot dictate what the owner applies for.

21. See above.

22. The issue of the impact on the character of Roath Court Road is discussed elsewhere in this report. The application referred to, relating to a property at 40 Princes Street, dates from 2001 and was for an extension to a dwelling – neither the policy context nor the nature of the development are comparable with the application for HMOs now being considered.

23. It is not mandatory for developers to carry out any pre-application consultation for developments such as this. The developer would have been aware of local residents' feelings following the submission of objections to the previous application.

24. The applications have different reference numbers and different descriptions therefore should not be easily confused. There is no evidence to suggest that the applicant deliberately tried to mislead the public. The appeal and the new application could have run concurrently but the applicant chose to withdraw the appeal and apply instead for a smaller development.

25. It is not necessary for the application to include drawings showing the scale of the proposal in comparison with the existing neighbouring housing on Roath Court Road, although it would have been helpful. The lack of such a drawing does not constitute grounds for refusal of the application. The scale of the existing and proposed buildings are evident and context can still be considered.

26. Up to date policy and Supplementary Planning Guidance has been used to assess the current application and is referred to in this report, and the current context has been considered.

27. The development approved under 08/01551/C has commenced and could be fully implemented subject to compliance with the Section 106 obligation attached to the permission. The permission did not expire in 2016 as it was established that works had commenced on 16/09/2016 and therefore, in accordance with Section 56(1) of the Town and Country Planning Act 1990, the permission remains "alive". The application that was made to remove the five-year limit (16/00742/MJR) was withdrawn because it was determined at that time that the development had commenced in accordance with the permission and all pre-commencement

conditions had been discharged prior to the commencement of development. 08/01551/C does not represent a precedent for the current proposals but it must be borne in mind that it is a “fall-back” position for the developer should this application be refused.

28. The Local Planning Authority does not have the power to insist that the development be carbon neutral. Energy efficiency is controlled under the Building Regulations, which requires developments to achieve certain CO2 emissions targets. This is separate to planning permission. The Local Planning Authority can, however, consider other aspects of sustainability such as green infrastructure and sustainable transport, hence the requirement for maximum rather than minimum numbers of car parking spaces for this development and the recommended conditions relating to landscaping, tree protection and cycle storage.

29. The application is not specifically for student accommodation, nor does it relate to a hostel or residential institution. The use applied for is “houses in multiple occupation.” It cannot be assumed that residents would engage in antisocial behaviour, which would in any case be a matter for the police, and it would be unreasonable to refuse planning permission on those grounds. The Planning Committee had no objections to the proposed use as houses in multiple occupation when considering the previous application (18/01669/MJR).

- 8.16 The objections raised by Councillors Peter Wong, Dan De’Ath, Mary McGarry and Sue Lent reiterate the concerns of local residents and are addressed above. With regard to their request for the application to be refused on the grounds that there will be no affordable housing contribution, it should be noted that the previous application triggered the need for a contribution not because of the number of bedrooms or the size of the buildings but because of the dimensions of the application site. Policy H3 of the LDP states that *“The Council will seek 20% affordable housing on Brownfield sites and 30% affordable housing on Greenfield sites in all residential proposals that:*
- i. Contain 5 or more dwellings; or*
  - ii. Sites of or exceeding 0.1 hectares in gross site area; or*
  - iii. Where adjacent and related residential proposals result in combined numbers or site size areas exceeding the above thresholds, the Council will seek affordable housing based on the affordable housing target percentages set out above.”*
- 8.17 The proposal does not contain 5 or more dwellings, it contains only two Houses in Multiple Occupation; the site area is below 0.1 hectares, and there are no adjacent, related proposals that would result in the combined numbers or site size areas exceeding the above thresholds. The Council is therefore unable to seek an affordable housing contribution in this case and the lack of such a contribution cannot reasonably form grounds for the refusal of planning permission.
- 8.18 The objections raised by Jenny Rathbone A.M. and Jo Stevens M.P. also reiterate the concerns of local residents, and are addressed above.

8.19 In conclusion, there would be no grounds for refusal of this application on the basis of the above considerations.

## 9. **OTHER CONSIDERATIONS**

### 9.1 *Crime and Disorder Act 1998*

Section 17(1) of the Crime and Disorder Act 1998 imposes a duty on the Local Authority to exercise its various functions with due regard to the likely effect of the exercise of those functions on, and the need to do all that it reasonably can to prevent, crime and disorder in its area. This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable increase in crime and disorder as a result of the proposed decision.

### 9.2 *Equality Act 2010*

The Equality Act 2010 identifies a number of 'protected characteristics', namely age; disability; gender reassignment; pregnancy and maternity; race; religion or belief; sex; sexual orientation; marriage and civil partnership. The Council's duty under the above Act has been given due consideration in the determination of this application. It is considered that the proposed development does not have any significant implications for, or effect on, persons who share a protected characteristic, over and above any other person.

9.3 *Environment (Wales) Act 2016*The Environment (Wales) Act 2016 imposes a duty on the Local Authority to seek to maintain and enhance biodiversity in the proper exercise of its functions. and in doing so to promote the resilience of ecosystems. It is considered that the proposed development does not have any significant implications for, or effect on, biodiversity.

9.4 *Well-being of Future Generations (Wales) Act 2015*The Well-being of Future Generations (Wales) Act 2015 places a duty on the Welsh Ministers (and other public bodies) to produce well-being objectives and take reasonable steps to meet those objectives in the context of the principle of sustainable development. The duty to improve the economic, social, environmental and cultural well-being of Wales, in accordance with the sustainable development principle, under section 3 of the Well-Being of Future Generations (Wales) Act 2015 (the WBFG Act), has been considered and account has been taken of the ways of working set out at section 5 of the WBFG Act in the determination of this application, and it is considered that this decision is in accordance with the sustainable development principle through its contribution towards one or more of the well-being objectives referred to in section 9 of the WBFG Act.



Site Plan 1:500

# Revised Application



Site Plan 1:1250

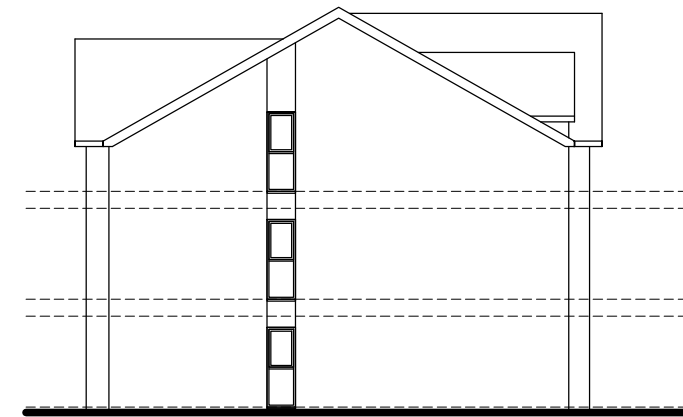
Residential Development at 217-223 Newport Rd WPI Estates	Job No. 2018-035 Dwg No. AL(090)01
Title Location Plan	
Date 05/2018	Scale 1:250/1250 @ A3


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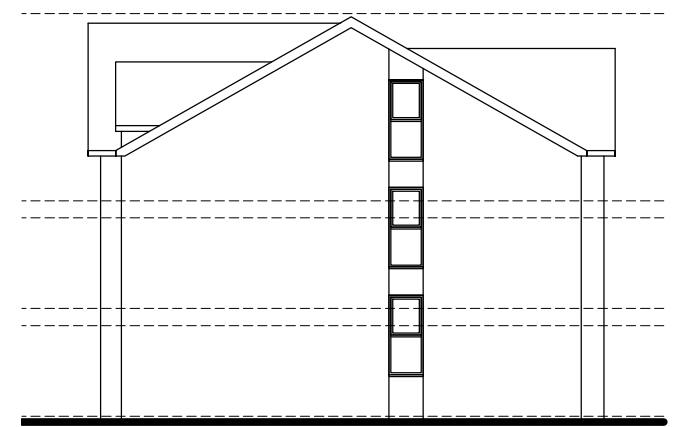
Front Elevation to Roath Court Rd - 1:200



Side Elevation Plot A/B - 1:200



Rear Elevation - 1:200




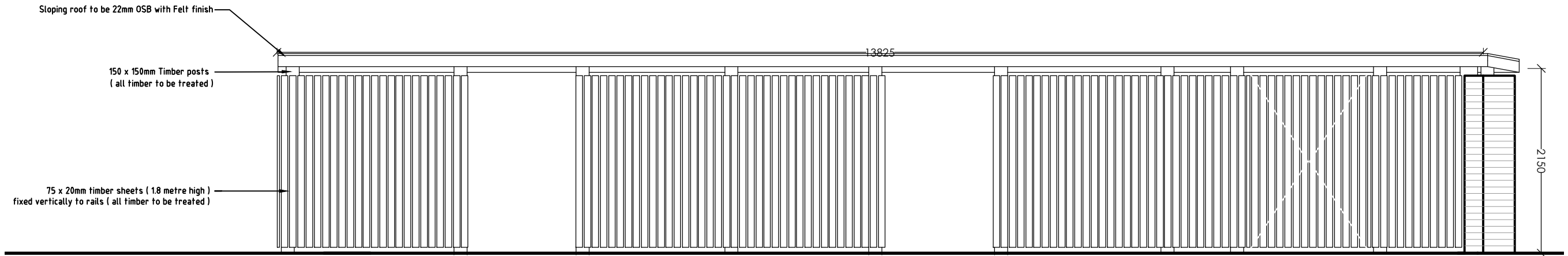
Side Elevation Plot A/B - 1:200



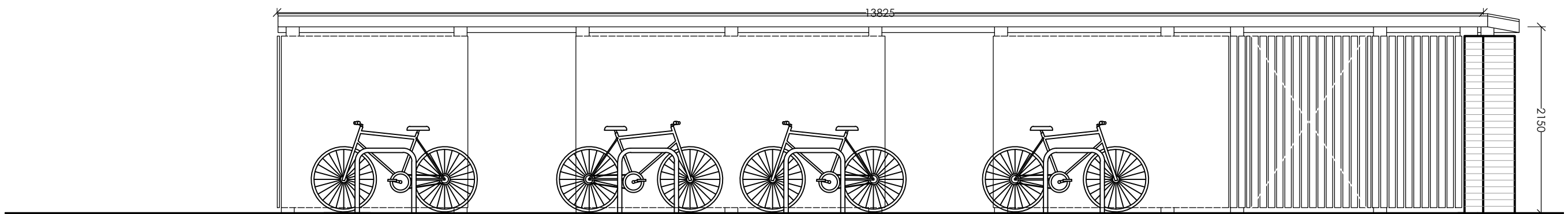
Side Elevation Plot C - 1:200

# Revised Application

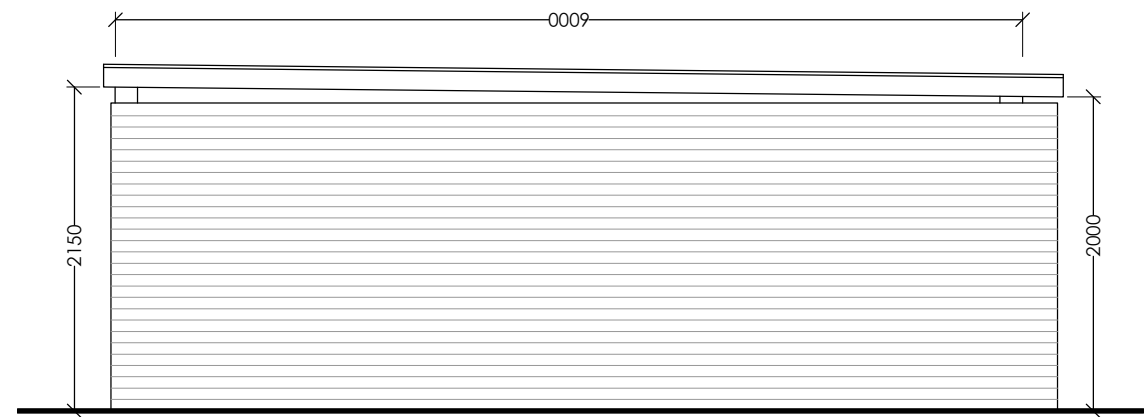
Residential Development at 217-223 Newport Rd WPI Estates	Job No. 2018-035 Dwg No. AL(01)10 Rev.
Title Proposed Elevations	
Date 05/2018	Drawn Scale 1:200 @ A3
	
<small>Unit 1A, Compass Business Park, Pacific Road, Cardiff, CF24 5HL</small>	



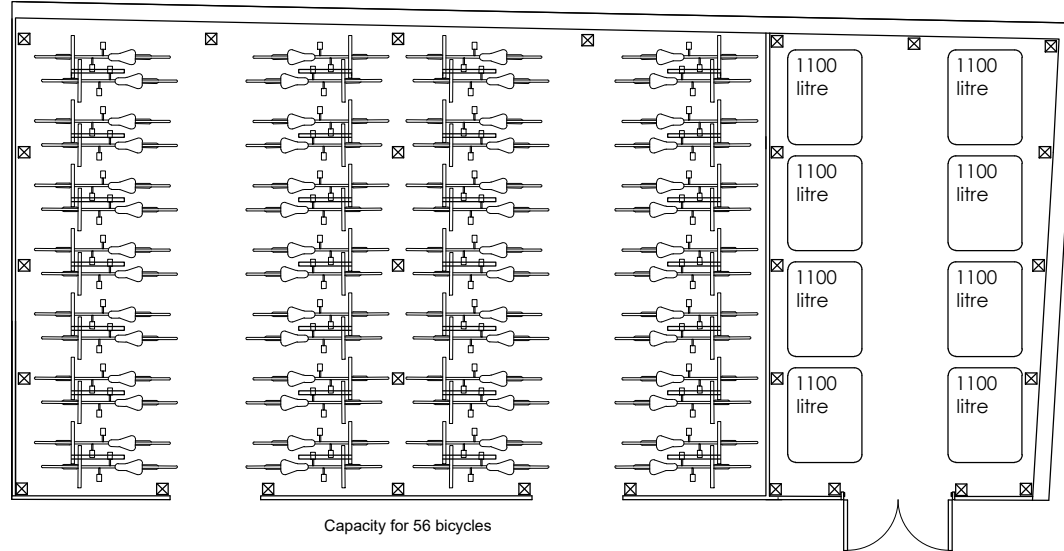
**Front Elevation**  
1:50 @ A3



**Front Elevation (Sectional)**  
1:50 @ A3



**Side Elevation**  
1:50 @ A3

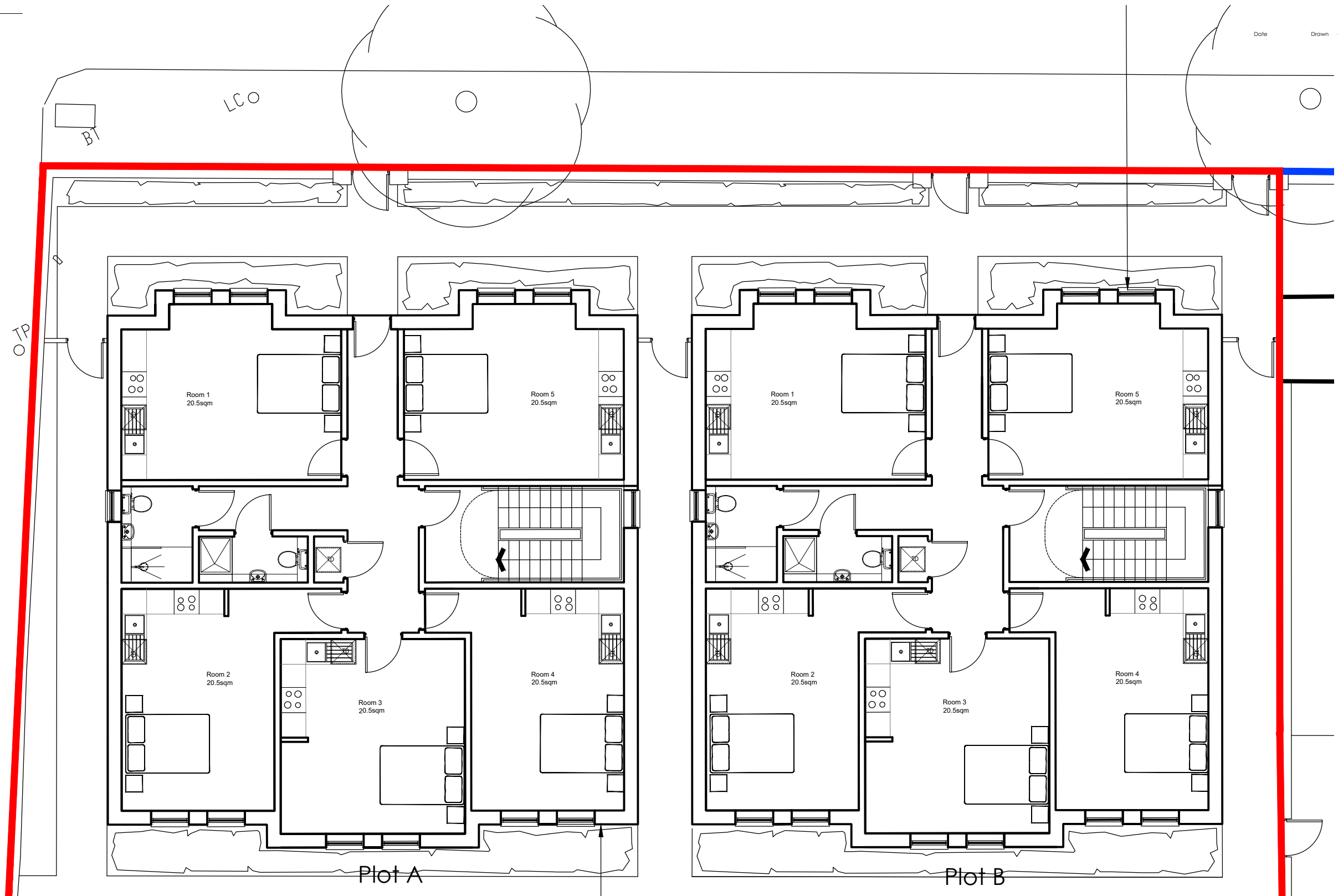


**Floor Plan**  
1:100 @ A3

# Revised Application

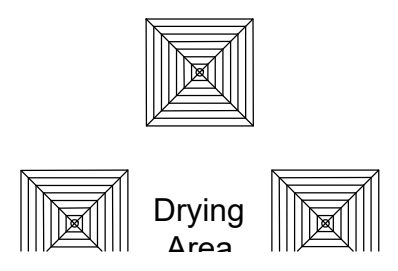
Residential Development at 217-223 Newport Rd WPI Estates		Job No. 2018-035 Rev. AL(02)01
Title Proposed Cycle and Bin Store		
Date 01/2019	Drawn WS	Scale 1:100 & 1:50 @ A3
		Town planners & Urban design
<small>Unit 1A, Compass Business Park, Pacific Road, Cardiff, CF24 6HL</small>		<small>www.c.jarchitects.co.uk tel: 029 20452100</small>





Plot A

Plot B



Plots A & B - Ground Floor Plan 1:100

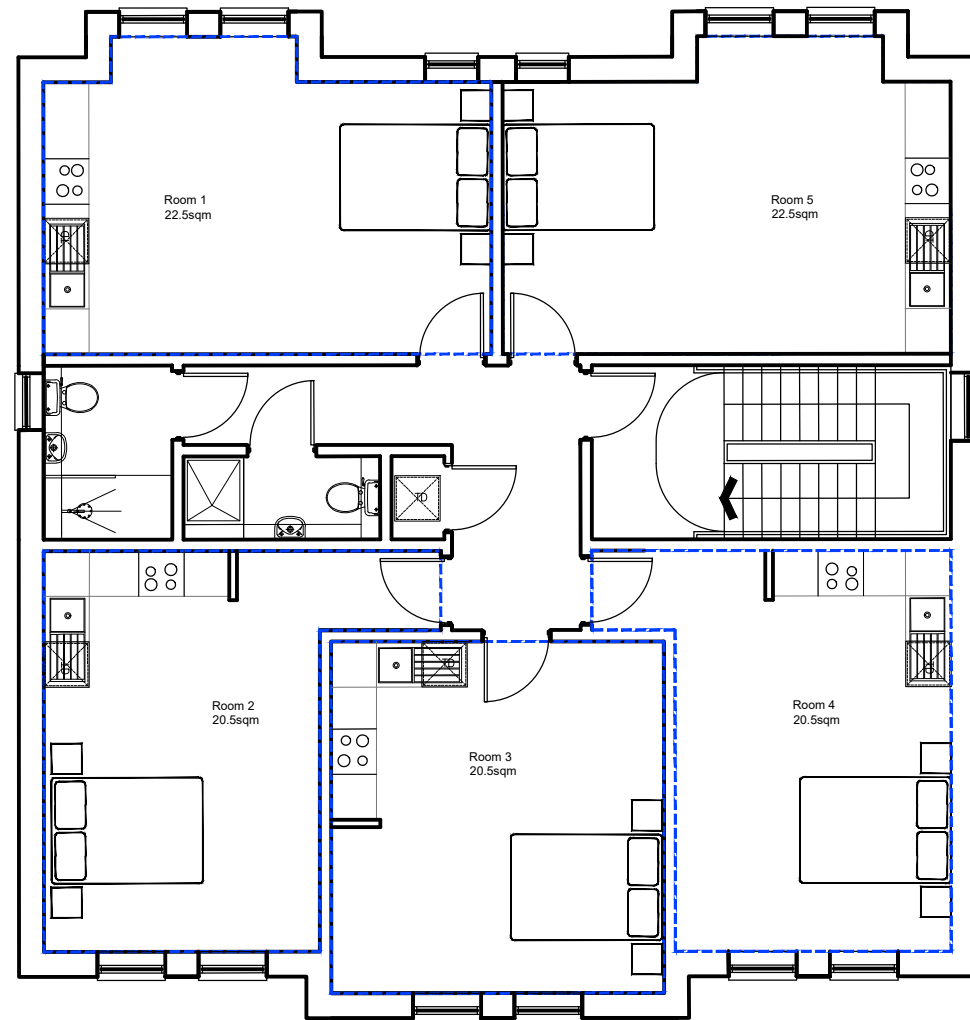
# Revised Application

Residential Development at 217-223 Newport Rd WPI Estates	Job No. 2018-035 Dwg No. AL(01)02
Title Plots A+B Ground Floor Plan	
Date 05/2018	Scale 1:250/1250 @ A3

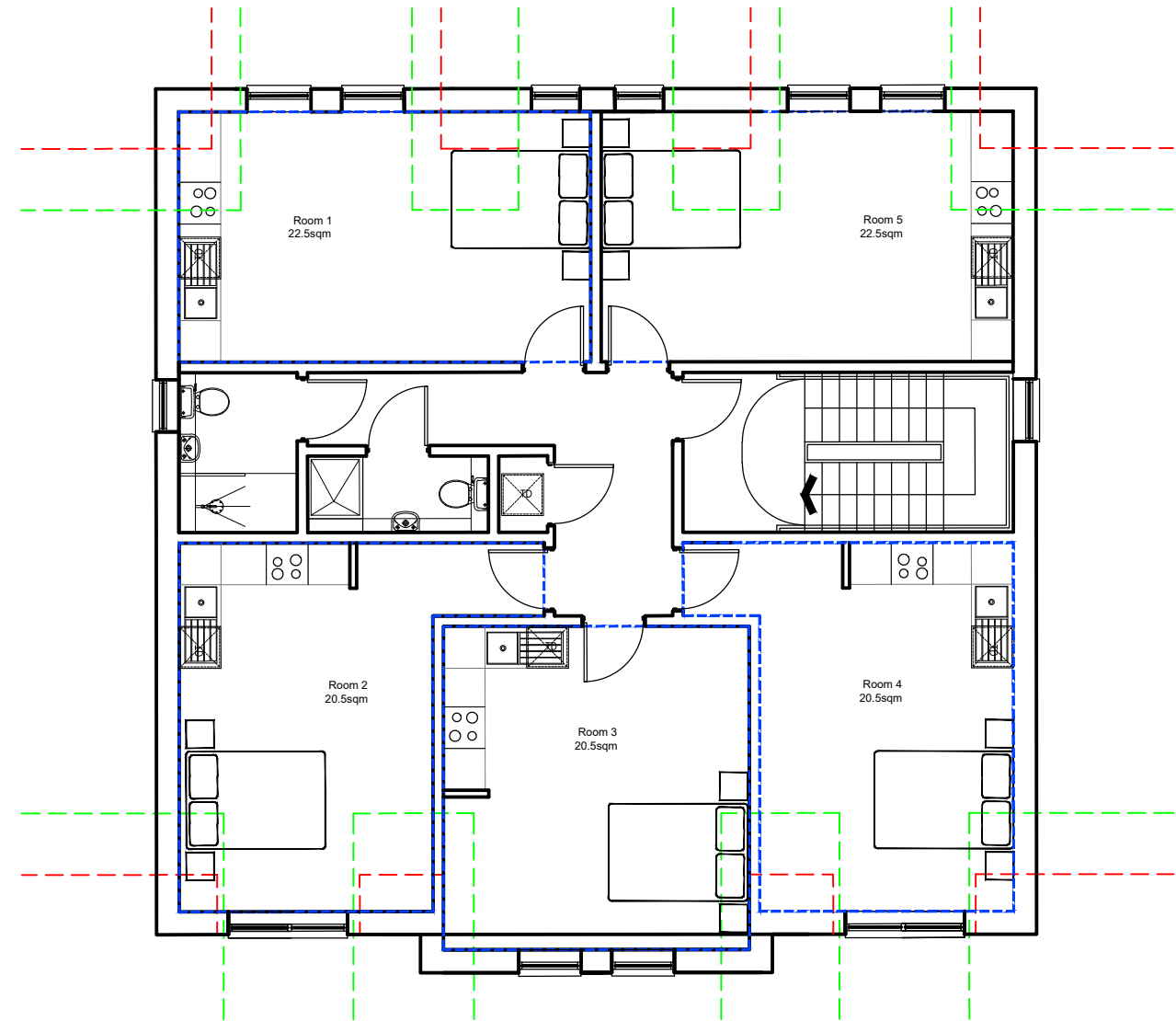
Ret

**C<sup>2</sup> Architects** Town planners  
Environmental & Urban design

Unit 1A, Compass Business Park,  
Pacific Road, Cardiff, CF24 6HL  
www.c2architects.co.uk  
tel: 029 20452100



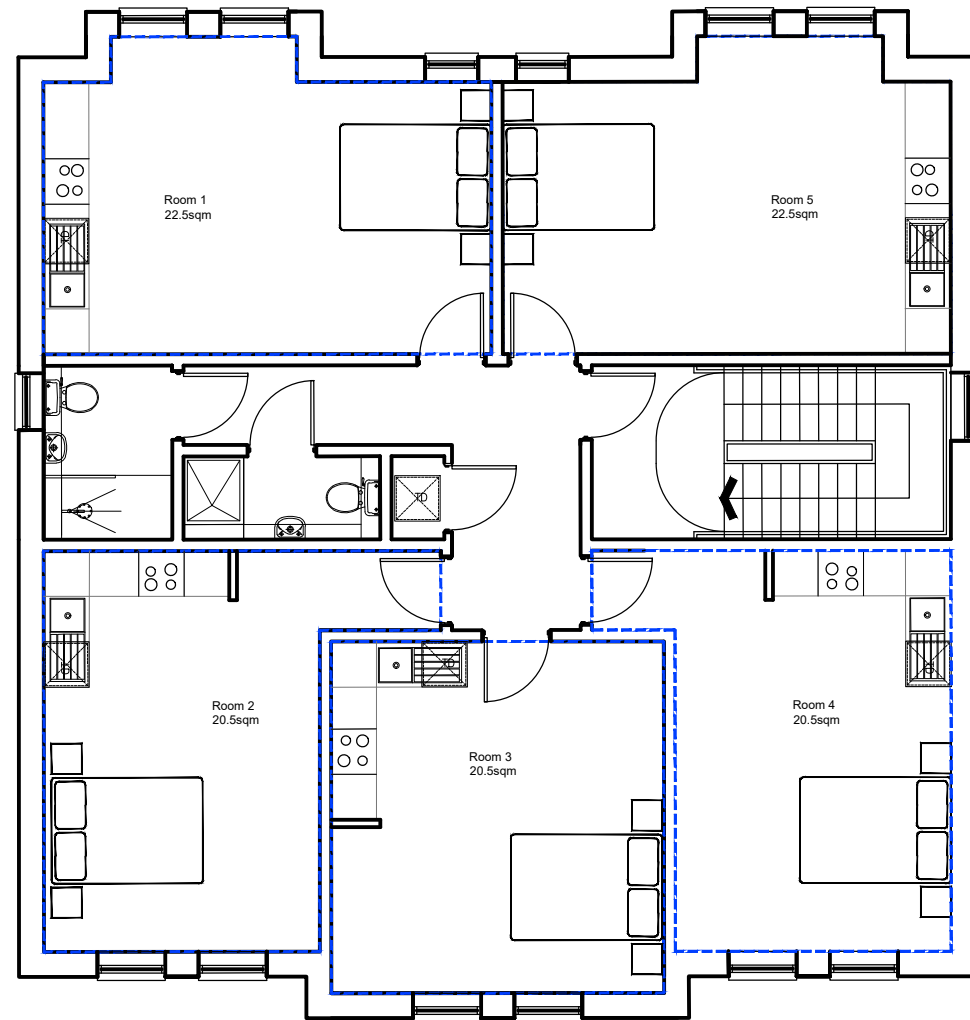
1st Floor Plan



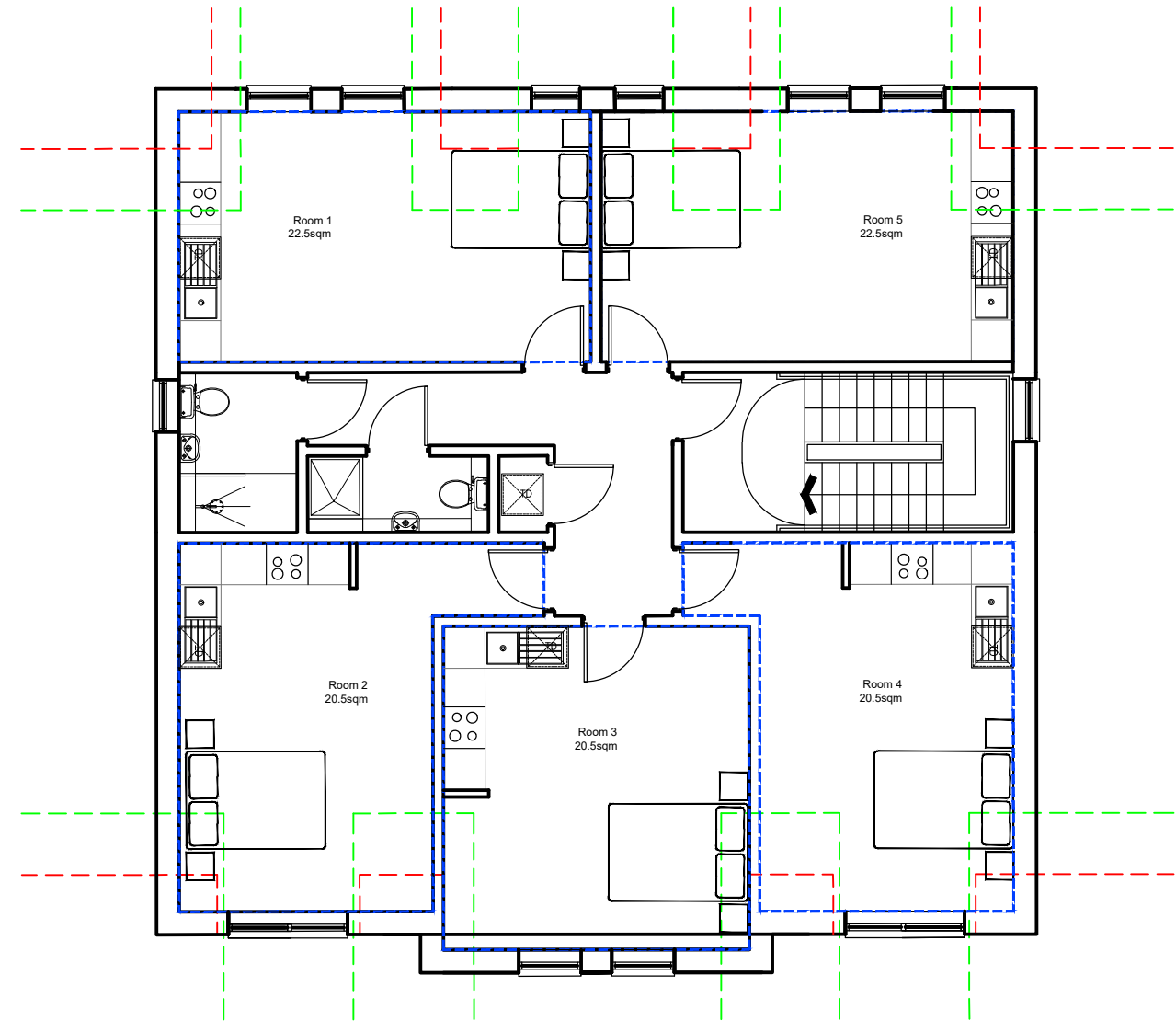
2nd Floor Plan

Plot A & B Upper Floor Plans  
**Revised Application**

Residential Development at 217-223 Newport Rd WPI Estates		Job No. 2018-035 Dwg No. AL(01)03 Rev.
Title Plots A+B Upper Floor Plans		
Date 05/2018	Drawn	Scale 1:100 @ A3
 <b>C<sup>2</sup> Architects</b>		Town planners Environmental & Urban design
<small>Unit 1A, Compass Business Park, Pacific Road, Cardiff, CF24 6HL</small>		<small>www.c2architects.co.uk tel: 029 20452100</small>




1st Floor Plan



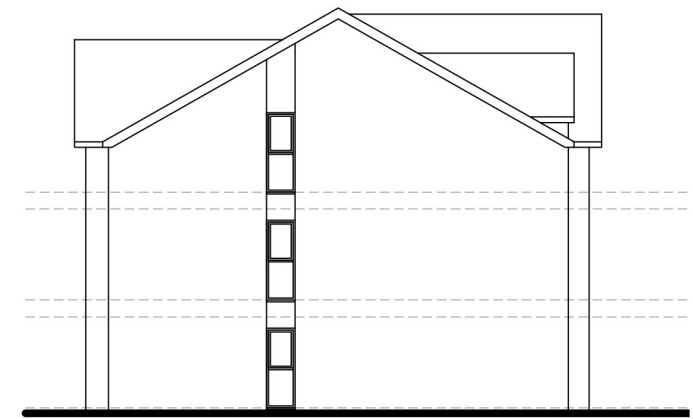
2nd Floor Plan

Plot A & B Upper Floor Plans  
**Revised Application**

Residential Development at 217-223 Newport Rd WPI Estates		Job No. 2018-035	Rev. AL(01)03
Title Plots A+B Upper Floor Plans			
Date 05/2018	Drawn	Scale 1:100 @ A3	
 <b>C<sup>2</sup> Architects</b>		<b>Town planners          Environmental &amp; Urban design</b>	
<small>Unit 1A, Compass Business Park, Pacific Road, Cardiff, CF24 6HL</small>		<small>www.c2architects.co.uk tel: 029 20452100</small>	



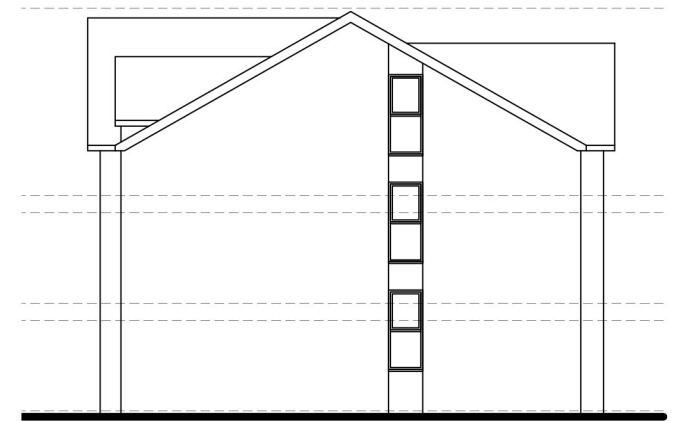
Front Elevation to Roath Court Rd - 1:200



Side Elevation Plot A/B - 1:200



Rear Elevation - 1:200



Side Elevation Plot A/B - 1:200



Side Elevation Plot C - 1:200

Residential Development at 217-223 Newport Rd WPI Estates	Job No. 2018-035 Dwg No. AL(01)10 Rev. A
Title Proposed Elevations	
Date 05/2018	Scale 1:200 @ A3

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 tel: 029 20452100

8.8m



**AMENDED PLAN**  
 CARDIFF COUNTY COUNCIL  
 STRATEGIC PLANNING  
 & ENVIRONMENT  
 08/11/08  
 DATE 12-11-08

Residential Development 217-233 Newport Rd. Richard Waters	Job No. 07-092 Dwg No. AL(0)01 Rev. B
Title Site Plan	Scale 1:200 @ A1
Date /04/08	Drawn ***

Architects  
Environmental & Urban design

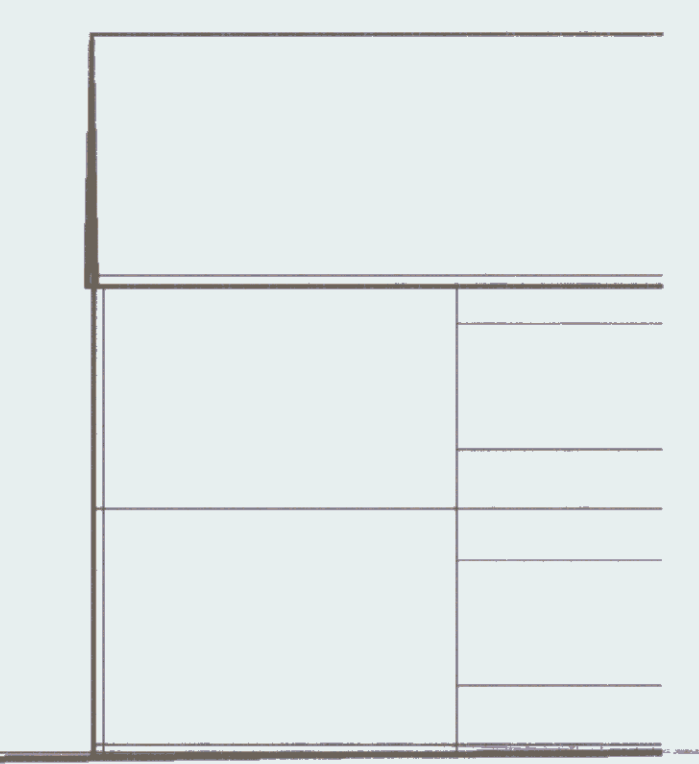
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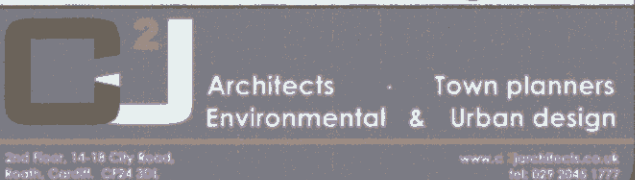
Elevation A



Elevation B



AMENDED PLAN  
 CARDIFF COUNTY COUNCIL  
 STRATEGIC PLANNING  
 & ENVIRONMENT  
 08/1551c  
 DATE 12-11-08

Residential Development 217 -233 Newport Rd. Richard Walters	Job No. 07-092	Rev. AL(0)30 B
Title Elevations A and B		
Date 24/04/08	Drawn ***	Scale 1:100 @ A1
		

**PETITION  
MP, AM & LOCAL MEMBER OBJECTION**

COMMITTEE DATE: 18/12/2019

APPLICATION No. **19/01802/DCH** APPLICATION DATE: 20/06/2019

ED: **LLANDAFF**

APP: TYPE: Householder Planning Permission

APPLICANT: Mr & Mrs Davies

LOCATION: 13 HEOL-Y-PAVIN, LLANDAFF, CARDIFF, CF5 2EG

PROPOSAL: PROPOSED SINGLE STOREY EXTENSION WITH  
MODIFICATIONS TO EXISTING

**RECOMMENDATION 1** : That planning permission be **GRANTED** subject to the following conditions :

1. C01 Statutory Time Limit
2. The development shall be carried out in accordance with the following approved plans: Drawing Nos.
  - 462 – P06 Rev C
  - 462 – P07 Rev B
  - 462 – P08 Rev B
  - 462 – P09 Rev D
  - 462 – P10 Rev E
  - 462 – P11 Rev C
  - 462 – P12 Rev C
  - 462 – P13 Rev C

Reason: To ensure satisfactory completion of the development and for the avoidance of doubt in line with the aims of Planning Policy Wales to promote an efficient planning system.

3. The proposal hereby approved shall accord with a scheme of details (1:10 scale part face and section drawings of typical door/window profile, mullions, transoms, glazing bar detail, opening method and finish) which shall first have been submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure that the finished appearance of the building is in keeping with the character of the conservation area.

4. Unless otherwise agreed in writing by the Local Planning Authority samples of all new/reclaimed materials for external finishes shall be submitted to and approved in writing by the Local Planning Authority. The development shall then be carried out in accordance with the

approved details and shall thereafter be maintained.

Reason: To ensure that the finished appearance of the development is in keeping with the character of the conservation area.

5. The extension hereby approved shall not be occupied at any time other than for the purposes in conjunction with and ancillary to the dwelling at 13 Heol-y-Pavin, Llandaff, Cardiff.  
Reason: The creation of a separate dwelling unit would be unacceptable in this situation.
6. The existing stone used in the construction of the west boundary wall shall be re-used as far as practicable in the event that the existing wall needs re-building, any new stone shall match the existing stone.  
Reason: To ensure that the finished appearance of the development is in keeping with the character of the conservation area.
7. The use of the flat roof of the extension hereby approved shall not be used for any type of external amenity space or recreational space whatsoever.  
Reason: To ensure that the privacy and amenity of the occupiers of the adjoining properties are protected.

**RECOMMENDATION 2:** That the applicant be advised that no works should take place on or over the neighbour's land without the neighbour's express consent and this planning approval gives no such rights to undertake works on land outside the applicant's ownership.

**RECOMMENDATION 3:** The proposed development site is crossed by a public sewer with the approximate position being marked on the attached Statutory Public Sewer Record. Under the Water Industry Act 1991, Welsh Water has rights of access to its apparatus at all times. No operational development shall take place within 3 meters either side of the centre line of the public sewer.

The applicant may need to apply to Dwr Cymru / Welsh Water for any connection to the public sewer under S106 of the Water industry Act 1991. If the connection to the public sewer network is either via a lateral drain (i.e. a drain which extends beyond the connecting property boundary) or via a new sewer (i.e. serves more than one property), it is now a mandatory requirement to first enter into a Section 104 Adoption Agreement (Water Industry Act 1991). The design of the sewers and lateral drains must also conform to the Welsh Ministers Standards for Gravity Foul Sewers and Lateral Drains, and conform with the publication "Sewers for Adoption"- 7th Edition. Further information can be obtained via the Developer Services pages of [www.dwrcymru.com](http://www.dwrcymru.com)

The applicant is also advised that some public sewers and lateral drains may not be recorded on our maps of public sewers because they were originally privately owned and were transferred into public ownership by nature of the Water Industry (Schemes for Adoption of Private Sewers) Regulations 2011. Under the Water Industry Act 1991 Dwr Cymru Welsh Water has rights of



access to its apparatus at all times.

The applicant is advised to contact Dwr Cymru Welsh Water's Developer Services contact centre on 0800 917 2652, quoting reference number PLA0043414 or via email at [developer.services@dwrwymru.com](mailto:developer.services@dwrwymru.com)

## 1. **DESCRIPTION OF PROPOSED DEVELOPMENT**

- 1.1 Planning permission is sought for a single storey extension to the side of an end of terrace property known as 13 Heol-y-Pavin. It should be noted that the property is unusual in that it, presents its side elevation directly to the public highway of Heol-y-Pavin and its principle (front) elevation to an adjacent side road known as Penedre. The private amenity space associated with the host property exists primarily to the front and side elevations. The proposed extension would be positioned to the side elevation occupying the majority of the side garden extending from the host dwelling to the common boundary/side elevation shared with No. 1 Penedre. The extension would also, abut the common boundary shared with No. 11 Heol-y-Pavin at its rear elevation. Whilst the proposed extension predominately incorporates a flat roof, due to site constraints, the roof is at varying heights. A small lean-to extension and an outbuilding would be demolished to make way for the proposed extension.

## 2. **DESCRIPTION OF SITE**

- 2.1 The application site No. 13 Heol-y-Pavin, also known as 'Rickyard Cottage' is a two storey cottage with stone walls and brick quoins and a hipped slate roof. An existing flat roof, rendered two storey extension has been added to the south gable end. A further single storey lean-to extension has been further added to the end of the two storey flat roof extension.
- 2.2 The application site is situated within the Llandaff Conservation Area, which is covered by Article 4 Directions removing some permitted development rights. The site is accessed from Heol-y-Pavin. Immediately adjoining the application site to the east is No.11 Heol-y-Pavin and to the south, No. 1 Penedre and to the west exists Penedre, a road which allows access to the properties situated along Penedre and access to the rear of properties fronting Cardiff Road.

## 3. **SITE HISTORY**

- 3.1 19/00475/DCH – Proposed single storey extension with reconfiguration of front elevation and replacement windows. Withdrawn May 2019.

## 4. **POLICY FRAMEWORK**

- 4.1 Relevant National Planning Guidance:

Planning (Listed Buildings and Conservation Areas) Act 1990  
Planning Policy Wales (Edition 10, 2018)  
TAN 12: Design (2016)  
TAN 24: The Historic Environment (2017)

4.2 Relevant Cardiff Local Development Plan Policies:

Policy KP 5: Good Quality and Sustainable Design  
Policy KP 17: Built Heritage  
Policy EN 9 Conservation of the Historic Environment

4.3 Relevant Supplementary Planning Guidance:

Residential Extensions and Alterations (2017)  
Llandaff Conservation Area Appraisal (2006)

5. **EXTERNAL CONSULTEE RESPONSES**

5.1 Glamorgan Gwent Archaeological Trust has been consulted and confirms that historically there may have been activity on the site, but it is unlikely given the scale of the proposal, that any archaeological deposits would be encountered during the proposed development.

5.2 Welsh Water has been consulted and raise no objections.

6. **REPRESENTATIONS**

6.1 The application was advertised by way of a site notice and press notice.

6.2 Members should be aware that during the processing of this application, amendments have been received to the proposal. As a result comments have been received from neighbouring occupiers and other interested parties and individuals on more than one occasion. All comments received during the processing of the application have been summarised in the report.

6.3 Following receipt of the amended scheme all previously consulted parties were re-consulted and these comments and objections are reported at the end of this section. It should also be noted that all comments received have been summarised.

**Representations Initially Received**

6.4 Rt Hon. Mark Drakeford, Assembly Member for Cardiff West and First Minister of Wales has objected to the application. A summary of the grounds of objection relating to this planning application are as follows:

The proposed extension threatens to significantly damage the historic character of the conservation area. There is inevitable danger of physical damage to surrounding historic structures. This is pronounced at Heol-y-Pavin, which is a small and narrow street, making access difficult for construction vehicles and equipment. Residential streets near Cathedral Green, particularly Heol-y-Pavin, are heavily developed and it is one of the most densely populated areas in Llandaff. Allowing building works will not help alleviate all the usual problems associated with this.

There is also, no clear public or economic interest to this application which would help to mitigate the problems set out.

- 6.5 Kevin Brennan, Cardiff West MP, has written a letter of concern relating to the application. A summary of the grounds of concern relating to this planning application are as follows:

A resident contacted me and raised concerns regarding the application and I understand other residents have also raised objections.

My constituent raised concerns relating to the location of the extension in a conservation area with a particular historic character that threatens to damage this character.

Heol-y-Pavin is a small and narrow street which makes access difficult for construction vehicles and equipment.

The extension may cause physical damage to surrounding historic structures. The surrounding area and Heol-y-Pavin are already over developed and these are the most densely populated areas in Llandaff.

- 6.6 Local members have been consulted and an objection has been received from Councillor P Hill-John, these comments have been summarised as follows:

As Ward Councillor, I would like to endorse the representations made by both the Conservation Group and the Llandaff Society.

I would also like to make special reference to the height of the proposed section along the neighbouring wall of number 11 Heol-y-Pavin, and ask that clarification is sought given the drawings / reports are insufficient in this regard.

- 6.7 A valid petition of over 50 signatures has been received objecting to the application (submitted by the occupier of No. 11 Heol-y-Pavin).

- 6.8 Neighbours have been consulted and 8 letters of objection have been received from 5 Heol-y-Pavin (i), 11 Heol-y-Pavin (ii), 1 Penedre (iii), 3 Penedre (iv), 4 Penedre (v), 19 The Cathedral Green (vi), No. 27 (ii) and 33 Cardiff Road (vii).

(i) 5 Heol-y-Pavin

People have the right to improve their properties. This should be done with due reasonable concern for the local environment and the impact of such changes on their adjacent neighbours. Materials to be used continue to be out of keeping with the character of the local area and the scale and size of the project has not been scaled down. This is important due to the impact the height of the extension would have on adjacent houses particularly number 11 Heol-y-Pavin and number 1 Penedre.

The Council planning department should reject the proposals for the reasons stated and for parts of the application that are not clear to what is intended.

The owners of number 13 should make an effort to reach out and work constructively with the local community and Cardiff planning to work on a solution that would satisfy their needs to improve their property, whilst in turn having due regard to the fair and reasonable aspects of the objections by members of the local community.

- (ii) 11 Heol-y-Pavin and 27 Cardiff Road (raise similar objections and as a result have been summarised in this section).

Planning policy and guidance is stated by the objectors.

**Failure to consult neighbours.**

The applicant failed to consult before this application was lodged.

**Defective/ inadequate application.**

Application fails to pay due regard to it being in the heart of Llandaff City.

The application fails to address:

How the development would preserve or enhance the conservation area.

How the development responds to:

The local character and context of the built and landscape setting.

How the design:

Responds to the site, context, brief, vision, relevant policy and objectives of good design

How the development is:

Limited in depth and width to avoid an overbearing appearance.

Height indications are confusing/inconsistent / inadequate.

Plans show differential floor levels and are confusing.

No indication of floor slab level. There appears to be no objective indication of the exact level of the concrete floor slab. So height calculations from it are effectively meaningless.

No material reduction in height /inconsistent indications of height.

Clarification of height needed.

Inadequate Design and Access statement.

Inadequate Heritage Impact statement.

Geraint John Planning Consultants Letter only relates to height and not other planning considerations.

Current height indications would result in a building that was too high.

**Height and scale**

It appears very high.

Visibility from the road/public viewpoint/closing out distant views. The development is visible from the road and will encroach upon spaces between buildings, closing out distant views.

In addition the following views/ vistas will be obscured/ degraded:

To the stone wall of No 1 Penedre . There will now be a discordant, dominant and ugly metal sloping roof facing Penedre.

Views up and down Penedre.

From the back gardens of houses in Heol-y-Pavin, particularly No 11 Heol-y-Pavin.

From Heol-y-Pavin.

Whilst loss of or interference with a view, vista is not normally a basis for refusal planning applications, in the case of conservation areas that does not apply, and should be a material consideration.

Dominate neighbouring properties/ Loss of amenity

The development would dominate the garden of No 11 Heol-y-Pavin and the front and entrance to No 1 Penedre.

Whilst the height appears to have been lowered immediately adjacent to number 11 Heol-y-Pavin the height towards Penedre would still dominate. Likewise, whilst the frontage to Penedre has been replaced by a sloping roof, its height (and discordant metal) would still dominate.

This extension will dominate enclosed back garden at No 11 Heol-y-Pavin.

11 Heol-y-Pavin had privacy and is not overlooked and had views at the back of surrounding trees and rooftops, a unique feature of this property near the heart of the city centre.

Will significantly dominate and overlook No. 11 Heol-y-Pavin's back garden which is treasured by owners.

### **Infill**

It covers a substantial area of garden.

The area covered by the proposed extension is substantial; it extends beyond the existing garage towards Heol-y-Pavin.

It is infill development.

It fails to satisfy the planning guidelines.

The design and access statement does not address them.

### **Flat roof**

Due to its height, the flat roof will form a discordant element in an area which almost exclusively consists of steep pitched roofs.

It has no affinity to or connection or unity with the original cottage building or any of the (visible) buildings around it.

It is incongruent to the conservation area.

### **Materials**

Against guidance.

The proposal would bring a discordant mish- mash of plastic patio doors and windows (all of differing size), metal sheeting, pvc sheeting, metal roofing, timber cladding into the heart of the conservation area.

Not against high - quality, aesthetically pleasing modern design, but this complacent, cheap and incongruent mess is definitely not that.

Stone and stained timber materials need to be clarified.

The stained timber is not sympathetic with the location, and no justification given. There is no equivalent timber on the original building. No description of pattern, material or colour.

It's the sort of timber cladding seen on undistinguished flats constructed some years ago in Cardiff Bay before that particular design motif was dropped by volume builders.

This would be completely inappropriate on Rickyard Cottage; it would be entirely discordant with the truly delightful, and characteristic local stone on the original house, and its context generally.

Slate grey coloured fascia panels and pressed metal coping, concern is raised over these materials to be used.

Fully glazed patio doors:

No reference at all in the application to these tediously predictable "any place, anywhere, any town" doors. They would be totally out of character with a small Victorian cottage, built in characteristic stone, at the heart of a conservation area. They self - evidently do not preserve the conservation area and could not possibly enhance it. It is a lazy, complacent, inappropriate design element.

Fencing/parking lot:

At present, the vista into the forecourt of Rickyard Cottage from Heol-y-Pavin is a pretty one. It is one of the many Llandaff conservation area "vistas", and much valued locally.

The Application proposes it be replaced by a fenced parking lot. It would not be a harmonious outcome.

## **Design**

The design is undistinguished, predictable, complacent. The overall effect would be a mishmash of discordant /alien, materials and window and door styles do not comply with planning policy and the design and access statement do not address policy guidelines.

Fails to harmonise with the original house.

Fails to respond to and respect the site.

Fails to respond to local character as stated in planning policy, spg and guidelines.

### (iii) 1 Penedre

Slight changes to plans, but concerned about impact on house and surrounding area.

Entrance to my house is dark and will be built higher than the current garage at no. 13 and will block light which will have huge impact on my natural light to home.

Design does not mirror other homes in conservation area and design will not add to the aesthetics of the area.

Space will be overcrowded, especially street facing.

Most concern is over proximity of the new build, which is large, to my end wall and the impact on the foundations of my house.

### (iv) 3 Penedre

Comments submitted for application no: 19/00475/DCH still stand.

Application at odds with nature of conservation area in which vastly extended cottage exists.

This revised application is only altered in minor respect, but with new proposal to place sheet metal roof adjacent to Penedre Lane, alongside other undesirable features previously presented.

Planning committee should heed the overwhelming opposition to the proposal.

(v) 4 Penedre

Comments submitted for application no: 19/00475/DCH remain the same.

Application fails to satisfy requirements of policy documents both nationally and locally regarding extensions and alterations in the Llandaff Conservation Area.

It seeks to impose an un-aesthetic concrete/metal/plastic assault on the eyes, out of step with Radyr stonewalls of neighbouring properties.

Application should be refused.

(vi) 19 The Cathedral Green

Proposal is out of character with local architecture.

The corridor of the proposed extension suggests the new extension being used as a separate house.

The proposed elevation will adversely affect light reaching adjacent gardens and give a crowded feel to attractive corner of Llandaff.

(vii) 33 Cardiff Road

Object as we did the previous application, but reflecting the minor changes.

Out of character with Llandaff Conservation Area.

Damage appearance of adjacent lane, particularly the metal roof would be out of character with local materials in Penedre.

Spoil the character and appearance of Rickyard Cottage unbalancing the form of the original building by the proposed large extension.

Extension should not be necessary for beneficial use of historic cottage. Any expenditure should be used to improve the appearance of the existing two storey extension.

Applicant has not consulted neighbours, so reduced chance of a satisfactory agreement being reached, which passes responsibility for local arbitration that is an unreasonable burden on the Planning Department, which should only be concerned with wider and long-term considerations.

- 6.9 The Llandaff Conservation Group comments that the Group refers officials to all objections to this application and whether concerns about the impact of proposed changes will still adversely affect the conservation area. Whilst recognising the householders should have reasonable opportunity to adjust the property to meet living requirements, the Group wishes to draw attention to the fact that this location and immediate surroundings has conserved a particular rural feel and unique identity to it, which currently retains the historical atmosphere and character of the Rickyard that gives it its name. The proposal for a fence/gate outside the cottage will disrupt the symmetrical facade of the cottage and obscure the view into the yard, it will therefore have an adverse

effect on the historical quality of the conservation area.

6.10 The Llandaff Society objects to the proposal. A summary of the grounds of objection relating to this planning application are as follows:

- (i) Whilst the revised scheme goes some way to meeting previous strong objections, particularly by retaining the central porch, they still do not result in satisfactory resolution of the key design issues - how to amend this property without destroying its integrity, and the integrity of its setting.
- (ii) The slightly reduced impact of the re-design of the extension on no: 11 Heol-y-Pavin is appreciated, however, problems remain in terms of the proposal's impact on our precious Conservation Area. The 2 key problems are;
  - (a) the grossly inappropriate slab-like single storey extension and the alien materials proposed, which would ruin views of the wonderfully timeless stone wall of the end house in Penedre, to other houses in the vicinity, and
  - (b) the yard gates which - because they are set back from the road - would cut across the middle of the front façade of the property.

The proposals would neither conserve nor enhance the Conservation Area and should be refused.

The Llandaff Society urges the Council to refuse this revised proposal.

The Llandaff Society also objects to the proposal as it is clearly contrary to Local Development plan policies KP17 and EN9. The form of the proposal is incongruous, with intrusive lines and overpowering scale and of inappropriate materials and with total disregard for context. The amended plans do not dispel concerns that irreparable harm will be caused to this intimate and distinctive part of the conservation area. Allowing it would ruin the area and undermine policy and would not protect or enhance the conservation area.

6.11 The agent for the scheme has submitted a letter supporting the proposals. A summary of the letter is as follows:

Planning permission was sought for a single storey extension under application no. 19/00475/DCH, but was subsequently withdrawn on the advice of the Local Planning Authority who expressed concerns over impact of the development on neighbouring properties.

Since this time the applicant has undertaken a detailed topographical survey of the site, to determine the ground levels and height of boundary treatment along shared boundaries.

The revised submission provides sections showing the relationship of the proposed development to the boundary lines of neighbouring properties. It demonstrates that the proposed development would not have 'undue effect' on the amenity of neighbouring occupiers, given the extension would not significantly rise above the existing boundary treatment shared with neighbouring properties and would not result in any unacceptable overshadowing. Therefore, the proposal has addressed the concerns of the Council and should be supported by the Local Planning Authority and planning permission ought to be granted.



## **Representations Relating to the Current Amended Scheme**

- 6.12 Local members were re-consulted and a further objection has been received from Councillor P Hill-John who suggests that the amended scheme should be refused for the following summarised reasons:  
It will have a considerable impact on the integrity of the setting within the Conservation Area.

The proposal would obscure the side wall of No. 1 Penedre, which contravenes the recommendations set out in the Conservation Area Appraisal Review. All should be aware of the reasons for the Review and what its purpose is. Pages 49, 53 and 56 within the Review document raise matters as follows:

The alterations and extensions will affect and overlook neighbouring properties. The extension be visible from the road and encroach upon spaces between buildings.

The extensions will not be in character as timber cladding is not within the vicinity.

The Review should be referenced, particularly with regard to the area, additional controls within Llandaff, painting and contravention of the Article 4.

Reference should be made to the Planning (Listed Buildings and Conservation Areas) Act 1990, which advises that LPA's should formulate and publish proposals for the preservation and enhancement of any parts of their area, and asks why the LPA does not uphold this.

- 6.13 Further to consultations on amended plans and additional information received, an additional 4 objections have been received from the following neighbours, 11 Heol-y-Pavin (a), 1 Penedre (b), 3 Penedre (c), 27 Cardiff Road (d).  
A summary of the grounds of objection relating to this planning application are as follows:

(a) 11 Heol-y-Pavin

The modified plans, of the proposed new building (in reality a new house and not an extension) do not protect or enhance the conservation area.

The plans show the building will be to a height in line with the second storey window sill of the old cottage, which means it is approximately 3.14 metres high. The building will stick out like a sore thumb in the conservation area and contravenes planning laws put in place to protect conservation areas.

The Conservation area Appraisal review should be referred to and reminds us what needs to happen to ensure what should and should not be built in the conservation area of Llandaff.

Cardiff's Local Development Plan states: development relating to any heritage assets will only be permitted where it can be demonstrated that it preserves or enhances the assets architectural quality, historical and cultural significance,

character, integrity and/or character.

The plans proposed a flat roof, painted timber cladding, pressed metal sheeting, pvc plastic windows. Does the Planning Department think these features preserve and enhance the integrity and character of the conservation area?

The building will affect and overlook neighbouring properties. The high square grey metal/timber clad flat roofed box will be highly visible to both Penedre and Heol-y-Pavin.

The building will encroach on spaces between buildings. This new house will be against the side wall of 1 Penedre and against the side wall of the outhouse of 11 Heol-y-Pavin, which is much lower (2.3m). It is infill of open space, which is limited in the conservation area.

The building will not be in character with the surrounding area. The Planning Committee should come to Llandaff and see if any other high flat roofed, timber clad buildings with plastic windows can be found in the conservation area.

The Authority needs to uphold its own principles.

They are required by law to 'formulate and publish proposals for the preservation and enhancement of any part of the area.

Planners should refer to the Listed Buildings and Conservation Areas Act 1990. There are no heritage gains and a recommendation to allow this building flies in the heart of the conservation area and flies in the face of all and any legislation I have seen and read around buildings in protected conservation areas.

This application must be refused.

(b) 1 Penedre

Concern is raised over the impact of the new build on my property. Previously raised concerns are:

Potential impact on the foundations of my house due to proximity of the build and the age of my house.

Loss of light to front of my property.

Loss of character to the area and my gable end wall with characteristic stone work.

Concern is raised at proximity of the new build to my end wall. There is damp affecting this wall and concern about how to conduct repair work and get access to my wall with an extension that is so close.

For these reasons I object.

(c) 3 Penedre

The latest set of plans substitute a sheet metal sloping edge on the extension for three varying height flat roofs in a row, what a mess.

The residents in the vicinity have paid well over the odds for our properties on the basis of loving the historic context of our homes and we do all we can to preserve this unique part of heritage.

The response to this proposal in Llandaff conservation area should be an automatic and emphatic no.

If the level of opposition, including all interested parties, the local MP and the First Minister of Wales does not hold sufficient weight, what is the point of having a consultation process.

(d) 27 Cardiff Road

Supplemental objections (with visuals) – after two applications (one withdrawn), errors in plans, numerous amendments and numerous queries unanswered and unaddressed (e.g. floor slab height), the building is still a bad and obtrusive proposal which neither preserves or enhances the conservation area.

There has been no attempt by the applicant to engage local residents, despite the fact the view from Heol-y-Pavin will be ruined, when Council guidance recommends this.

The proposal is highly visible which is against planning guidance.

An inappropriate flat roof, will not enhance the conservation area and does not comply with planning policy and guidance for conservation areas.

The height is out of proportion with the main house and dominating. The old plans and new plans remain unaltered in height.

The materials do not harmonise with the surroundings and do not comply with planning policy, it will have sheet metal coping, upvc doors and windows and even more timber cladding.

How can the Council ever grant this application without breaching that planning policy and guidance.

6.14 The Llandaff Conservation Group further objects as summarised: The current proposals do not represent an enhancement to the Conservation Area and do not outweigh the objections and concerns previously raised by the Group. Officials are requested to pay attention to the application and ensure the Conservation Area Guidance and principles are met.

6.15 The Llandaff Society and the Planning and Conservation Advisor to the Llandaff Society object to the proposal. A summary of the grounds of objection relating to this planning application are as follows:

The Chairman of the Llandaff Society and Planning and Conservation Advisor to the Llandaff Society consider the amended plans to still have a serious negative effect on the Conservation Area for the following reasons:

(i) adverse impact on views into this intimate corner of the Conservation Area from Heol-y-Pavin, a key pedestrian route towards the Green and Cathedral.

(ii) The modern slab block extension proposed would mask the lower half of a beautiful stone built pine end wall of 1 Penedre and it would be close to the entrance pathway and porch of this Victorian cottage.

For these reasons it is considered contrary to LDP policy EN 9, to the CAA (existing and proposed) and to the Council's Infill SPG and should be refused.

## 7. **ANALYSIS**

7.1 The key issues for the consideration of this application are: a) design of proposal and impact on neighbouring properties; b) impact on conservation area.

7.2 In assessing the impact of the proposed development, the proposal should be considered against Policy KP 5 of the Cardiff Local Development Plan which

states that:

'all new development will be required to be of a high quality, sustainable design and make a positive contribution to the creation of distinctive communities, places and spaces by:

i. Responding to the local character and context of the built and landscape setting so that layout, scale, form, massing, height, density, colour materials, detailing and impact on the built and natural environment are all addressed within development proposals;

x. ensuring no undue affect on the amenity of the neighbouring occupiers.....'

7.3 With regard to guidance which helps inform Policy KP 5, the following advice within the adopted Residential Extensions and Alterations Supplementary Planning Guidance 2017 (SPG) can be considered, noting that due to the position of the proposed extension, a number of points may be applicable to the site.

7.4 Paragraph 7.6 of the adopted Residential Extensions and Alterations Supplementary Planning Guidance states that:  
*'Side extension: design principles: proposals should be appropriate to the existing house in terms of....scale, form and finish.'*

7.5 In Paragraph 7.12 of the SPG, it is stated that: front extensions should normally be sited behind the building line. The building line is considered a line formed by the fronts or sides of houses along a street. Each case is a matter of judgement. Some areas have a clear building line whereas others are more variable.

7.6 With regard to siting, scale and impact on the street scene, Paragraph 7.24 of the SPG states that the scale and form of an extension must be in keeping with, subservient to the existing building and its setting.

7.7 The SPG guidance continues at Paragraph 7.25 with regard to overlooking, stating extensions should not result in adverse loss of privacy to neighbours.

7.8 At Paragraph 7.26 of the SPG it is stated that:  
*'extensions should not be overbearing to your neighbours or result in an unacceptable loss of daylight or sunlight to neighbouring properties.'*

7.9 Impact on amenity, privacy and visual amenity.  
SPG - Paragraph 7.56 states: any extensions to the side or rear .....should maintain a 'reasonable' extension to ....garden ratio. This will vary according to the size and location of the house.

7.10 Detailing and materials  
Paragraph 7.65 of the SPG states all materials and detailing should reflect or complement the existing house.

- 7.11 Paragraph 8.31 continues *'Original gates, railings and walls should be retained, restored and repaired.'*
- 7.12 In respect of considering Built Heritage, Policy KP 17 of the Cardiff Local Development Plan states that: *'Cardiff's distinctive heritage assets will be protected, managed and enhanced, in particular the character and setting of its Scheduled Ancient Monuments; Listed Buildings; Registered Historic Landscapes, Parks and Gardens; Conservation Areas; Locally Listed Buildings and other features of local interest that positively contribute to the distinctiveness of the City.'*
- 7.13 Also, in terms of development affecting the historic environment, Policy EN 9 of the Cardiff Local Development Plan states that: *'Development relating to any of the heritage assets...or their setting will only be permitted where it can be demonstrated that it preserves or enhances that asset's architectural quality, historic and cultural significance, character, integrity and/or setting.'*
- 7.14 In addition it should be noted that the adopted Llandaff Conservation Area Appraisal provides general guidance with regard to residential extensions and suggests that matters to be considered include the effect of overlooking or dominance to neighbouring properties.
- 7.15 The adopted Appraisal also mentions that Heol-y-Pavin and nearby streets have a varied streetscape and building scale is modest. Natural materials play a strong part in defining the street scene and decoration of houses, with high stone boundary walls defining roads and plot rhythms.
- 7.16 The adopted Appraisal continues with - Issues which are of concern in this vicinity include loss of architectural detailing and lack of maintenance to boundary walls.
- 7.17 Therefore, enhancement opportunities to be welcomed are the use of natural building materials.
- 7.18 The adopted Appraisal also generally suggests the special character of Llandaff be maintained and new development integrated taking into consideration the affect or overlooking of neighbouring properties or domination of neighbouring properties.
- 7.19 Thought should be given to: is the extension visible from the road or public viewpoint. Will it encroach upon spaces between buildings closing out distant views. An extension that would intrude upon well established views is unlikely to protect the character of the conservation area.
- 7.20 Also, encouragement is given to the removal or improvement of previous inappropriate work as part of any new proposal.
- 7.21 The design of successful extensions requires a knowledge of building type and a sensitive handling of scale and detail.

- 7.22 The adopted Appraisal continues with new development: The following principles are designed to ensure that Llandaff's distinctive character is protected and enhanced in proposals for development:

New development should respect its historic context in scale, form, materials and design. This can be expressed in: continuity or variety in building line, adjacent building heights, roofs and cornice lines, excellence in new design.

- 7.23 In terms of conservation areas, Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that: *.....with respect to any building or other land in a conservation area.....special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area.*
- 7.24 Planning Policy Wales Edition 10 Paragraph 6.1.16 states that: Preservation or enhancement of a conservation area can be achieved by a development which either makes a positive contribution to an area's character or appearance or leaves them unharmed. Mitigation measures can also be considered which could result in an overall neutral or positive impact of a proposed development in a conservation area.
- 7.25 TAN24: The Historic Environment states at Paragraph 6.3 that: *Local Authorities must also give special attention to the desirability of preserving or enhancing conservation areas when taking planning decisions.*
- 7.26 Paragraph 6.7 also, states that: *Many conservation areas include sites or buildings that make no positive contribution to, or indeed detract from the character or appearance of the area; their replacement should be a stimulus to imaginative, high-quality design and an opportunity to enhance the area.*
- 7.27 Tan 12 Design states at paragraph 5.6.1: in conservation areas there should be regard to the desirability of preserving or enhancing their character and appearance.

### **Design of the Proposal and Impact on Neighbouring Properties**

- 7.28 The proposed single storey extension would be to the side of an existing two storey extension and would be roughly 'L' shaped. The extension would back onto the common boundaries with No. 11 Heol-y-Pavin and No. 1 Penedre. An existing small extension and an outbuilding would be demolished to accommodate the proposal. The design of the single storey extension would be contemporary, with conventional glazed openings, fronting onto the existing courtyard and a single ply membrane flat roof with metal fascia.
- 7.29 The submitted scaled drawings indicate that the proposed single storey extension would be roughly rectangular in shape measuring 8.5 by 7.8- 8.0 metres (noting that boundaries are not parallel). It would have a flat roof of varying heights ranging from 2.1 to 2.8 metres above finished floor level. (finished floor level would be 10cm above existing ground level). Where the extension meets the existing boundary wall along Penedre, the parapet height of the roof would be 0.2 metres higher than this wall and 2.2 metres higher than the street level of Penedre. Along the common boundary wall with No. 11 Heol-y-Pavin, the proposed flat roof would have heights of either 2.3 or 2.65

metres along this boundary, with the majority of the lower part of the roof being alongside this existing wall. The proposed flat roof would be 0.3-0.5 metres higher than this boundary wall although a small section of the higher roof would be 0.85 metres above this wall. The boundary wall has two heights at 1.8 and 2.0 metres. The lean-to structures situated at either end of this boundary wall (within the curtilage of No. 11 Heol-y-Pavin), have maximum heights of 2.85 and 2.5 metres. The tops of both of these sloping roofs are marginally higher than the proposed flat roof.

- 7.30 The height and length of the proposed flat roof extension along the common boundary with No.11 Heol-y-Pavin does not raise concern. The height of the structure has been kept to a minimum and has been lowered to 2.3 metres along the majority of the boundary with No. 11 Heol-y-Pavin.
- 7.31 The height and length of the extension along the common boundary with No. 1 Penedre does not raise concern. The height of the structure has been kept to a minimum along the west stone boundary wall with a flat roof commencing at a height of 2.1 metres and rising to a maximum height of 2.8 metres (all from finished floor level) where the proposed extension adjoins the front elevation of No. 1 Penedre. It then becomes a flat roof of varying heights of 2.8 metres dropping down to 2.3 metres where it meets the common boundary with No. 11 Heol-y-Pavin.
- 7.32 In consideration of the above, it is concluded that the proposal would not result in an un-neighbourly or overbearing form of development, nor would it cause unacceptable harm to the amenities of the neighbouring occupiers. It is therefore, considered acceptable in terms of impacts upon neighbouring amenity when considered against relevant policy and guidance.
- 7.33 The design of the extension is modern and uses natural materials along with modern materials, including conventional glazed openings and a low profile single ply membrane flat roof covering with a metal fascia. It is considered that the proposed flat roof would help to rationalise the flat roof of the existing two storey extension and that the subservient nature of the proposed extension to the existing building and its discreet position and use of complementary materials are considered to result in an acceptable design response in this instance.

### **Impact on Llandaff Conservation Area**

- 7.34 Throughout the planning process, the applicant has generally responded positively to observations raised relating to the character and appearance of the building and the wider conservation area. Accordingly, positive design changes have been made to the proposals.
- 7.35 The proposed extension will replace a small modern lean-to extension and outbuilding, which are considered to be unsympathetic to the character and appearance of the conservation area.
- 7.36 Measures which are considered to positively impact upon the existing situation

at the site, include the replacement of casement windows with upvc sliding sash windows, a replacement timber porch which would replace an existing unsympathetic upvc version, stone cladding to the ground floor and timber cladding to the first floor of the existing rendered two storey side extension, together with the demolition of two modern structures.

- 7.37 It is not uncommon for contemporary additions to adopt a low profile form in order to harmonise with the established traditional character of an area. While this may be unacceptable in more prominent locations, this is a relatively discrete location; where conventional glazed openings and the form of the roof are not considered to detract from the character of the conservation area.
- 7.38 Llandaff is characterised by an organic layout that has developed over many years. This property is unusual in that it does not front the main highway and is set back from the terrace at Penedre. Evidence from historic mapping indicates that a building (given address of No. 15 Heol-y-Pavin) occupied the yard area in the 1950's. Since this building was demolished and the changes made to the side, the open character of this corner and associated view towards the stone gabled terrace of Penedre has formed a part of the area's character. However, in consideration of the single storey design and positioning of the extension, it is considered that this view would not be compromised to a significant degree.
- 7.39 In respect of views and the visibility of the proposed development it is considered that the proposal will not result in harm to the character or appearance of the conservation area due to its position within the site and single storey design. Glimpsed views would be possible from public view points, however, the proposed extension would for the most part be concealed by the stone boundary wall.
- 7.40 The views across the application site towards properties along Penedre are not considered to be materially harmed, as the proposal will replace an existing outbuilding which in part conceals the front elevation of the adjacent property. It is considered that the proposed low flat roof with grey metal fascia would be a sufficiently recessive feature that would reflect the appearance of slate roofs and darkly finished fascias/gutters which characterise the area. This flat roof form is also considered to be a preferable alternative to a necessarily steeper mono or dual pitched slate roof, which would be likely to increase the prominence of the proposed new building and potentially block more of the view towards the stone gable of No. 1 Penedre and likely have an adverse effect on the amenities of adjacent occupiers. Views of the proposed flat roof itself will only be possible from very limited private viewpoints.
- 7.41 Members should be aware that the Llandaff Conservation Area Appraisal Consultation Draft 2019 review document is currently out to public consultation and is not as yet adopted guidance. However, it is considered that the proposals do not conflict with the guidance within the existing or draft appraisal documents.
- 7.42 Overall, it is considered that the proposal as amended accords with Policies KP



5, KP 17 and EN 9 of the Cardiff Local Development Plan, in that it effectively responds to local context and preserves the character of the Llandaff Conservation Area. The alterations proposed to the windows, porch and cladding to the existing building will enhance the area's character and appearance.

### **Third Party Representations**

- 7.43 In respect of the third party representations which have not already been addressed in the report:  
The request to consider previous comments and objections has been noted and carried out.  
Some comments raised are not considered to be material planning considerations.

### **Other Considerations**

- 7.44 Section 17(1) of the Crime and Disorder Act 1998 imposes a duty on the Local Authority to exercise its various functions with due regard to the likely effect of the exercise of those functions on, and the need to do all that it reasonably can to prevent, crime and disorder in its area. This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable increase in crime and disorder as a result of the proposed decision.
- 7.45 The Equality Act 2010 identifies a number of 'protected characteristics', namely age; disability; gender reassignment; pregnancy and maternity; race; religion or belief; sex; sexual orientation; marriage and civil partnership. The Council's duty under the above Act has been given due consideration in the determination of this application. It is considered that the proposed development does not have any significant implications for, or effect on, persons who share a protected characteristic, over and above any other person.
- 7.46 Well-Being of Future Generations Act 2016 – Section 3 of this Act imposes a duty on public bodies to carry out sustainable development in accordance with the sustainable development principle to act in a manner which seeks to ensure that the needs of the present are met without compromising the ability of future generations to meet their own needs (Section 5). This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable impact upon the achievement of wellbeing objectives as a result of the recommended decision.
- 7.47 The Environment (Wales) Act 2016 imposes a duty on the Local Authority to seek to maintain and enhance biodiversity in the proper exercise of its functions and in doing so to promote the resilience of ecosystems. It is considered that the proposed development does not have any significant implications for, or effect on, biodiversity.

## 8. **Conclusion**

- 8.1 Having regard to the policy context above, the proposal is considered to be acceptable and planning permission is recommended for the reasons stated above.



**Existing Block Plan**

Boundary wall

Penedre

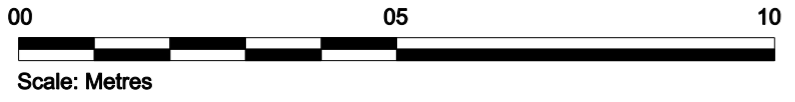
**Richard Whitaker Architects Ltd**  
 43 Plas Saint Andresse  
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 CF64 1BW  
 Tel: 029 2071 2631  
 Mob: 0781 8888 070  
 Email: mail@richardwhitaker.com

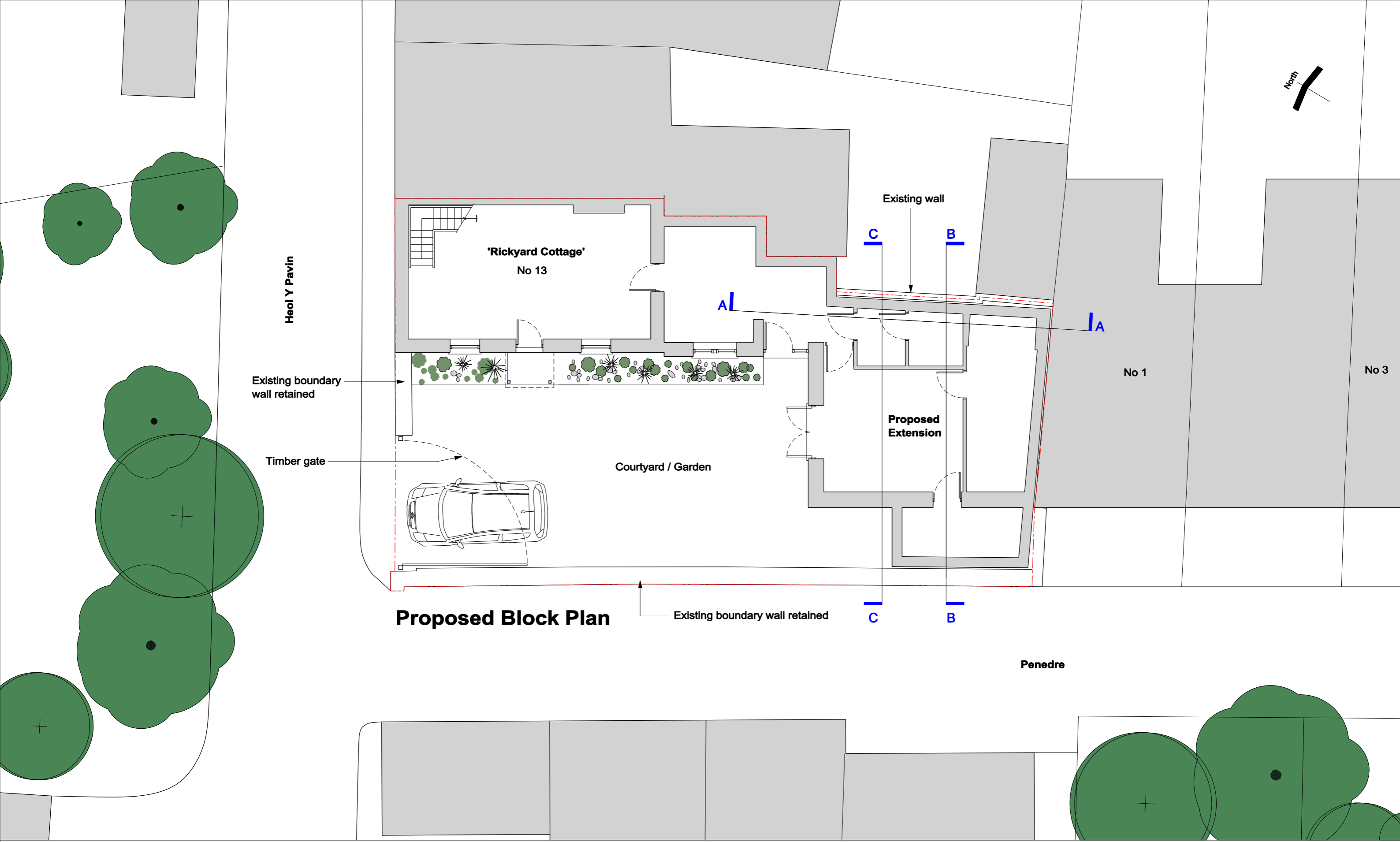
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**13 Heol Y Pavin**  
**Llandaff. CF5 2EG**

Scale: 1:100 @ A3  
 Date: June 2019

Title: **Existing Block Plan**

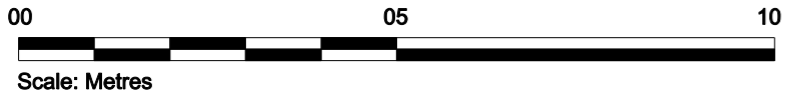
Drawing No: **462 - P01**





**Proposed Block Plan**

Penedre



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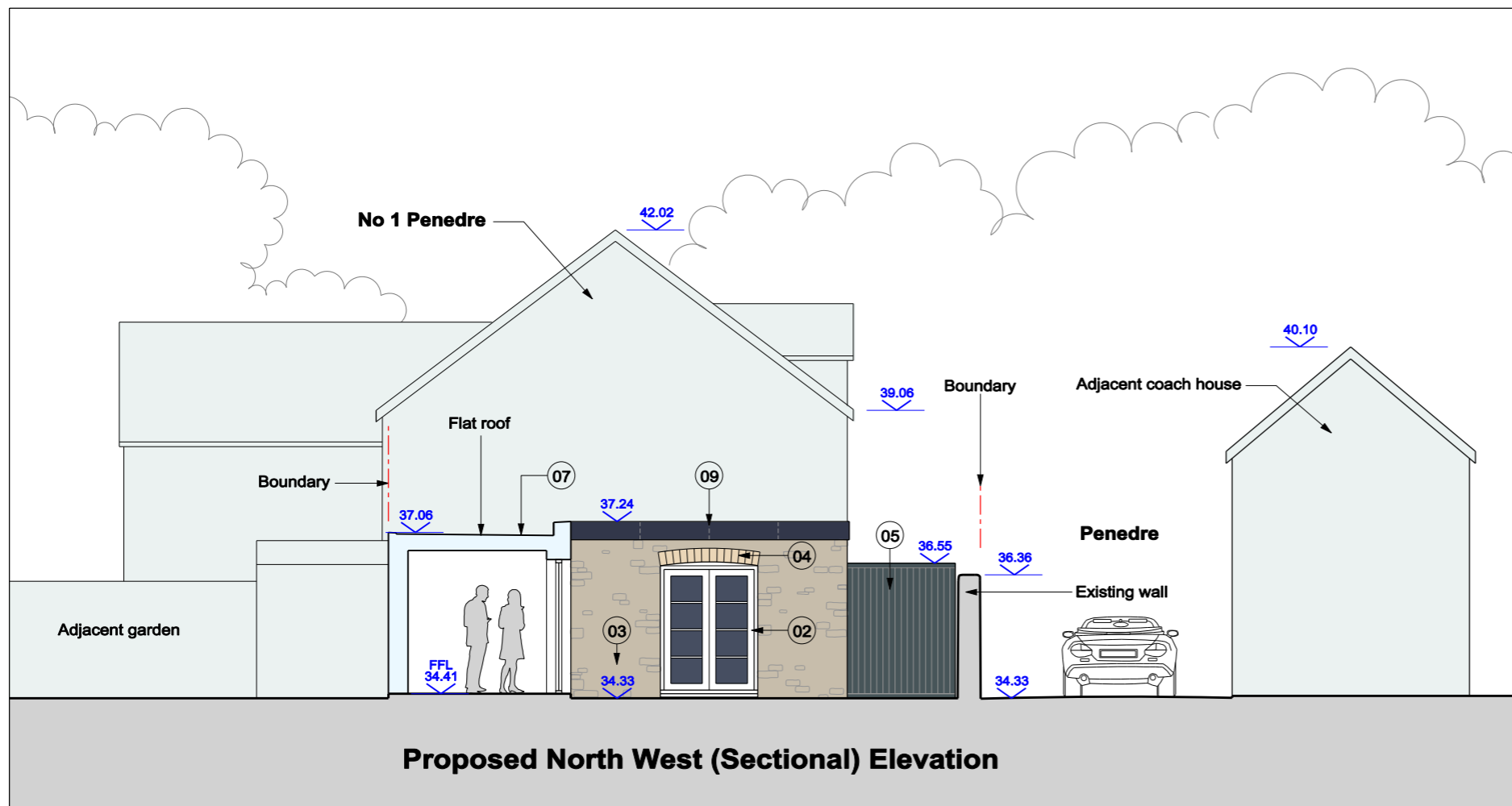
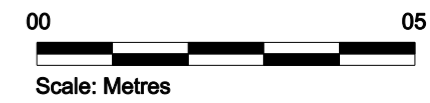
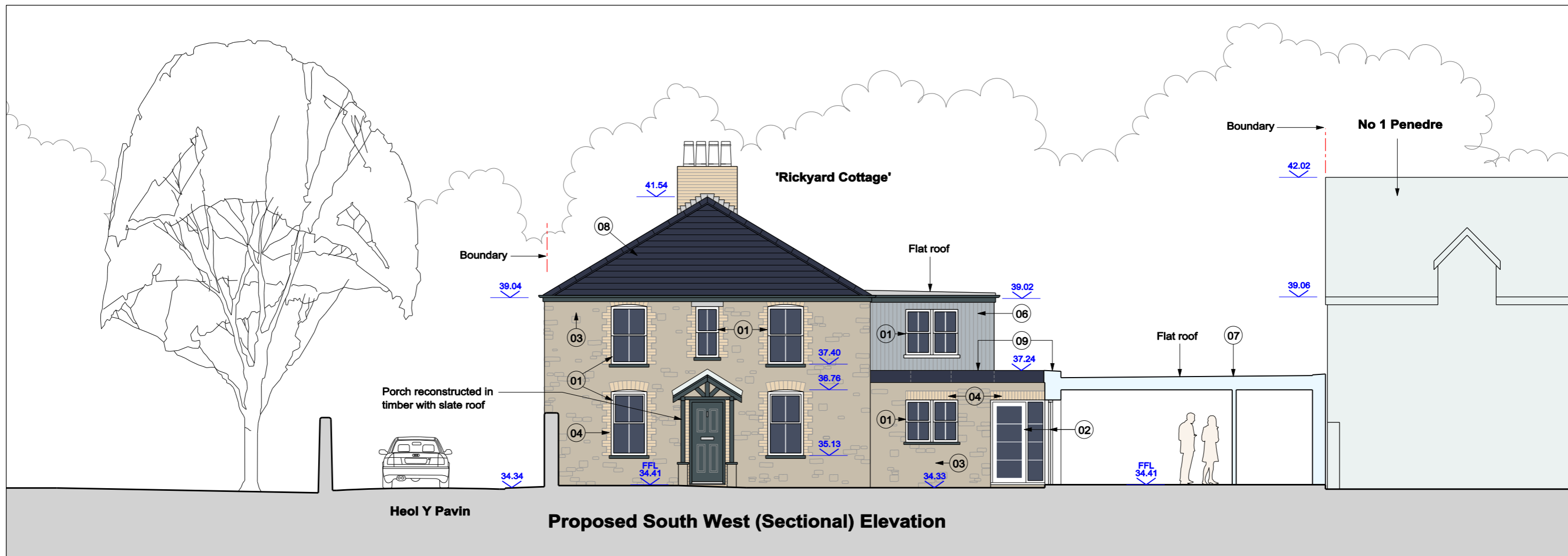
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**13 Heol Y Pavin**  
**Llandaff. CF5 2EG**

Scale: 1:100 @ A3      Date: June 2019

Revision: C

Title: **Proposed Block Plan**

Drawing No: **462 - P06**



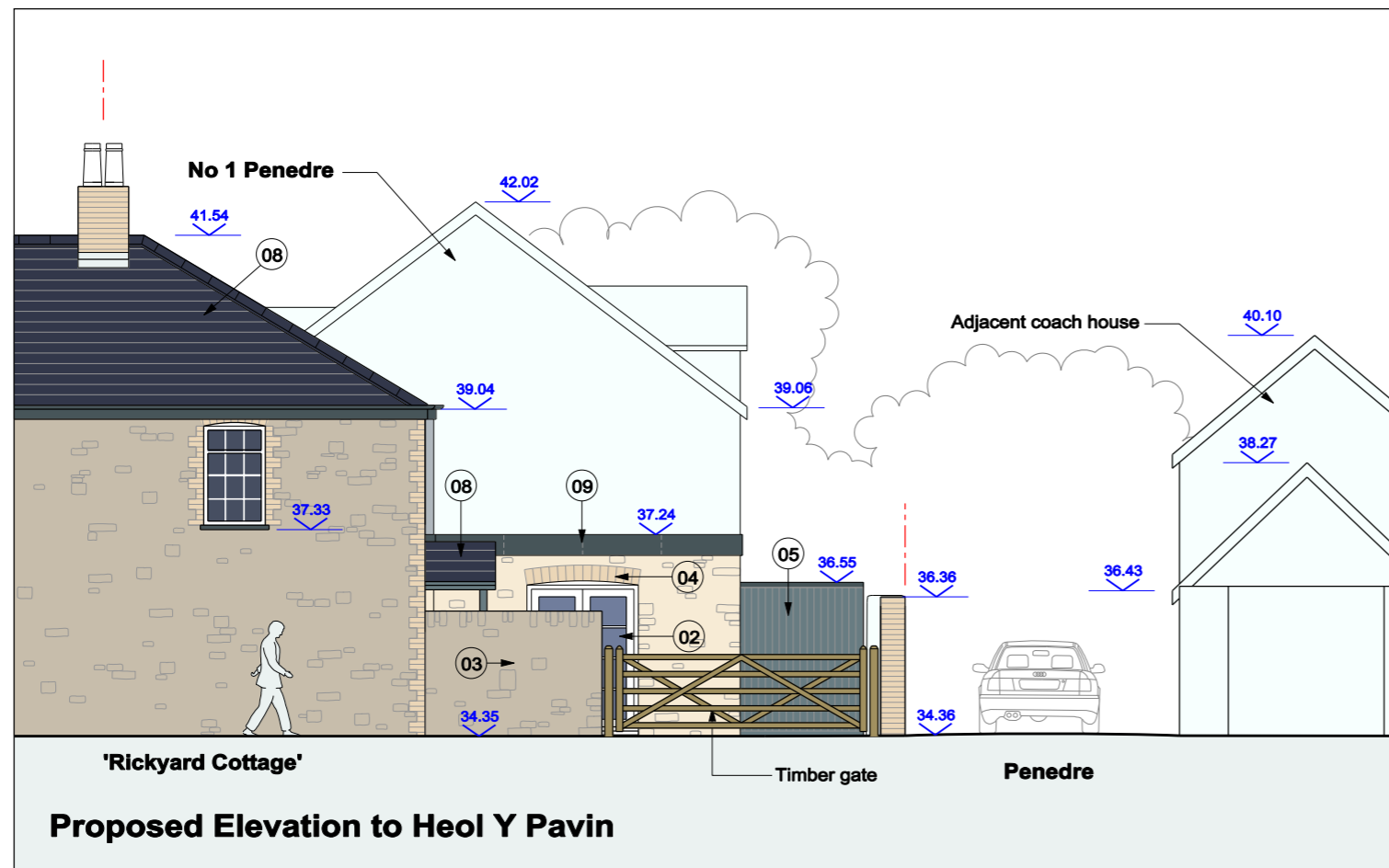
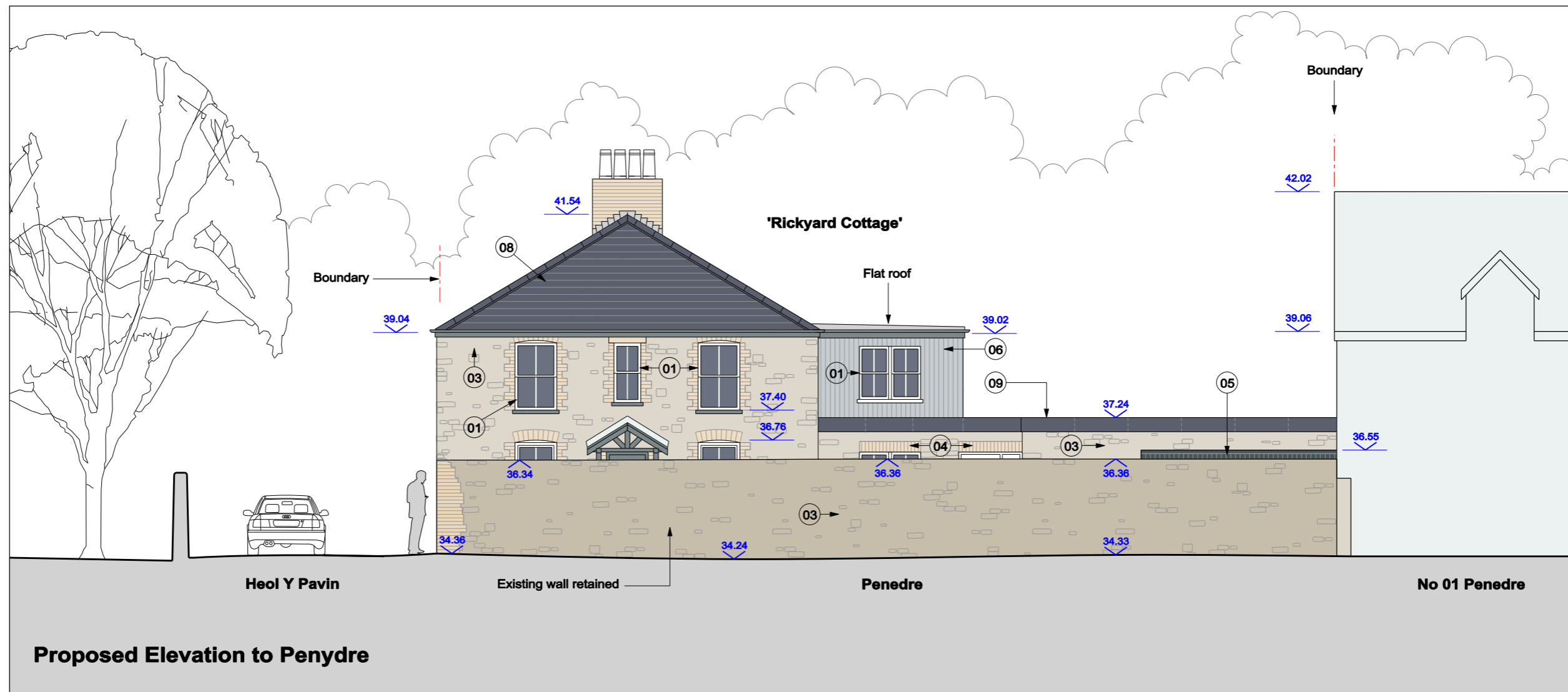
- LEDEND:**
- 01 = uPVC sash windows
  - 02 = uPVC doors
  - 03 = Natural stone
  - 04 = Facing brick
  - 05 = Dark stained timber cladding
  - 06 = Timber cladding
  - 07 = Single ply membrane flat roof
  - 08 = Slate
  - 09 = Grey coloured metal fascia

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Site: **Rickyard Cottage**  
**13 Heol Y Pavin**  
**Llandaff. CF5 2EG**  
 Title: **Proposed Elevations**  
**(sheet 01)**

Drawing No: **462 - P09**

Scale: 1:100 @ A3      Date: June 2019      Revision: D



**LEDEND:**

- 01 = uPVC sash windows
- 02 = uPVC doors
- 03 = Natural stone
- 04 = Facing brick
- 05 = Dark stained timber cladding
- 06 = Timber cladding
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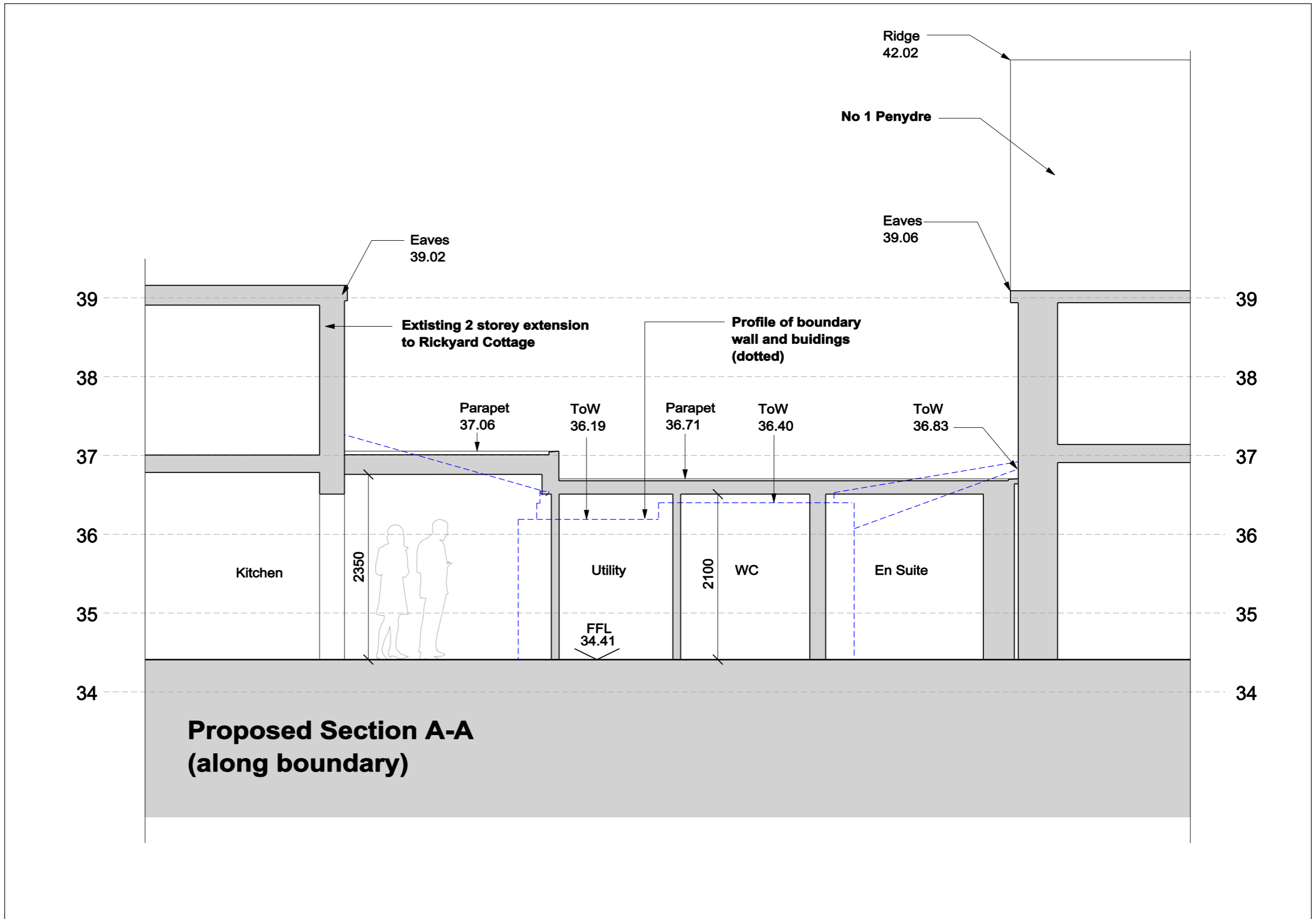
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Site: **Rickyard Cottage**  
**13 Heol Y Pavin**  
**Llandaff. CF5 2EG**

Title: **Proposed Elevations**  
**(Sheet 02)**

Drawing No: **462 - P10**

Scale: 1:100 @ A3 Date: June 2019 Revision: E



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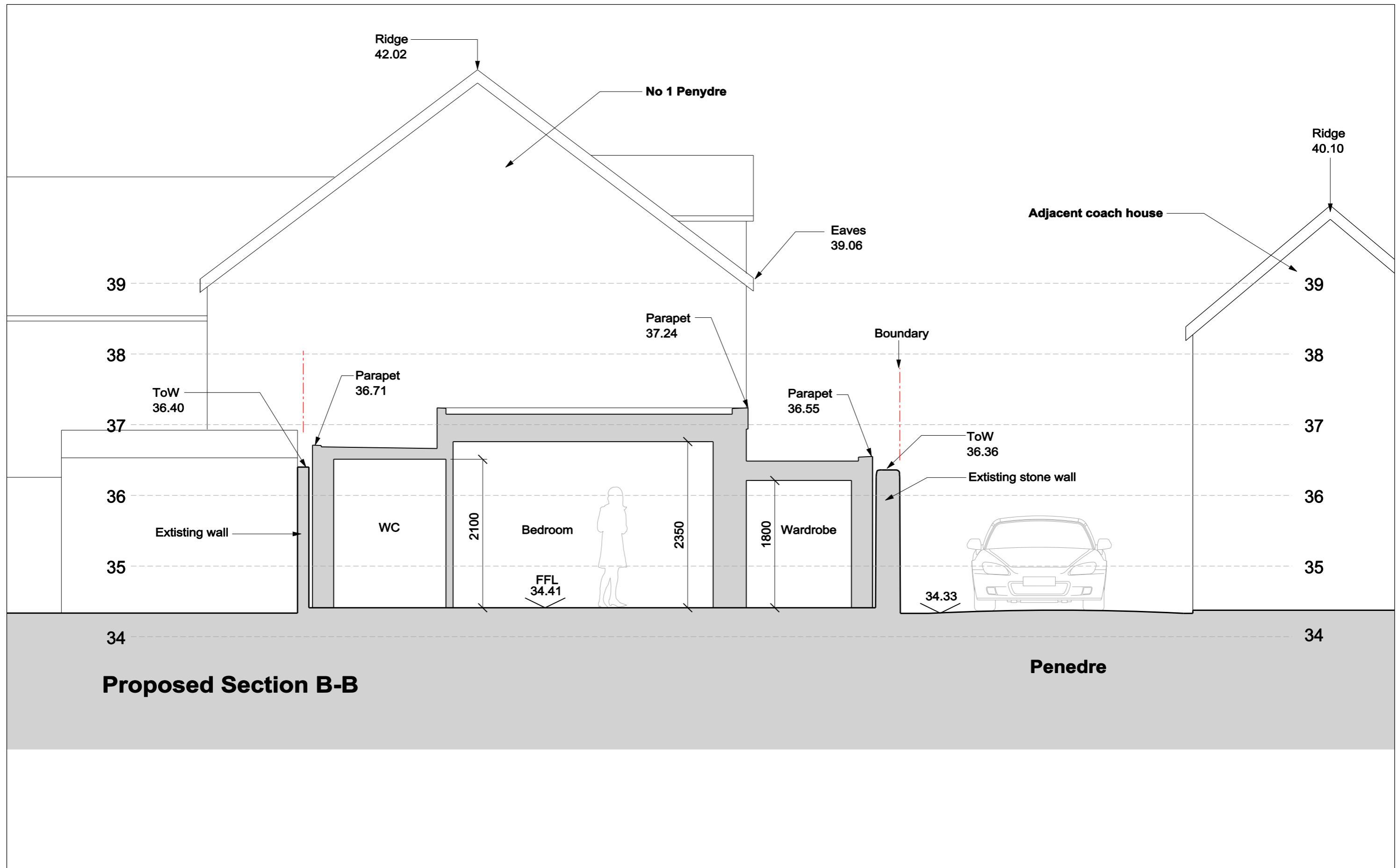
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**13 Heol Y Pavin**  
**Llandaff. CF5 2EG**

Scale: 1:50 @ A3  
Date: June 2019

Revision: C

Title: **Proposed Section A-A**

Drawing No: **462 - P11**



**Proposed Section B-B**

**Penedre**

00 01 02 03 04 05



Scale: Metres

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Site: **Rickyard Cottage**  
**13 Heol Y Pavin**  
**Llandaff. CF5 2EG**

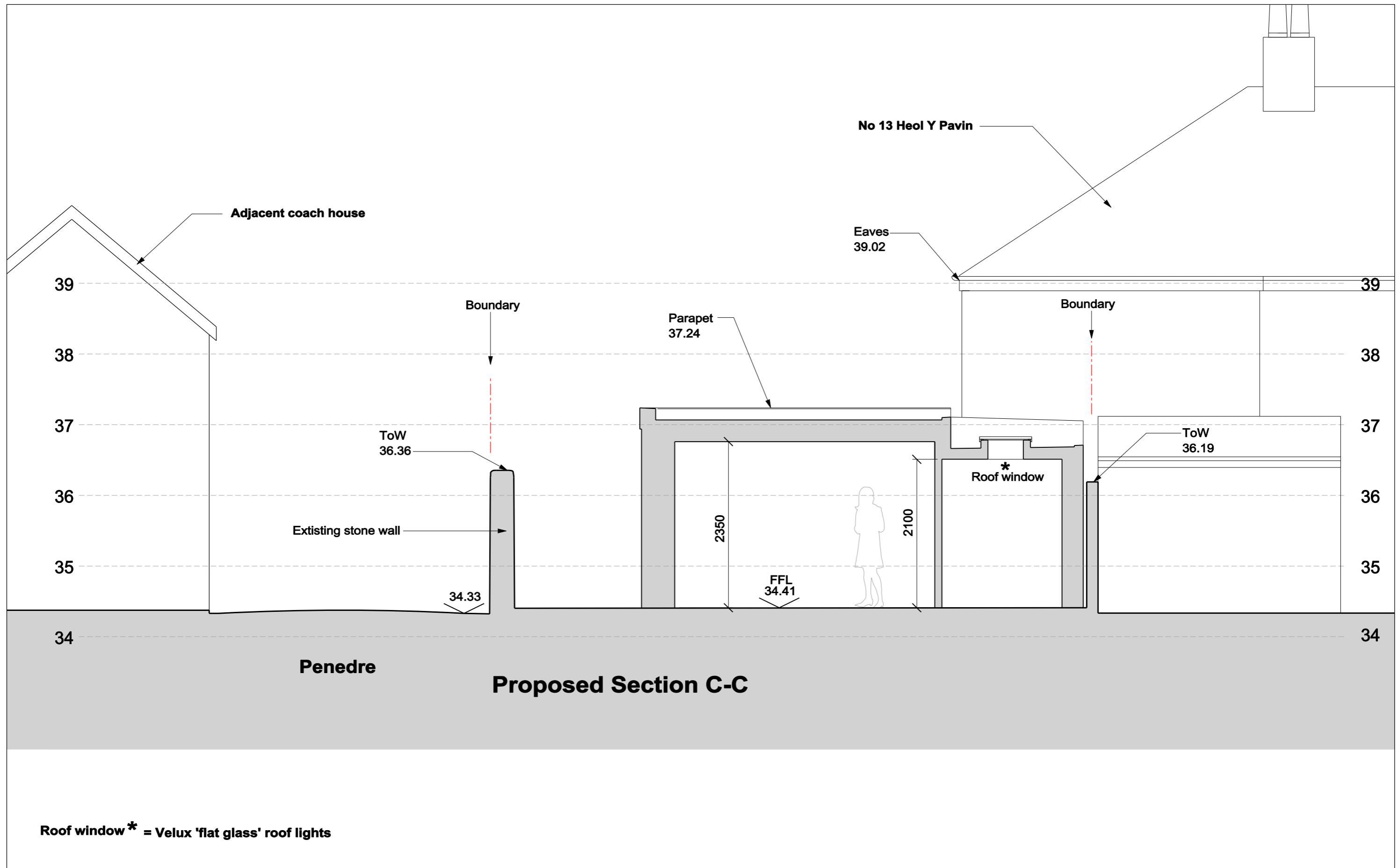
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 Date: June 2019

Revision: C

Title: **Proposed Section B-B**

Drawing No: **462 - P12**





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Site: **Rickyard Cottage**  
**13 Heol Y Pavin**  
**Llandaff. CF5 2EG**

Scale: 1:50 @ A3  
 Date: June 2019

Revision: C

Title: **Proposed Section C-C**

Drawing No: **462 - P13**

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## LOCAL MEMBER, MP AND AM OBJECTION

COMMITTEE DATE: 18/12/2019

APPLICATION No. **19/01521/MJR** APPLICATION DATE: 17/06/2019

ED: **CATHAYS**

APP: TYPE: Full Planning Permission

APPLICANT: Mr Ellerman

LOCATION: LAND TO THE REAR OF 14, QUEEN ANNE SQUARE,  
CATHAYS PARK

PROPOSAL: PROPOSED CONSTRUCTION OF 15 SINGLE ROOMS AND 2  
SELF CONTAINED STUDIOS STUDENT HUB WITH ON SITE  
CYCLE AND REFUSE STORE, PARKING AND AMENITY

RECOMMENDATION: That planning permission be **REFUSED** for the following reason :

1. The proposals for vehicular access to the site, including disabled access, are unacceptable, taking into consideration the constraints of the rear lane access and North Road, and the harm to highway safety resulting from the consequent use of North Road by vehicles, including deliveries, contrary to policies KP8, T5 and T6 of the Cardiff Local Development Plan (2006 – 2026).

### 1. DESCRIPTION OF PROPOSED DEVELOPMENT

- 1.1 This is a full planning application, as amended, for a 3 storey student accommodation building, with on-site parking, cycle and refuse storage, and amenity facilities. It would comprise 15 single bedrooms and 2 larger self-contained studio rooms. The ground and first floors would provide 7 single bedrooms per floor, with a further 3 rooms and communal facilities at 2<sup>nd</sup> floor. Each single bedroom would be en-suite, with sleeping and study zones, whilst the two studio rooms would comprise a sitting area in addition to kitchenette and bathroom facilities.
- 1.2 The building would be designed as a pair of three storey villas to reflect the large late Victorian properties which characterise the North Road section of the Cathays Park Conservation Area, with red brown facing brick walls with brick plinths, reconstituted stone string courses and bands, half timbering, reconstituted bath stone double height bay windows, and reconstituted bath stone cills and lintels, timber sash windows, a timber front door with a reconstituted bath stone porch. The roof would have two conservation style rooflights on the front and rear elevations. Existing boundaries to the north, east and south would be maintained in their current form. The existing pedestrian entrance in the boundary wall fronting North Road would be altered to form an entrance between two stone capped pillars and a gate and a gate with black

painted railings. An electric sliding gate is proposed to secure the vehicular access to the lane.

1.3 Pedestrian access would be solely from the North Road frontage and vehicular access from the rear lane, accessed off North Road. The site access is shown on the topographical survey as approx 3.5m, with the access reducing to approx 2.7m wide outside the site as it turns at a sharp angle to the lane. (See Section 2 for measurements from officer site visit.) The DAS notes that vehicle movement to the rear of the site would be controlled via a management agreement, monitored by a controlled access and only available to authorised visitors at specified times. Pedestrian access would be controlled by a coded door entry system. The DAS notes that a single car parking space is proposed for both general maintenance and deliveries. The Transport Technical note notes that this space would be used to service the proposed building, inclusive of maintenance and would provide a dedicated drop-off point for students at the start and the end of term times (para 2.4.2). Cycle storage for 16 cycles and refuse storage would be located to the rear of the site, and waste collected via the rear lane access. Whilst land ownership is not a planning matter *per se*, the Agent has confirmed that the site owner has legal right to utilise the lane up to the boundary of the site. Planting is proposed along the North Road frontage and to the rear of the building, with the vehicle parking area and paths to be formed in permeable materials.

1.4 The Transport Technical Note (Revision E dated 26<sup>th</sup> September 2019, as amended) provides further details, including:

*Refuse vehicle access* – to be serviced by an 18 tonne refuse vehicle, as advised by the Council's Waste Management team. Vehicle to turn left into the northern lane from North Road, before turning south at the rear of the Christian Science Church and reversing along the northern rear lane as far as possible, with bins then carried /dragged from the bin store. A swept-path analysis is provided for an 18 tonne refuse vehicle.

*Construction vehicle access* – it is anticipated that the range of construction vehicles are likely to include 8.4m concrete mixer, 7.1m mobile crane, 6.3m skip lorry, small tipper lorry and low loader with excavator, the majority of which would access the site in the same way as the refuse vehicle. Swept path analyses are provided for a 8.4m concrete mixer, a 7.1m mobile crane and a 6.3m skip lorry; these are not shown actually entering the site. The Technical Notes states that an excavator would need to be transported to the site to prepare the groundworks and that this would need to stop on North Road and require temporary lane closure, which would need to be agreed with the highway authority in advance, outside peak period.

*Student access and the start and end of term* – The single car parking space would be used to service the proposed building, inclusive of maintenance, and provide a dedicated drop-off point for students at the start and end of term. A strict time booking system would be used to manage the number of vehicles on site and ensure they do not arrive/leave at the same time, to be managed by the letting agents. The student/parents would drive up the lane to access the

site at their allotted time, park to unload their car and leave the site before the next vehicle arrives. The TN notes that whilst there is only one car parking space, there is sufficient space to accommodate an additional vehicle in the event that there is an unavoidable overlap. Swept path analysis drawings are submitted to demonstrate that various car sizes (4.7m Estate Car, 4.5m Volkswagen Touran and Mini with wing mirrors folded out and Luxury 4 x 4 with wing mirrors folded in can drive up the lane in a forward gear, manoeuvre within the car parking space and exit the site in a forward gear. The TN concludes that due to the size of the development and the fact that they are all furnished, it is likely that the provision of dedicated time slots for each resident throughout a 1 to 2 day time period would be an effective and easy way to manage the arrival and departure of students at the beginning and end of term.

- 1.5 Whilst not included within the TN and with no supporting commentary, swept path analysis have also been submitted for a 7.5t box van, 7.9m fire engine and ambulance, showing the vehicles as accessing the site by reversing up the rear lane, but not entering the site itself.
- 1.6 Two sets of amended plans / additional information were issued for consultation 09/09/2019 and 07/11/2019.
- 1.7 The September submission included the following:
  - An amended Block Plan following detailed survey of the proposed access and including amendments to building position, landscaping, refuse and cycle storage
  - Amendments to the detailing on the front elevation of the building, and new architectural detailing and materials specification
  - Tree Assessment (including details of the impact of the development on the adjacent trees within Nazareth House) and landscaping
  - A Transport Technical Note, including additional information in respect of Construction Traffic Management, details of refuse, servicing/deliveries and student access, management of start and end of term, refuse process and management
  - Further swept path assessments.
- 1.8 The November submission included the following:
  - An amended block plan to incorporate amendments to cycle parking
  - A new Topographical survey plan
  - An amended Transport Technical Note, including updated swept path analysis
  - Additional swept path analyses
  - Details of two tier, cycle storage for 16 cycles
  - An Air Quality Assessment
- 1.9 The application is supported by:
  - a Design and Access Statement
  - a Transport Technical note, including swept path assessments of various vehicles
  - a Tree Assessment

- an Air Quality Assessment.

## 2. **DESCRIPTION OF SITE**

- 2.1 The application site comprises a broadly level and rectangular shaped, vacant and overgrown plot of land measuring 425m<sup>2</sup>, which originally formed part of the curtilage and rear garden of 14 Queen Anne Square. The site benefits from outline planning permission for a single dwelling and constitutes previously developed land in policy terms.
- 2.2 The site lies to the rear of 14 Queen Anne Square, and adjoins that property's rear garden and garage to the east, and the gardens of Nazareth House care home to the north. It has a frontage onto A470 North Road to the west, which is a major traffic route into the City. To the south, the site adjoins the Rosemount Funeral Home at 30 North Road and a rear lane.
- 2.3 The boundary to no 14 Queen Anne Square, to the east, is formed by its garage, a high brick wall and timber fencing, and that to Nazareth House by a low brick walls with brick piers and timber fencing, rising to approx 2.2m. The garage belonging to no 14 Queen Anne Square has a window that faces into the site. Beyond the northern boundary and within the gardens to Nazareth House is a collection of densely planted mature trees. The southern boundary is formed by a stone wall, approx 1.8m high. The western boundary to North Road is formed by a high stone wall with a timber gate.
- 2.4 The lane from which vehicular access is proposed, runs parallel to North Road, to the rear of, and providing access to, various Queen Anne Square and North Road properties, both north and south of its junction with North Road. The northern part of the lane is private (not adopted highway), whilst the southern part and the length that forms the junction with North Road is adopted. The northern part of the lane is over 65m long, single track, uneven, overgrown in places with vegetation and unlit. Whilst varying in width, the lane is shown on the submitted topographical survey as being approx 4.6m wide outside the site. Some planters are present to the rear no No 13 Queen Anne Square (as shown on the tracking diagrams) and trees further south, restricting the available width of the lane.
- 2.5 Pedestrian access to the site is currently via a gate in the high stone wall that forms the western site boundary onto North Road. There is no direct vehicular access to the site off North Road. A vehicular access to the site has been formed off the rear lane. The point of access to the site is constrained by the existing garage belonging to 14 Queen Anne Square and a left bend in the lane as it approaches the site entrance. A 2m close board fence has been erected across the proposed access. A site visit was arranged with Agent, case officer and Transport officer to remove the fence, and inspect and measure the access. The width at the point of access to the site was measured as approx. 3.6m wide from the stone wall to No 14's garage, aprox 4.14m from the line of the rear of the garage to the wall opposite and approx. 2.83m wide in between where the access bends.

- 2.6 There are various Traffic Regulation Orders (TROs) in the vicinity of the site, which are in place to manage traffic and parking. There are double yellow lines outside the site on North Road, meaning that no waiting can take place, and additional 'No Loading' restrictions, meaning that loading cannot take place on Monday - Saturday between 07:45-09:15 and 16:00-18:00 hours.
- 2.7 A tidal lane traffic control scheme is also operated by the Council along the North Rd frontage of the site to manage traffic along North Road, whereby three lanes are provided, with the direction of the middle lane switching depending on the time of the day. (The in-bound lane is the lane closest to the site frontage.) Traffic signs and overhead gantries indicate when the lane is open or closed to traffic in a particular direction. Coloured cats eyes are also used, glowing red to show a line that cannot be crossed, and white between traffic lanes going in the same direction. On Monday to Saturday the tidal flow operates as 2 lanes inbound between 05:00-14:30 and 18:30-19:55, and two lanes outbound at other times. Sundays are as above with the exception that 16:30 – 18:30 there are 2 lanes outbound. There are different arrangements when there are special events/matches in the City Centre, where timings are changed depending on the time and nature of the event. It takes around 10 minutes for a manual change to the lane control to take effect on the ground. A key time for the tidal flow would be when there are 2 outbound lanes and the lane adjacent to the site is the sole inbound lane, which could be used and blocked at times by a vehicle associated with the new development. The lane adjacent to the site would be the sole inbound lane in the afternoon between 14:30 – 18:30, and evening/night, 19:55 – 05:00. These tidal flow hours do not match the hours of the 'no loading' restrictions adjacent to the site on North Road (07:45-09:15 and 16:00-18:00).
- 2.8 There are various parking restrictions on the rear lane, as follows:
- single yellow lines (no waiting at specified times) on both sides of the unadopted rear lane north of no 9 Queen Anne Square and approx. up to the shared boundary between nos 12 and 13 Queen Anne Square, with signage listing the specified times for no waiting as Mon – Sat 8am – 6:30pm. However, Transportation advise in their comments that the actual order wording refers to 'at any time' (i.e. double yellow lines), and that the order is not legally enforceable as a result of this discrepancy. Transportation advise that even if there was not this discrepancy, Cardiff Council would not normally enforce parking on private roads without the permission of the landowner and would not do so give the current surfacing condition of the road.
  - single yellow lines (no waiting at specified times) on both sides of the southern part of the rear lane (which is adopted highway) and a residents parking bay.
  - double yellow lines (no waiting at any time) on the section of the lane that runs north east from its junction with North Road (also adopted highway).
- 2.9 With regards heritage assets, the site lies within the Cathays Park Conservation Area, on the opposite side of North Road from the boundary of the Grade 1 listed registered Historic Park and Garden 'Cardiff Castle and Bute Park'. The

Rosemount Funeral home complex lies to the SSE of the site and includes the two storey Grade II listed detached mid19<sup>th</sup> c villa. The southern elevation of the villa is the principle elevation, which is set at right angles to North road. A single storey flat roofed outhouse/garage building lies adjacent to the shared boundary with the site, itself attached to a single storey out building, with dormer windows and roof accommodation. The Grade II listed Nazareth House lies to the NNW. The site also lies within approx 0.85k of the Cardiff Castle and Roman Fort Scheduled Monument and approx 0.75km from the Dominican Friary Scheduled Monument.

- 2.10 The Councils Conservation Area Appraisal for the Cathays Park (2009) notes that the Cathays Park Conservation Area was designated in 1975 and extended in 1992 to include Blackweir Farm to the west, Nazareth House to the north and the Prudential Assurance Building and the Hilton Hotel to the south. It includes a number of distinct 'areas of special character', of which Queen Anne Square is one. Queen Anne Square consists of 30 red brick, two storey Neo-Georgian houses built in the 1930s and 1950s and set in generous grounds and arranged in two parallel lines either side of a rectangular shared lawn glanced by street trees and a shared circular driveway. North Road is a further 'area of special character', with the appraisal noting it is a major traffic route into the city that dominates the character of this sub-area. The appraisal refers specifically to the series of attractive semi-detached Victorian villas which have retained much of their original detailing, located along North Road, further to the south of application site. They comprise 2 1/2 and 3 storey villas, of traditional appearance, a common scale and proportion, with slate pitched roofs and brick, render and stone detailing. A more recently built religious establishment, the Christian Science Church, at 28 North Road adopts a modern interpretation of the adjacent traditional properties. There are a number of student residences and university buildings and civic buildings in close proximity to the site, within the Conservation Area.
- 2.11 The site falls within flood zone A and, therefore, outside areas considered at risk from flooding now and in the future. The site falls within a highly sustainable location, within easy access of shops/local facilities in North Road, Cathays Terrace, with public transport and cycle facilities readily accessible from the site. A large public car park is located directly opposite the site and beyond this, Bute Park.

### 3. **RECENT PLANNING HISTORY OF RELEVANCE**

#### **Site history**

- 3.1 16/00604/MNR – 'Outline planning permission for a single dwelling (renewal of earlier permissions' – permitted 06/05/2016

It is noted that access was conditioned as a reserved matter and that the conditions also included the following access related conditions:

- *All vehicular access to the site shall be provided via the lane to the rear of 30 North Road only and the access shall be permanently maintained. Reason: In the interests of highway safety and to maintain the free flow of traffic along North Road, in accordance with policy T5 of the Cardiff Local Development Plan.*



- *The details submitted as reserved matters shall include a turning space within the curtilage of the site enabling vehicles to enter and leave the site in a forward gear. The approved space shall be maintained thereafter and not be used for any purpose other than the turning of vehicles. Reason: In the interests of highway safety, in accordance with policy T5 of the Cardiff Local Development Plan.*

The delegated report notes in para 8.2 that, 'access to the site can be achieved via the rear lane. The lane is restricted in width but is adequate to serve the development - it is already used for vehicular access by adjoining properties, and its use would not harm the amenities of the residents of Queen Anne Square or cause their access to become blocked provided that parking and turning space is provided within the application site. Transportation officers have no objection to the proposals'.

11/0044/DCI – 'Variation of condition 1C of 08/00854/C to allow application for approval of the reserved matters to be made to the local planning authority before the expiration of three years from the date of this permission' – permitted 11/05/2011

08/00854/C – outline permission granted for 1 no dwelling – permitted 28/05/2008

02/00441/C – outline permission granted for a single dwelling (renewal of 99/00265/C) – permitted 16/04/2002

99/00265/C – outline permission granted for a single dwelling (renewal of 96/00142/C) - 17/02/1999

96/00142/C – outline permission granted for a single dwelling (renewal of 92/01861/C) - 21/03/1996

92/01861/C – outline permission granted for single dwelling –09/02/1993

Planning history cited by the Applicant.

3.2 19/01863/MNR – 16 North Road- Convert existing office building into 4 luxury 2 bedroom flats and double storey extension –approved 29/09/19

19/01864/DCH - 18 North Road - Demolition of existing rear previous extension and erection of double storey extension with balcony – approved 29/08/19

#### 4. **POLICY FRAMEWORK**

##### Local Policy

4.1 **Cardiff Local Development Plan 2006-2026** (Adopted January 2016)

Key Policies:

KP5: GOOD QUALITY AND SUSTAINABLE DESIGN

KP6: NEW INFRASTRUCTURE

KP7: PLANNING OBLIGATIONS

KP8: SUSTAINABLE TRANSPORT

KP13: RESPONDING TO EVIDENCED SOCIAL NEEDS  
KP14: HEALTHY LIVING  
KP15: CLIMATE CHANGE  
KP16: GREEN INFRASTRUCTURE  
KP17: BUILT HERITAGE  
KP18: NATURAL RESOURCES

Detailed Policies:

H3: AFFORDABLE HOUSING  
EN7: PRIORITY HABITATS AND SPECIES  
EN8: TREES, WOODLANDS AND HEDGEROWS  
EN9: CONSERVATION OF THE HISTORIC ENVIRONMENT  
EN10: WATER SENSITIVE DESIGN  
EN13: AIR, NOISE, LIGHT POLLUTION AND LAND CONTAMINATION  
EN14: FLOOD RISK  
T1: WALKING AND CYCLING  
T5: MANAGING TRANSPORT IMPACTS  
T6: IMPACT ON TRANSPORT NETWORKS AND SERVICES  
C3: COMMUNITY SAFETY/ CREATING SAFE ENVIRONMENTS  
C5: PROVISION FOR OPEN SPACE, OUTDOOR RECREATION,  
CHILDREN'S PLAY AND SPORT  
C6: HEALTH  
W2: PROVISION FOR WASTE MANAGEMENT FACILITIES IN  
DEVELOPMENT

- 4.2 The application site falls within Cardiff's settlement boundary, as identified on the adopted Cardiff LDP Proposals Map, outside the Central & Bay Business Areas.

**Supplementary Planning Guidance (SPG) and other local guidance**

- 4.3 Relevant SPG approved following the adoption of the Cardiff Local Development Plan:  
Green Infrastructure (approved November 2017)  
Infill Sites (approved November 2017)  
Managing Transport Impacts (Incorporating Parking Standards) (April 2018)  
Planning Obligations (approved January 2017)  
Residential Design Guide (approved January 2017)  
Waste Collection and Storage Facilities (approved October 2016)  
Student Accommodation (March 2019)  
Cardiff City Centre Conservation Area Appraisals (2009), including – Cathays Park

National Planning Policy

- 4.4 Planning Policy Wales (**Edition 10, December 2018**) and the Wales Spatial Plan set out the land use policies of the Welsh Government. These are supplemented by a series of Technical Advice Notes and Circulars.
- 4.5 Key statements contained within Planning Policy Wales include:
- *Legislation secures a presumption in favour of sustainable development in accordance with the development plan unless material considerations*

*indicate otherwise to ensure that social, economic, cultural and environmental issues are balanced and integrated (para 1.17)*

- *All development decisions, either through development plans policy choices or individual development management decisions should seek to contribute towards the making of sustainable places and improved well-being (para 2.2)*
- *Planning policies, proposals and decisions must seek to promote sustainable development and support the well-being of people and communities across Wales. This can be done through maximising their contribution to the achievement of the seven well-being goals and by using the five Ways of Working, as required by the Well-being of Future Generations Act. This will include seeking to maximise the social, economic, environmental and cultural benefits, while considering potential impacts when assessing proposals and policies in line with the Act's Sustainable Development Principle (para 2.8)*
- *Planning authorities should ensure that social, economic, environmental and cultural benefits are considered in the decision-making process and assessed in accordance with the five ways of working to ensure a balanced assessment is carried out to implement the Well-being of Future Generations Act and the Sustainable Development Principle. There may be occasions when one benefit of a development proposal or site allocation outweighs others, and in such cases robust evidence should be presented to support these decisions, whilst seeking to maximise contributions against all the well-being goals (para 2.21)*
- *Meeting the objectives of good design should be the aim of all those involved in the development process and applied to all development proposals, at all scales (para 3.4)*
- *Development proposals must address the issues of inclusivity and accessibility for all. This includes making provision to meet the needs of people with sensory, memory, learning and mobility impairments, older people and people with young children. Good design can also encourage people to meet and interact with each other, helping to address issues surrounding loneliness. Good design must also involve the provision of measures that help to reduce the inequality of access to essential services, education and employment experienced by people without access to a car. Design measures and features should enable easy access to services by walking, cycling and public transport (para 3.6)*
- *Developments should seek to maximise energy efficiency and the efficient use of other resources (including land), maximise sustainable movement, minimise the use of non-renewable resources, encourage decarbonisation and prevent the generation of waste and pollution (para 3.7)*
- *The layout, form, scale and visual appearance of a proposed development and its relationship to its surroundings are important planning considerations. A clear rationale behind the design decisions made, based on site and context analysis, a strong vision, performance requirements and design principles, should be sought throughout the development process and expressed, when appropriate, in a design and access statement (para 3.9)*
- *In areas recognised for their particular landscape, townscape, cultural or historic character and value it can be appropriate to seek to promote or*

*reinforce local distinctiveness. In those areas, the impact of development on the existing character, the scale and siting of new development, and the use of appropriate building materials (including where possible sustainably produced materials from local sources), will be particularly important (para 3.10)*

- *Local authorities are under a legal obligation to consider the need to prevent and reduce crime and disorder in all decisions that they take (para 3.11)*
- *New development must provide appropriate levels of secure, integrated, convenient and accessible cycle parking and changing facilities (para 4.1.34)*
- *Planning authorities must require good standards of car parking design, which do not allow vehicles to dominate the street or inconvenience people walking and cycling. Car parking should be overlooked by surrounding properties, to provide natural surveillance (para 4.1.52)*
- *Planning authorities will need to ensure that in development plans and through the development management process they make the most efficient use of land and buildings in their areas. Higher densities must be encouraged on sites in town centres and other sites which have good walking, cycling and public transport links (para 4.2.22)*
- *Infill and windfall sites can make a useful contribution to the delivery of housing. Proposals for housing on infill and windfall sites within settlements should be supported where they accord with the national sustainable placemaking outcomes (para 4.2.23)*
- *Whilst employment and residential uses can be compatible planning authorities should have regard to the proximity and compatibility of proposed dwellings to existing industrial and commercial uses to ensure that both residential amenity and economic development opportunities are not unduly compromised (para 5.4.15)*
- *The Welsh Government's specific objectives for the historic environment seek to ...preserve or enhance the character or appearance of conservation areas, whilst the same time helping them remain vibrant and prosperous... (para 6.1.16)*
- *There is a strong presumption against the granting of planning permission for developments, including advertisements, which damage the character or appearance of a conservation area or its setting to an unacceptable level. In exceptional cases, the presumption may be overridden in favour of development considered desirable on public interest grounds (para 6.1.15)*
- *Preservation or enhancement of a conservation area can be achieved by a development which either makes a positive contribution to an area's character or appearance or leaves them unharmed. Mitigation measures can also be considered which could result in an overall neutral or positive impact of a proposed development in a conservation area (para 6.1.16)*
- *The presence of a species protected under European or UK legislation, or under Section 7 of the Environment (Wales) Act 2016 is a material consideration when a planning authority is considering a development proposal which, if carried out, would be likely to result in disturbance or harm to the species or its habitat and to ensure that the range and population of the species is sustained. Planning authorities should advise anyone submitting a planning application that they must conform with any*

*statutory species protection provisions affecting the site, and potentially the surrounding area, concerned (para 6.4.22)*

- *Planning authorities should protect trees, hedgerows, groups of trees and areas of woodland where they have ecological value, contribute to the character or amenity of a particular locality, or perform a beneficial and identified green infrastructure function (para 6.4.25)*
- *Planning authorities should be aware of the risk of surface water flooding, usually caused by heavy rainfall, and ensure developments are designed and planned to minimise potential impacts. Development should not cause additional run-off, which can be achieved by controlling surface water as near to the source as possible by the use of SuDS (para 6.6.27)*

## **Technical Advice Notes (TANs) and other legislation**

### 4.6 Key TANs include:

- TAN 2: Planning and Affordable Housing (2006)
- TAN 11: Noise (1997)
- TAN 12: Design (2016)
- TAN 15: Development and Flood Risk (2004)
- TAN 18: Transport (2007)
- TAN 24: The Historic Environment (2017)

Section 66(1) of Planning (Listed Buildings and Conservation Area) Act 1990: In considering whether to grant planning permission for development which affects a listed building or its setting, the local planning authority...shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

Section 72 (1) of Planning (Listed Buildings and Conservation Area) Act 1990: In the exercise, with respect to any buildings or other land in a conservation area... special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area.

## 5. **INTERNAL CONSULTEE RESPONSES**

- 5.1 **Pollution Control (Contaminated Land)** raise no objection and recommend conditions to control ground gas, unforeseen contamination, imported soil, imported aggregates and use of site won materials, with advice recommended in respect of contamination and unstable land.
- 5.2 **Pollution Control (Noise)** raise no objection and recommend conditions to require a scheme to protect future occupiers from road traffic noise and a scheme to protect neighbouring occupiers from noise from fixed plant and equipment on site, and an advisory in respect of construction noise.
- 5.3 **Pollution Control (Air Quality)** require the submission of an Air Quality Assessment given the size of the development, its proposed residential use and location in close proximity to the busy road network (A470), to assess the potential impacts on ambient air quality and the magnitude / risk of these

potential air quality impacts on local/ current and future residents. Detailed advice is provided on the scope of the required assessment.

- 5.4 In response to the September amended plans submission, the Air Quality officer confirmed that the AQA would be needed upfront, prior to determination, not by condition as proposed.
- 5.5 Following the November submission, the Air Quality officer confirmed that an Air Quality Assessment (AQA) and that they are satisfied on grounds of air quality, subject to a condition to require a Construction Environmental Management Plan for the construction phase, outlining a detailed Dust Management Plan.
- 5.6 In respect of the initial submission, the **Council's Tree Officer**:
- Advises that in the absence of a tree assessment, they cannot confirm that unacceptable harm would not result to the evergreen trees off-site to the north in the grounds of Nazareth House
  - Advises that landscape treatment to the North Road frontage is important and should focus on the interception of pollutants and visual buffering and provides planting detailed planting advice
  - Advises that landscape details should be preferably provided upfront and sets out the submission requirements.
- 5.7 In respect of the September amended plans submission, the Council's Tree Officer:
- Advises that the RPA of the off-site 'B' category tree group 4 would be impinged upon slightly by the development, including the planting of trees and hedging, but would not result in unacceptable harm to trees of amenity value so long as the development is undertaken in accordance with an approved Arboricultural Method Statement and Tree Protection Plan secured via a discharge of pre-commencement condition.
  - Welcomes the indicative landscaping showing on the block plan, but wishes to see the landscaping detail upfront.
- 5.8 The Tree Officer confirms they have no additional observations to make in respect of the November amended plans submission.
- 5.9 The **County Ecologist** advises that the site previously supported a number of trees, which would have provided habitat for, amongst other, nesting birds and perhaps roosting bats. Noting that almost all of the vegetation has been removed, they support the comments made by the Tree Officer in respect of the importance of green infrastructure along the site frontage and also advise of the Council's duty under the Environment (Wales) Act 2016 to maintain and enhance biodiversity, and sets out a requirement for the provision of bat and bird boxes, to be secured by condition. They note that features which are integrated into the buildings rather than attached to the outside are preferable as they are more secure in the long-term and less prone to interference by the public.
- 5.10 In respect of the initial submission, **Transport** advise:

- *The site is proposed to have one car parking space located to the east of the building (with a turning area) that is accessed via the narrow (unadopted) lane that runs north from North Road Lane. The swept path assessment using a VW Touran indicates that it is a very tight squeeze between the wall and the garage, and one of the track drawings appears to show over-running of some of the lines. It is therefore requested that further swept path assessment is undertaken on a topo base, clearly indicating what each line (of relevance) represents. In any case though, the submitted swept path plans effectively indicate a limit to the size of vehicle that can access the parking area, and noting this is not an especially large vehicle.*
- *Further details should be provided as to who/what the parking space will be for. The DAS indicates it will 'allow for general maintenance and deliveries', although as a result of the constraints it is considered that no delivery vehicles would realistically be able to access it and probably very few 'maintenance' vehicles. Whilst we are acceptable to the provision of one space in principle there does need to be clarity as to who it is intended for. In any case, a suitable planning condition would be sought, referencing that a maximum of one vehicle can park in the rear area.*
- *There are no details on Construction Traffic Management. Given the location of the site right next to the busy North Road, and noting the narrow lane referenced above, we need to understand in basic terms what the construction traffic/access plans will be during the build process, in order to get to a position that we can be satisfied that the development would not have an undue effect on the network. Subject to reviewing this and being broadly content, further details can follow via a CMP condition.*
- *Further details should also be provided on Refuse, Servicing/Deliveries and Student Access, and a key issue would be how the arrival of students at the start of term would be managed, and given the lack of easy waiting locations, this would probably need to be dealt with via a strict time booking system. It may be appropriate for some of the car park on the west side of North Rd to be booked out and for students/parents to park their cars there and walk with their belongings to the apartment from there.*
- *The refuse storage area is a considerable distance from North Road, and we would not want a situation where the relatively large distance means that bins might be left elsewhere within footway access areas, or that generally wheeling bins out to North Road would cause any problems for pedestrians. Further details should be provided on how the refuse process will work and who will be responsible for moving the bins.*
- *The proposed development is for 17 rooms, and thus the proposed number of cycle parking spaces (11) is acceptable. However, it is not apparent they can be easily accessed (there may be a number of columns close to where they would be wheeled out, and there is less than 1m clearance between the cycle store doors and the parking space), or exactly what type of parking is proposed. However, spacings between the stands appear tight (less than the 0.5m minimum we require) and no covered Sheffield stand is provided, that would offer some flexibility for different bike types. Additional details are required.*
- *Depending on the details provided regarding construction traffic, servicing and deliveries it may be appropriate for a Section 106 contribution to cover the potential for additional money for extended parking restriction hours,*

*and it is likely we would require resurfacing of the footway on the North Rd frontage.*

5.11 In respect of the September amended plans submission, Transportation advise that:

- Tracking plans 1.1.04 and 05 have missing notes/ vehicle information, and 04/05 and 01/03 the same drawing titles.
- the following details – previously requested – still need addressing – the ability of the rear lane to be used for access (whether refuse, constriction, maintenance) with regards other properties access off the lane, the purpose/rationale of the one parking space being provided, the little detail as to how student access would be managed, the need for the exact type of cycle parking proposed including security, spacing.
- a clean copy of the base topo is needed, as the details are overlain by tracking and block plan details and hard to read
- all the information will be fully reviewed on submission of additional plans.

5.12 Following the November submission, Transportation advise:

#### *Site Access*

*The site is shown to have one car parking space in a rear area to the east of the building, and be accessed via North Lane (the northern unadopted cul-de-sac leading up to the site). Whilst there is a relative 'pinch-point' between the stone wall and the brick wall of the garage as the rear lane bends left into the site, revised swept path assessment is indicating that some vehicles (including an Estate Car) can fit through the gap, and turn in the rear area and access the car parking space, although it is not clear which larger vehicles cannot make the movement, but any maintenance/service vehicles that need to access the space would obviously have to be small enough to make the required movement. The revised tracks have generally been undertaken with the wing mirrors out (as requested) although this may not be the case for the 4X4 vehicle. The drawings do now show the sliding gate that is proposed, although ideally some additional details (including a manufacturer spec) would help to clarify exactly what is proposed. A Technical Note (TN) has been submitted to support the application and this indicates that the car parking space is available for service/maintenance use and would not be available for use by students.*

#### *Refuse Access*

*Cardiff Council Waste section originally indicated that smaller 18 Tonne refuse vehicles would reverse part-way up North Lane. However, it is now understood they would not support this and would only stop refuse vehicles further down North Lane at the limit of adopted highway. An alternative approach to refuse could be for the site to use a private refuse arrangement. Further details on the proposed refuse strategy should be supplied.*

#### *Service/delivery vehicles/taxis*

*These could expected to comprise general delivery vehicles such as food shopping and Amazon deliveries. The swept path assessment indicates that a 7.5T box van could reverse most of the way up the lane. This is not necessarily an easy manoeuvre or one that would be taken by delivery vans depending on the information they have to hand. The alternative would then be to deliver via*



stopping on North Road, although this is not mentioned in the TN. Further details in relation to this are made in the section below. Taxi access has not been mentioned in the TN. Some taxis would be able to use the rear lane and turn, although they might not actually do this depending on their knowledge of the area/what they are told by their passenger. Otherwise it is assumed that taxis would stop on North Road. The TN does not include information on the likely number of these vehicles that are anticipated.

#### *Emergency access*

Swept path assessment has shown an ambulance and fire tender could reverse most of the way up the lane. In practice, it is considered that in emergencies such vehicles would just park up on North Road, and this is not considered to be a problem considering the likely low frequency when this would occur.

#### *Construction traffic*

The TN indicates that a concrete mixer, crane and skip vehicle could access the top of the lane by reversing up it (or down). This would need to be controlled via the use of a banksman. It may be necessary for other large construction vehicles (e.g. excavator) to access the site, and these would need to use North Road and utilise a temporary lane closure. This is likely to be acceptable in principle, and further details on this could be set out via the CEMP condition that would be placed on any permission.

#### *Student access at start of term*

Students will need to be able to access the site at the start of term with their belongings. This could be facilitated via the use of the rear area (including parking space), with slots to control that only one student can be using the parking space at a time. This would need to be managed on the chosen weekend via use of staff to control matters, and these could also direct cars to the adjacent North Road car park if any slots are running late. Students would need to be fully informed of the booking-in process beforehand, including advising that certain larger vehicles may not be able to access the rear area. These matters would need to be set out fully in a Student Travel Management document which would be conditioned.

#### *Legal status of North Lane*

The northern stub of North Lane is an unadopted road. However there is a parking restrictions traffic order on it, originally dating from the 1970's, but which was consolidated around 10 years ago. Whilst the lane has signing and lining (albeit faded) relating to single yellows the actual order wording refers to 'at any time' (i.e. double yellow lines). There is therefore a discrepancy in the order and it is not legally enforceable. In any case, even if there was not this discrepancy, Cardiff Council would not normally enforce parking on private roads without the permission of the landowner, and would not do so given the current surfacing condition of the road. At the next time the orders are reviewed they would likely seek to amend the order so as the parking could theoretically be enforced. Looking on Land Registry it appears that the lane is not registered. Do we know if any of the nearby residents claim ownership?

*In the absence of any mechanism for the Council to enforce parking, should*

*any vehicle obstruction occur on the lane it would technically be a police matter, who would also need to take a view as to whether any obstruction was taking place.*

#### *North Road parking/loading*

*In the vicinity of the site North Road has double yellow parking restrictions, meaning that no waiting can take place, and additional No Loading plates, meaning that loading cannot take place on Monday – Saturday between 0745-0915 and 1600-1800 hours. Whilst the exact number of additional traffic movements has not been confirmed, and there is some uncertainty as to exactly how much of the service/taxi traffic etc. would use North Road or North Lane it is apparent that there will be some increase in the use of North Road as a result of this development. Outside the site North Road is within the limits of a tidal lane traffic control scheme, whereby three lanes in total are provided, but the direction of the 3rd lane switches depending on the time of day. There is a particular concern that when the tidal flow is showing 2 lanes outbound, that only leaves one lane in the inbound (southbound) direction, and this could technically be utilised at times by a vehicle associated with the new development. We would therefore seek an extension of the No Loading period, at least to cover the daytime period when the tidal system has only one lane southbound. This would require a Section 106 contribution to deal with the TRO process, and the details of time periods etc. would need to be determined.*

#### *Cycle Parking*

*This has been revised and a 2-tier system is now proposed which will provide capacity for 16 cycles, which is in excess of the minimum of 9 spaces that are required according to the SPG. Whilst a cycle parking drawing is included in the TN there is no scale on it, and I am unclear as to what the mechanism is to access the upper tiers. As a general rule we like to see a variety in the type of cycle parking provision, and would ideally want a 2m clear space in front of the cycles, which may not be achievable. Also, it is not apparent that the store would have a secure door/gate? The horizontal spacings appear to be acceptable. Further details should be supplied.*

*Whilst a number of the points previously raised have now been addressed there are some outstanding points (as above) that would ideally be clarified and set out in a revised version of the TN. This should also include the swept path drawings (and commentary on them) that have been separately submitted.*

5.13 The following further advice was provided by way of clarification:

- *Given the potential safety concerns arising from vehicles associated with the development (e.g. deliveries) stopping/loading on North Road we would not be in a position to support the development, as the TRO referred to below cannot be guaranteed to happen. The site has consent for a single dwelling and thus theoretically there could be a 17-fold increase in delivery/service/other vehicular movements on North Road, in comparison with the consented situation. In the absence of detailed/agreed information on realistic amounts of traffic that could end up using North Road adjacent to the site it would seem reasonable to take this position to safeguard potential safety/traffic concerns.*

- *The rear lane access into the site parking area involves a tight manoeuvre which could not be achieved in practice by vehicles much bigger than a Large Car (exact limit of vehicles that can access the area has not been determined), and which a number of drivers may have reluctance to do in practice. In addition it is around 60m from the start of the unadopted lane to near the garage, a distance over which any larger vehicles (not able to turn in the rear area) would have to reverse. This is a significant length, above the maximum reversing distance recommended for refuse vehicles and emergency vehicles.*
- *We are not convinced of the extent to which certain vehicle types would in reality use North Lane instead of North Road e.g. delivery drivers that have not visited the site before (they could well simply follow whatever route their sat nav shows), taxis dropping off passengers.*
- *Access to the site for wheelchair users may be difficult. If they were dropped off by vehicle it would either have to be small enough to access the rear area, or make an unsatisfactory stop on North Road.*

*I would suggest grounds for objection are:*

- *Concern that the rear lane access into site is a difficult manoeuvre and consequently only a small number of vehicle types could actually turn in the site. Other vehicles would need to reverse up to 60m on the lane.*
- *As a result of the difficulties in accessing the lane there is likely to be use of North Road by various vehicles (e.g. deliveries), and this has safety implications due to the nature of the road and it being within the tidal flow section adjacent to the site.*

*The cycle parking, student management and construction traffic matters could potentially dealt with via condition at appeal.*

5.14 In respect of the initial submission, **Waste Management:**

- Advise that that in order to find a solution for the storage and collection of waste and recycling, it has been agreed with the Waste Collections Team that a smaller 18 tonne vehicle could be used to service the development, which could drive into the lane and reverse to the proposed development.
- Advise the Applicant that that there could be the potential for missed collections should undisciplined parking on the rear access lane result in obstruction to the refuse vehicle, and that if this becomes problematic a collection point near the land entrance would need to be considered
- Provide details of the recommended refuse storage provision requirements for the development and detailed advice on the design of communal bin stores and their access requirements, and the provision of bulk containers by the developer
- Advise that refuse storage must be retained for future use.

5.15 Further to a query from the case officer, Waste Management confirmed:

- In the event of the rear lane being blocked, it would be down to the property managers to move the bins to an alternative bin collection point if necessary or alternatively arrange a private collection of waste.

- If the lane is blocked by cars the refuse lorry would not be able to reverse the vehicle to within 25m of the development and waste would not be collected, noting that crews are not expected to pull the bins further than 25m.
- 5.16 Following the November submission and a change of management, Waste Management confirmed they have concerns regarding collecting from this location, advising that it is not normal practice to collect from un-adopted lanes, and given the overgrown trees, the broken surface of the lane, and the inability to enforce parking restrictions, they would be unable to provide a safe and reliable collection service from this location. They note that bins could be collected from the end of the lane (to the rear of the Christian Science Church), where the road is adopted. They note that alternatively collection could potentially be arranged via a private contractor, who may have more appropriate vehicles, and/or less stringent health and safety requirements.
- 5.17 In respect of the initial submission, **Parks** request a financial contribution towards the provision of new open space or the design, improvement and/or maintenance of existing open space of **£9,291**. They also confirm that have introducing screening between the property and North Road would be beneficial and agree with the Tree Officer's comments.
- 5.18 **Drainage Services** provide their standard response to applications subject to surface water drainage proposals under the SAB application process, noting that the applicant has provided insufficient or conflicting detail relating to either flood risk or surface water drainage proposals, such that it is difficult to assess the potential impact of the proposed development in respect of flood risk management matters. They advise that they would expect a drainage strategy as a minimum advising how they propose to dispose of the surface water from the site and also a site investigation report to prove/disprove the viability of infiltration methods. They recommend a decision is deferred until all relevant details are provided, but advise that if the LPA are minded to grant planning permission, a series of drainage conditions are recommended to require a detailed drainage scheme for the site based on a site investigation report into the potential for infiltration.

## 6. **EXTERNAL CONSULTEE RESPONSES**

- 6.1 **Cadw** advise that the development will have no impact on the setting of the registered Cardiff Castle and Bute Park historic park and garden. In providing this advice, they note the proposal is to build a traditional style three-storey building with a slate roof similar in style to other buildings in the Cathays Park Conservation Area in which it will be located, that the part of the registered historic park closest to the proposed development is now in use as a car park which has altered it from its historic use, and that – whilst the proposal will add a new building into the surroundings of the historic park - its design is similar to other buildings in the area.

- 6.2 **The Glamorgan / Welsh Historic Gardens Trust** advises that it is a very uninteresting design, that it does not enhance the Conservation Area and is not really a suitable use of the site given its location and neighbours.
- 6.3 **South Wales Police** provide a range of design advice to address community and crime prevention.
- 6.4 **Dwr Cymru Welsh Water** note the intention to drain both foul and surface water to the mains sewer, that the application is subject to a sustainable drainage review by the SAB and that as they have not had sight of any application to date they cannot assume that a mains sewer connection is the most sustainable option and conclude that they are unable to support the application in full at this stage. They also note that no details have been provided to show how the proposed development will connect to the existing public sewer network, noting that the closest public sewer in North Road is a 975x600mm brick work to which they would not accept any new direct connections into and urge early engagement to develop a suitable foul and surface water drainage strategy for the site. Notwithstanding the above, DCWW request drainage conditions and advisory notices are attached to any permission if the LPA is minded to grant planning permission.
- 6.5 Further to a query from the case officer as to whether they are satisfied that a drainage connection is possible, in principle, DCWW advised that they are content the sewer has the required capacity but that, in the first instance, they would not support a new direct connection to the strategic asset due to health and safety and the risk of damage due to their age. They note that there are options to find alternative options and survey work could locate a suitable alternative, confirming that a connection onto the brick work sewer is possible, but that the logistics and cost are usually unviable, such that they ask for all alternative options to be investigated first. They note that there are often unmapped sewers that could be located which could form a connection.

## 7. **REPRESENTATIONS**

- 7.1 The application was advertised by means of press and site notices, and neighbour consultation letters.
- 7.2 8 letters of objection have been received from owners/occupiers of the following addresses in respect of the **initial consultation**: 13, 14 (X2), 20, 29 Queen Anne Square, Chairman of the Queen Anne Square Management Company Ltd, Nazareth House and 19 St Angela Road, Heath, summarised below:

### Use

- Lack of need for more student accommodation
- Concern that the development if built would not be needed or occupied, taking into consideration the overprovision of purpose built private student accommodation in the city, the fact that other student developments in more suitable locations have failed, and the constraints of this development, including its unsuitable location, North Road frontage, narrow access and limited parking. This could result in the developers

applying for change of use soon after completion, with the concern that this development would not be suitable for any alternative purpose and risks becoming a 'white elephant' if approved.

#### Residential Amenity

- Overbearing impact on the occupiers of 14 Queen Anne Sq
- Overlooking into the funeral home, convent and garden of no 14 and would be clearly visible from within Queen Anne Square
- Harm to the use and amenity of 14 Queen Anne Sq as a result of loss of privacy from overlooking into rear garden, bedrooms and bathroom
- Increased height of proposed development compared to the previous approved application for two storey dwelling, resulting in significant overshadowing of the garden and rear property at 14 Queen Anne Sq due to SW facing aspect
- Harm to the amenity, peace and tranquillity of Queen Anne Sq residents, Nazareth House care home (particularly to residents recovering from illness who use the adjacent gardens for convalescence) and Rosemount Funeral Home (including users of the Quiet Rooms and the Chapel of Rest) from adverse noise impact, particularly during summer months when windows will be open. Residents note they already have to the issue of all night parties going on from the houses on Column Rd, with up to 8 students in them, compared to the 17 proposed here.
- Increased in antisocial behaviour and disturbance from students and their 'drunken ill-thought through activities' such as garden trespass
- Increase in crime and disorder, whether by the students themselves or as targets of crime, noting vandalism and thefts in Queen Anne Square and two stabbings in the Cathays Area, one by a student
- Noise pollution from construction
- Increase in littering and fly tipping, similar to the streets in Cathays

#### Access and Highway matters

- Unsuitability of the site entrance at only 2.25m wide and positioned at a sharp angle to the lane, reducing the effective width of the access, such that only the smallest of city cars will be able to enter the site. It would not be possible for a delivery van, removals lorry, heavy construction equipment, refuse vehicle or emergency vehicle to access the site.
- Objectors challenge whether the manoeuvre shown on the Swept Path Analysis for a 4.5m Volkswagen Touran could be carried out safely in reality. There is a risk that vehicles would strike the garage at 14 Queen Anne Square and/or the boundary wall of the Funeral Home.
- The developers have erected a wooden fence panel, which deliberately masks the compromised access from view. The developers should be instructed to remove this prior to any site inspection. The fence has been erected in the lane, outside the land included in the site's title deed, which make the entrance appear wider than it actually is.
- Unsuitability of vehicular access to the site, via a one-track, unadopted lane off North Road, the busiest road into the City Centre, leading to increased traffic and congestion.

- The double yellow lines are painted only up to and access the lane to the rear of 12 Queen Anne Square, as the owner had at the time installed a gate across the lane. The owners have removed the gate but have used the area beyond the yellow lines for their own parking and composting.
- A single car parking space is proposed on site, leading to students and their visitors' parking cars in the narrow access lane and restricting access further and overspill parking into Queen Anne Square, which is private and would reduce access for residents
- Unsuitability of vehicular access given Queen Anne Square residents regularly park at the rear entrances to their properties in the lane and have garages, which open directly into the lane and noting that there is very limited room for two vehicles to pass each other. Nos 9 – 14 inclusive have rear gates and /or garages onto this lane and a legal right to use it.
- The Ambulance Service, Fire Service and the Council's Highways and Waste Management teams should be asked to assess the access
- The change of use of the lane is a potentially serious security risk for Armed Forces officers and their families who occupy nearby houses and the MoD, South Wales Police and South Wales counter-terrorism authorities should be asked to assess the access
- Road safety impact, noting that turning into and out of the access lane – particularly turning right into North Road - is difficult and potentially dangerous especially at peak times
- Traffic congestion and harm to road safety from delivery, service and emergency vehicles having to park on the southbound carriageway of North Road, exacerbated by a 'blind curved section of the road'. Concern that traffic may be tempted to try and pass stationary vehicles here even if the North Road lane control signals are showing the red X to prevent this, risking a head on collision with approaching traffic travelling north.
- A realistic assessment is required as to whether a small overgrown lane is suitable and whether the dangerous entrance to/from a lane-controlled contraflow section with stopping/waiting restrictions, sitting shortly after a curve, on the busiest road into the City Centre is actually appropriate.
- If visitors, taxis, deliveries etc attempt to stop outside on North Road, there will be implications for traffic flow which could cause serious back-ups during rush hour. Any suggestion of deliveries using the parking area opposite are impractical due to the distance from any safe crossing point.
- Detrimental impact on traffic flow from road closures during construction
- The site is unique in being the only property on the contraflow section of North Road not to have either multiple parking spaces to the rear, or a suitable side road in close proximity which allows free short term parking.
- It is noted that No Right Turn restrictions have been imposed on other turnings to/from North Road into entrances which are far less concealed, and that the Council has erected enforcement cameras in light of the risks.
- At the time outline planning permission was first granted in 1993 Planning Committee acknowledged that due to the busy nature of North Road vehicular access to the plot from north Road would be inappropriate and potentially hazardous and it was deemed that use of the rear lane for one additional property would not be unreasonably detrimental to the amenities and residents in QAS providing that adequate provision for parking and

vehicular turning could be accommodated within the site boundary. The Management Company and other residents had not previously objected to the proposals relying on Planning Committee's assessment, as reiterated 10/04/2002 that use of the lane to access one additional property would not be unreasonably detrimental to residents. The owners of 13 Queen Anne Square objected to a renewal of the outline permission on grounds of the unsuitability of the access, with a request that Highways Dept visit the site but they do not believe that such a visit was carried out.

- The assessment should take into account the servicing demands of 17 students units, with modern day needs for regular access to the site for takeaway food and online shopping/ parcel deliveries, visitors etc, compared to the single dwelling permitted at outline

#### Heritage and Placemaking matters

- This is another unwanted blight on the centre, focussed on assisting developers to impact on the historic features of the city and if permitted will be used to permit further destruction of the area
- The development is totally out of character with the surrounding area / rest of Queen Anne Square, which falls within the Cathays Park Conservation Area with the application site at one point forming part of the Queen Anne Square estate.
- Adverse noise effect on the peace and serenity of the Conservation Area
- The development is incompatible with Queen Anne Square by virtue of its intensification of use
- The application is a 'far departure' from the outline planning permission for a two storey single private dwelling that exists on the site.

#### Environmental and other matters

- Air quality impact of the change of use from a single development to a commercial development with parking, with the need to consult the Council's Environmental Strategy department
- All of the trees on the site have been felled in contravention of the planning permission, with consequent increase in air and noise pollution for neighbouring properties
- The residents of other properties in Queen Anne Square bar no 13 only learnt of the application through their local Councillors who had delivered a leaflet to each house advising them of the application

7.3 6 letters of objection have been received from the owners/occupiers of 9, 13, 14 and 29 Queen Anne Square, Nazareth House and the Queen Anne Square Management Company following consultation on the **September amended plans consultation**, summarised below. Many of the same objections are raised and these are not re-stated.

- The lounge/dining area has insufficient seating for 17 students. There needs to be sufficient communal space to ensure that students are not left alone in their little rooms given the issues surrounding mental health and suicide.



- Failure of the development could lead to the property being empty or being used as a HMA, reducing the security of the area and noting the number of military personnel living nearby.
- The land should be used constructively and in accord with its location adjacent to North Rd and within the Conservation Area, but multiple-occupancy should not be the immediate solution.
- Questions whether the swept-path analysis documents are correct, noting that the swept path for the Volkswagen Touran excludes wing mirrors and door panels/wheel arches and that the actual vehicle width including mirrors is 2.087m –wider than the site entrance. Even so, the drawings indicate that the vehicle is touching the walls.
- There are no measurements given for swept path analysis drawings for the larger estate cars. Service vehicles tend to be small vans, which would be wider still.
- The vertical plane of the entrance walls is not straight, being of rough stone construction, such that the true width of the access may be narrower than that shown
- the objector attempted to drive onto the site in their MBW x1 (the smallest of the range with a width of 2.06m with mirrors extended), which proved impossible
- a site visit must be undertaken by Highways and Planning, requiring the removal of the fence erected across the lane, which gives a misleading impression of the width of the entrance
- questions whether it is acceptable to accept that the site cannot be included on general waste collection, such that the Council would make a special vehicle available leading to inefficiencies and adding to the Council's costs
- confirms residents do regularly park in the rear lane, which would block access to a refuse vehicle and lead to refuse operatives having to drag a large number of bins potentially over a distance of approx 110m, noting that the right to park in the lane is long established
- it is just possible for a non-SUV-sized car to pass another similarly small parked car in the lane, but nothing larger
- bins would have to be dragged 20m even if a refuse vehicle has reversed to the end of the lane, adding to time and costs.
- The refuse store is immediately adjacent to the gardens of no 14 Queen Anne Square and Nazareth House, with concerns raised about the smell from refuse within their garden and home when the windows are open. Even if the building is fully enclosed it will be likely that the students would leave the door open. If a refuse collection is missed, as it's a possibility due to site access constraints, the risk of smell would be exacerbated particularly if the refuse store is full and bags are placed outside.
- The amended plans do not address the very serious issue of emergency vehicle access
- Complete absence of local amenities, with corresponding need for frequent and multiple grocery and other deliveries, taxi drop-offs etc

7.4 6 letters of objection have been received from the owners/occupiers 13 and 29 Queen Anne Square, 19 St Angela Road, Priest in Charge-The City Parish of St John The Baptist and the Chairman of the Queen Anne Square Management

company following consultation on the **November amended plans consultation**, summarised below. Many of the same objections are raised and previous objections resubmitted, and these are not re-stated.

- The amended submission does not address the main issues
- The swept path analysis drawings do not show anything different in relation to the site's severely restricted access.
- support for the construction of a single family dwelling as previously approved for the site, noting this would be the only type of development that could be appropriate for this site.
- Concerns over the practicality of the proposals for student access at the start and end of term, with allocated time slots, with students arriving from all over the UK and beyond, with questions as to where they would park whilst they wait given waiting in the lane would restrict access to other arrivals.
- Question as to how the sliding gate would work given the restricted access, how it would be operated and how arrivals of taxi and delivery vehicles would be controlled.
- Access to emergency, refuse and service vehicles concerns – swept paths analyses for ambulance, fire engine and 8m box van do not show manoeuvring on site, as per the title, as none access into the site. Note the risk to safety, as fire engines and ambulances would not be able to access a point adjacent to the site if the lane is blocked and would have to operate from a distance from 110m. The swept path for the 4x4 vehicle is questioned given the compromised access into the site. Photographs of the lane are submitted showing the access and parked vehicles.

7.5 Local Ward Members Cllr Merry, Cllr Weaver and Cllr Mackie object to the application as **originally submitted** on the following grounds:

- *We wish to object to this application and can only echo the comments made in the objection by the resident's committee – apart from clarifying that we always opposed the original application to convert Park Place to student accommodation!*
- *As stated in the committee's objection letter there has been a consistent pattern of failed student accommodation developments in recent months and we understand that there was a survey of the market submitted as part of an application in the Plasnewydd ward. This may not be a material consideration in terms of this planning decision but if we want sustainable development in our city we must consider whether building could be converted to an alternative use in line with our SPG on student housing. We do not, for example think that the flats would meet our flat planning guidance and once built we would be under pressure to allow a conversion to avoid an empty building. This application is similar to that on Park Place and it would seem an even less desirable location as while it would be attractive facing on to the park there are no facilities close by.*
- *The committee has thoroughly analysed the access to the property in their objection and we would support their comments. We have been out to visit the site and can confirm the access is almost impossibly tight as recognised by the earlier council decision on a previous application which as a result asked for a turning circle. We believe that even with a turning circle within the site that accessing the site itself would be extremely difficult if not*

*impossible. We recognise that the developer will argue that it is a “sustainable location” but occupiers will undoubtedly want car access at drop off, pick up times and do not see how this could be safely achieved. Ditto taxis - taxis late at night are a regular complaint at other student halls, taxis can hardly safely drop off on North Road and any suggestion that the taxis could stop on the lane must be firmly rebutted for the safety of future residents particularly vulnerable female students. As there are no shops close by it is undoubtedly the case that students would use food delivery services and how could they make deliveries without causing chaos and blocking the lane? We would also have concerns in terms of emergency vehicle access – we believe that it would be impossible for a fire engine to access the site. We would also ask waste to consider how waste could be removed in view of the limited access.*

- We would like to be clear that we believe it is essential not only that this application goes to planning committee but that the committee should visit the site to see it for themselves. The wood panelling blocking the access is wider than the access itself so the pictures give a misleading impression of how tight the access is.*
- We also do not believe that the application responds to its location within a conservation area. The building is significantly taller than the funeral home which is adjacent and is an elegant lower building that is grade two listed. It would be sacrilege to place this mock gothic building next to it. We would also have concerns about the appropriateness of some of the materials in view of the location including the windows. Finally of course the character of a conservation is wider than the buildings themselves but their use. As this site is located opposite one of our great civic parks, Nazareth House (a residential care home), backing on to Queen Anne Square and next to a funeral home we would argue it would be impossible to claim that this is in character with this part of the conservation area.*

7.6 In response to the **September amended plans consultation**, Local Ward Members Cllr Merry, Cllr Weaver and Cllr Mackie raised various queries over the plans and requested additional information, also raising concerns:

- that students would be likely to have shopping and food deliveries, leading to vans parking on the pavement on North Road or blocking the rear lane while unload.
- That the Manual for Streets sets out Building Regulation B5 (2000) requirements in respect of Access and Facilities for the Fire Service, with Section 17 ‘Vehicle Access’ providing that advice providing that there should be a minimum carriageway width of 3.7 m between kerbs; there should be a minimum gateway width of 3.1 m; there should be vehicle access for a pump appliance to within 45 m of every point within single family houses; and that fire service vehicles should not have to reverse more than 20 m.”
- They note that this is not a family home which raises even greater concerns and that there is no way that the rear access would meet these guidelines, asking whether the Council routinely consult the fire service about applications for flats and whether we would be accepting that North Road would be blocked.

- They confirm their concerns relate not simply to this application but the wider issue, noting the concerns there have been about fires in flats since Grenfell.

7.7 This was followed up by a further objection:

- *We would like to support the comments made in this objection and we have raised similar questions about access. We do not accept that the access is wide enough or safe enough for the vehicles that will need to get on to the site with students arriving, deliveries from multiple sources and taxis. This is of particular concern when you think of the potential of female students being dropped off in the rear lane either because the taxi driver is unable or unwilling to drive through such a narrow gateway. The access is also narrower than that set by building regulations for fire engines. We also note that the developer in response to concerns about the start of term suggests there will not be problems as the accommodation is furnished so students will not need to bring much – but furnishings still have to get into the building somehow! Is the developer suggesting that like the waste lorries larger vans can park in the lane? The lane is access for other properties and this could cause repeated issues.*
- *We suspect too that delivery drivers (such as supermarket and online shopping deliveries) will be tempted to chance a ticket and park at the front of the property, they may not even find the rear access, causing traffic issues on a main arterial road into Cardiff – the A470. There are no food shops close to the development so there are likely to be multiple deliveries as the students are living independently of each other.*
- *We therefore confirm that this additional information has done nothing to address the concerns raised in our original objection which still stands.*
- *We will also be requesting a site visit so that members of the planning committee can see just how poor the access is.*

7.8 Following the **November** submission, Cllr Mackie asks if the Fire Service was consulted on access to the development and asks for their assessment of the access for fire engines.

7.9 Following the November submission, Cllr Merry writing on behalf of herself, Cllr Weaver and Cllr Mackie:

- Confirm they continue to object to this planning application despite the additional information submitted
- Advise that if access is so tight that on a traffic sweep they feel the need to show the wing mirrors folded in then in all likelihood (even if access is actually possible) delivery vans, taxis etc will be tempted to stop on North Road with resulting traffic chaos. Alternatively with taxis in particular dropping off late at night they worry about them insisting on passengers getting out in the lane putting them at risk.
- Confirm their concern about access for emergency vehicles and request that it be noted that the lane is access for several properties so it should not be presumed that vehicles can park up in the lane without impacting on others.

7.10 Jenny Rathbone AM objects to the application as **originally submitted** on the following grounds:

- *I wish to object to this application and want to echo the comments made by the resident's committee and local councillors in their submissions.*
- *Lately, there has been an increasing pattern of failed developments supposedly earmarked as student accommodation. I have concerns that this pattern may continue with this development and that any alterations from its intended purpose could have serious knock-on effects and may not meet the Council's guidelines on converted student housing. It is concerning that the Council may be forced to make an uncomfortable decision on allowing conversion in order to avoid an empty building if the proposed student development falls through.*
- *Access to the building on the proposed site is incredibly tight, which will cause very serious issues for residents with mobility problems in addition to access problems for public or personal transport. Occupiers will wish to have car access at school drop off and pick up times, therefore it seems that even the solution of a turning circle would prove insufficient to deal with the saturation of vehicles at peak times.*
- *The safety of students is paramount, I am therefore incredibly concerned about taxis who will be dropping off students, particularly female students, being able to access the site and perform safe drop offs. It is not safe for taxis to perform drop offs on North Road, and suggestions that students could be dropped off in the lane is unacceptable.*
- *A lack of local amenities means that students will also be using food delivery services, which would cause chaos as these services attempt to drop off to student accommodation, blocking the roads. Other local services, such as the emergency services and waste collection services would also find it incredibly difficult to access the site.*
- *I note that that the building is significantly taller than the lower, grade-two listed building which currently houses a funeral parlour next door. In addition, the building is located close to a residential care home. Given the sensitive nature of both these buildings, I do not believe student accommodation fits the local aesthetic*
- *I urge that this application goes to committee and the committee a site visit is organised in order to see for themselves how unsuitable this site is for the intended purpose.*

7.11 Jenny Rathbone AM objects to the application as amended in **September** on the following grounds, reiterating previous objections and noting that the revised plans still do not alleviate the concerns raised in their initial objection from July. They note that local services, such as the emergency services and waste collection services would also find it incredibly difficult to access the site, even under the revised plans.

7.12 Jenny Rathbone AM objects to the application as amended in **November**, noting the revised plans do not alleviate their concerns raised in July and September.

7.13 Jo Stevens MP objects to the application on the following grounds:

- The need to undertake a site visit to view the site and means of access to it
- The historic planning approval for a single dwelling with in-built parking and turning is materially different, with the new proposal presenting very different access issues given the transient nature of the student tenants
- Road safety risk to vehicles and pedestrians from traffic entering and exiting the narrow lane due to the 'extremely concealed' access point onto North Road and requiring a right turn manoeuvre for access to North Road going north
- Taxi drivers and passengers may not attempt to access the lane
- Narrow vehicle access, at 2.25m wide, which is too narrow for fire appliances and other emergency vehicles, leading to safety concerns
- Many vehicles will not be able to access the site or parking, with manoeuvres carrying a risk to neighbouring properties
- The wall that borders the site on the southern side appears to have recently been demolished, querying the ownership of the wall and impact of its reinstatement
- Impact on road safety from deliveries risking traffic infringements on a major arterial road by delivery vehicles that cannot or do not want to traverse the lane to access the rear of the development
- Questions whether access at the beginning and end of the academic year has been considered
- Lack of alternative use - If the development is unable to be let to full capacity it will remain unused or require permission for conversion to another use
- Gross over-development of the site with access problems that cannot be easily overcome or overcome at all.

## 8. **ANALYSIS**

8.1 The key issues for consideration are:

(i) *The principle of development*

8.2 The site, which previously formed part of the rear garden of no 14 Queen Anne Square, is located within the settlement boundary and the Cathays Park Conservation Area, outside the Central Business Area, and has no specific land use designation or allocation in the LDP. The principle of accommodating a single dwelling on this vacant, brownfield site has been established through the previous planning history, subject to conditions. Whilst purpose-built student accommodation falls within a different Use Class (Sui Generis), the principle of purpose-built student housing in this location is supported in the Council's 'Student Accommodation' SPG. Paragraph 4.3 notes that '*student accommodation is more acceptable (if not in the city centre or Bay) than in sustainable locations either as close to [the major university] campuses as possible, or within them*'. The SPG provides a map showing the location of the various major university campuses and shows the application site as falling within one such campus area. Responding to the objections raised, there is no policy or SPG requirement to consider the need for student accommodation. Taking the above into consideration, it is considered that the principle of student

accommodation on this site is acceptable in land use terms, subject to detailed amenity and other considerations set out below.

*(ii) Design and Impact on Heritage Assets*

- 8.3 The general duty placed on Local Planning Authorities when considering planning applications for buildings located within Conservation Areas is to pay special attention to the desirability of preserving and enhancing the character and appearance of those areas (section 72, Planning (Listed Buildings and Conservation Area) Act 1990). Additionally, in considering whether to grant planning permission for development which affects a listed building or its setting, the local planning authority is required to have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses (Section 66(1) of Planning (Listed Buildings and Conservation Area) Act 1990). Policies KP17 (Built Heritage) and policy EN9 (Conservation of the Historic Environment) provides protection for the full range of assets that make up Cardiff's historic environment. In terms of wider design considerations, policy KP5 (Good Quality and Sustainable Design) requires that all new development should respond 'to the local character and context of the built and landscape setting so that layout, scale, form, massing, height, density, colour, materials, detailing and impact on the built and natural heritage are all addressed within development proposals'. Policy C3 (Community Safety/ Creating Safe Environments) requires the development to be designed to promote and safe and secure environment and minimise opportunities for crime.
- 8.4 As noted in Section 2, there are a number of heritage assets in the vicinity of the application site that require consideration. The site falls within the Cathays Park Conservation Area, and within the setting of the Grade 1 listed registered Historic Park and Garden 'Cardiff Castle and Bute Park' on the opposite side of North Road and adjacent to two listed buildings. The site also lies within approx 0.85k of the Cardiff Castle and Roman Fort Scheduled Monument and approx 0.75km from the Dominican Friary Scheduled Monument.
- 8.5 Overall the scheme is considered to offer good design, to preserve the character and appearance of the Conservation Areas and the setting of the registered historic park and garden, listed buildings and scheduled monuments. In coming to this view, it is noted that the building has been designed as a pair of three storey villas, incorporating a pitched slate roof, scale, form and style to replicate the existing, large late Victorian villas which characterise the North Road section of the Cathays Park Conservation Area. Amended plans were submitted to improve the minor detailing of the building and ensure that it more closely represents those that it is seeking to imitate, with changes secured to the width of the bays, to introduce additional windows at second floor, to remove the Mullion in in the bay windows and introduce architraves and cills into the second floor windows. A scheme of architectural detailing and materials has also been submitted as additional information in response to the need to pay very close attention to architectural detailing and quality in materials.
- 8.6 The proposal is now considered to provide the necessary form, detail and quality that is required to conclude that the Conservation Areas's character and

appearance, and the setting of the identified historic assets would be preserved, subject to minor issues that could be appropriately dealt with by condition:

- the rooflights to the front are not supported and should be omitted. Those to the rear should be conditioned to be the flush conservation style.
- The use of large areas of reconstituted stone within the bay windows is of some concern, as this would need to be very well detailed and appropriately high quality in order to be acceptable. Natural stone would be preferred in this sensitive and prominent location.
- Imperial red bricks should be specified. These are widely available and would better reflect the character of villas that characterise the area. Coursing should be Flemish bond not stretcher bond, using black mortar.
- The plans show inconsistency in the ridge and finial details – on plan MAT 3 they are red clay, on other plans they are grey. They should be red. Use of reclaimed decorative finials would be beneficial, these are widely available from local reclamation yards.
- The insertion of an opening into the front boundary wall is considered acceptable and details should be secured by condition. The sliding gate is acceptable given its rear lane location.
- The provision of a hedge and tree planting within the site is welcomed and will enhance the character and appearance of the Conservation Area. Full and acceptable landscaping details should be secured by condition.

8.7 It should also be noted that Cadw have confirmed they have no objection to the impact of the proposed development on the registered historic parks and garden opposite the site, noting that the proposal is to build a traditional style three-storey building with a slate roof similar in style to other buildings in the Cathays Park Conservation Area in which it will be located. They note that whilst the proposal will add a new building into the surroundings of the historic park, its design is similar to other buildings in the area. The concerns raised by the Glamorgan/ Welsh Historic Gardens Trust that the proposal is an uninteresting design and does not enhance the Conservation Area are noted, but do not justify a refusal of the application. Whilst the design intent of replicating North Road properties may be perceived as uninteresting by some, it is acceptable as an approach, subject to the recommended conditions, and it is noted that the duty in respect of conservation areas is to preserve or enhance the Conservation Area. In concluding that the proposal would not harm the setting of the adjacent listed buildings, it is also noted that the southern elevation of the Rosemount Funeral Home listed villa is the principle elevation, which is set at right angles to North road and that the building is separated from the application site by a single storey, flat roofed outhouse/garage building and which is, itself, attached to a modern, 1 1/2 storey out building. The Grade II listed Nazareth House to the NNW is separated from the application site by a distance of over 100m and trees and landscaping. There would not be any harm to the setting of the Cardiff Castle and Roman Fort Scheduled Monument and the Dominican Friary Scheduled Monument, given their separation distances (0.85km and 0.75km, respectively) and the intervening screening provided by built form and Bute Park.

*(iii) Impact on the amenity of neighbouring occupiers and future occupiers*



- 8.8 Policy KP5 seeks to ensure that 'no undue effect on the amenity of neighbouring occupiers' results from development. The impact on the living conditions of adjoining occupiers has been carefully considered and the objections received noted. The proposal would not cause unacceptable harm to the amenities of adjoining residents and occupiers as a result of overbearing impact, outlook, loss of privacy or light, overshadowing or noise, taking into consideration the proposed building's separation distances between adjoining properties (including approx 22- 25m separation distance from both the rear of 14 Queen Anne Square and the main building of Rosemount Funeral home), the level of screening provided by trees and other planting along the boundary with Nazareth House, the absence of habitable room windows in the elevation facing Nazareth House gardens and the funeral home, the extent of the adjoining gardens at Nazareth House, the height (at approx 10.1m) and orientation of the building, and number of units proposed. The existing window in the garage of No 14 Queen Anne Square would not result in any unacceptable harm to the amenities of the occupiers of no 14 Queen Anne Square by virtue of a loss of privacy from in-looking, noting that it is a garage and not a habitable room.
- 8.9 The amenity of future occupiers would be acceptable, noting that the internal floor area of all units are above the minimum size requirements set out in the Student Accommodation SPG and that all living areas would have an acceptable outlook. There would not be any harm to the amenity of future occupiers from a loss of privacy from overlooking from the garage window of no 14 Queen Anne Square, noting that this serves a garage, the oblique angle involved and that future residents could erect a 2m fence to cover the window. Whilst there is sufficient on site amenity space to provide for refuse and cycle parking, there is very little meaningful space available for sitting out. This would not justify concern for the local planning authority in this instance, noting: that student accommodation is accepted as a distinct form of residential accommodation, that there are no standards for on-site amenity space set out within the student accommodation SPG, the close proximity of Bute Park and that the Applicant has agreed to the requested financial contribution of £9,291 towards off-site public open space.
- 8.10 The Student Accommodation SPG, in paragraph 6.4, states that 'the Council would welcome an Adaptability Statement, which would consider how the development would not prejudice future users, such as for residential or hotel use' and which 'may make reference to room sizes, floor plates, access or amenity issues'. The absence of such a statement is not a concern in this instance, given the scale and form of the proposed development, noting that this is not an application requirement, that any changes to alternative uses would be subject to a full and separate planning application, and given the negative recommendation.
- (iv) *Whether the proposal would make satisfactory provision for access, parking and circulation, and the storage of waste*
- 8.11 A key issue for consideration is whether the Applicant has satisfactorily demonstrated that appropriate access to the site can be achieved, both in the construction and operational phases.

- 8.12 As noted in section 1, pedestrian access is proposed from an amended entrance along the North Road frontage, and vehicular access and refuse collection proposed via the existing rear lane off North Road. 16 cycle spaces are proposed, and a single parking space would be provided for general maintenance and deliveries, and would not be used by students. There is no dedicated provision for disabled parking. The application is supported by a Technical Note and various swept path drawings, details of which are summarised in Section 1.
- 8.13 The site lies in a highly sustainable urban location, well served by public transport and within walking distance of shops and facilities in the City Centre, Cathays Terrace and North Road, and Cardiff University and the RWCMD. It is located on the eastern side of A470 North Road, a major arterial route into the city and a 'Rapid Transit Corridor' as identified on the LDP Constraints Map, one of four principal rapid transit corridors and a strategic bus route that connects Cardiff to the region (policy T2). To help maximise the efficiency of this route, traffic management measures are in place adjacent to the site's North Road frontage, including parking restrictions in the form of double yellow lines and additional 'No Loading' restrictions, and a 3 lane tidal flow control scheme. These measures are described in section 2, with the loading restrictions having the effect of limiting the times that vehicles associated with the development could stop adjacent to the site's pedestrian entrance. Section 2 also highlights that a key time for the flow of traffic, is when the tidal flow sequencing results in 2 outbound lanes and the lane adjacent to the site being the sole inbound lane; in the afternoon between 14:30 – 18:30, and evening/night, between 19:55 – 05:00 Monday to Saturday, with Sundays as above with the exception that there is one lane inbound 16:30 - 18:30. These tidal flow hours do not match the hours of the 'no loading' restrictions adjacent to the site on North Road (07:45-09:15 and 16:00-18:00 Monday to Saturday), such that there are periods when the lane adjacent to the site is the sole inbound lane when vehicles would be permitted to stop on the highway, including all day on Sundays. The physical limitations of the rear lane access are described in detail in Section 2, including the narrow entrance to the site formed between the garage of no 14 Queen Anne Square and boundary wall, which is further constrained by a 45 degree bend in the lane as it approaches the site entrance. The unadopted lane itself is narrow, single lane, uneven and unlit, and not wide enough for 2 larger vehicles to pass. Whilst the lane is subject to parking restrictions, Section 2 notes that the order is not legally enforceable. The presence of a pay and display car park on the opposite side of North Road is noted, albeit that the nearest safe crossing point is approx 45m from the site.
- 8.14 The detailed comments and concerns of the Operational Manager Transportation are provided in Section 5, and encompass site access, refuse access, service/delivery vehicles/ taxi, emergency access, construction traffic, student access at start and end of term, the legal status of North Lane, North Road parking/loading and cycle parking.

- 8.15 The Operational Manager Transportation concludes that they are unable to support the application on grounds of vehicular access, with suggested grounds for objection set out below:
- *Concern that the rear lane access into site is a difficult manoeuvre and consequently only a small number of vehicle types could actually turn in the site. Other vehicles would need to reverse up to 60m on the lane.*
  - *As a result of the difficulties in accessing the lane there is likely to be use of North Road by various vehicles (e.g. deliveries), and this has safety implications due to the nature of the road and it being within the tidal flow section adjacent to the site.*
- 8.16 This position and their detailed comments are supported. Whilst the swept path analyses shows that some cars are theoretically able to access the site and exit in a forward gear, they also show that that the 'Luxury 4 x 4' vehicle requires its wing mirrors to be folded in and that there is little margin for error for the rest, even the mini, with the reality being that some drivers may not be willing to risk damage to their paintwork or the adjacent garage and wall by attempting the manoeuvre. Swept path analyses also show the option of vehicles reversing down the rear lane and this is the only option shown for the 8m box van. However, it is certainly the case that some drivers may be unwilling to reverse the required distance down the lane, such that this reversing option is not a viable alternative. Furthermore, parked vehicles could be blocking the lane, restricting access for larger vehicles further, with the Council unable to enforce the parking restrictions. The Transport Technical Note advises that whilst there is only one car parking space on site, there is sufficient space to accommodate an additional vehicle in the event that there is an unavoidable overlap between vehicles (para 2.4.4). However, this point is not evidenced and Section 2 notes that the lane is overgrown with trees and vegetation in places. Faced with these constraints, there is real concern that certain vehicles would use North Road instead of the lane, where access is also constrained. Whilst letting staff could help manage access at the start and end of term, given the limited period of time and frequency, with drivers of larger vehicles alternatively directed to the public car park opposite, this is far from ideal, given the heavy loads that are likely to be involved. Moreover, it would not be reasonable to impose a condition to require deliveries and taxis to be managed in the same way, with the real concern that this would result in increased use of North Road, to the detriment of road safety. Delivery drivers may not have visited the site before and could simply follow whatever route their sat nav shows and may not be aware of the loading restrictions. The risks to road safety would be increased due to the nature of North Road and it being within the tidal flow section adjacent to the site.
- 8.17 It is noted that the application site benefits from outline planning permission for a single dwelling. However, this permission is not considered to justify a positive recommendation in this instance. Parking standards set out in the Council's SPG would permit a maximum of 1 car parking space for a single dwelling. It should also be noted that the permission was subject to conditions identified in Section 3 of this report, including a requirement that '*details submitted as reserved matters shall include a turning space within the curtilage of the site enabling vehicles to enter and leave the site in a forward gear*'. In

this circumstance, it is considered reasonable to expect that a future resident would choose to drive a car that could fit through their access and that they were happy with the required manoeuvre. However, it is considered that the same assumption could not be reasonably made in respect of drivers accessing the site for a 17-bed student accommodation to drop off students, where residents are temporary and where not all drivers would not be familiar with the site and access. Drivers accessing the site should reasonably expect to be able to access the site and exit safely in a forward gear. It is also noted that the outline permission was first granted in 1993, at a time when trends in food and on-line shopping deliveries were materially different and that it is not unreasonable to expect that there would be an increase in deliveries, compared to the fall-back position. The Agent has referred to the recent permissions at nos 16 – 18 North Road. These permissions are not considered to justify a positive recommendation, in light of the material differences between the applications, noting in particular that the access into the site itself is different, that they are served by a different rear lane, that the lane is adopted highway and that the site does not lie adjacent to the tidal flow section of North Road.

- 8.18 It is agreed that cycle parking, construction traffic (including the use of a banksman), and student management at the start and end of term could be dealt with by condition, should it be concluded at appeal that vehicular access was acceptable. Details of the sliding gate proposed to the rear, with tracking details, could also be required by condition. The use of a Parking and Traffic Management Plan to manage the start and end of term is commonly used for purpose built student accommodation scheme and is even specified as an approach in the Managing Transport Impacts SPG. The OM Transportation has advised that this would need to set out the fact that certain larger vehicles would not be able to access the rear area and require staff to control time slots and direct cars to the adjacent North Road car park if any slots are running late. It could also be required to specify the use of banksmen to assist larger vehicles at student drop off and pick up times. Whilst Waste Management have latterly advised that the Council would not be able to provide a safe and reliable collection service from this location, it is considered that refuse collection could also be addressed by a condition requiring a Waste Management Strategy where collection via a private contractor is a viable option. The location of the refuse store within the site is considered acceptable and Waste Management have not raised any objection. Whilst submitted tracking details show ambulances and fire tenders reversing up the 60m rear lane, which is above the maximum reversing distance recommended for such vehicles, the OM Transportation advises that, in reality, such vehicles would park up on North Road in an emergency, which they advise is not considered to be a problem given the likely low frequency. The emergency lights would help to warn approaching vehicles in such circumstances.
- 8.19 Notwithstanding the possibility of overcoming some concerns by condition, it is considered that vehicular access to the site is unacceptable and sufficient to justify a reason for refusal, as concluded by the OM Transportation. It should be noted that the Agent requested a meeting with a view to clarify the LPA's position and try to overcome the concerns. In response, the Agent was advised of the intention to report the Application to the next committee on the basis that

the grounds for objection are a result of the physical constraints of the rear lane access, the effect of the TROs in place on the rear lane and North Road, the nature of North Road and the location of the tidal flow section of North Road adjacent to the site, which are all factors outside the Applicant's control and which, it was considered, could not be satisfactorily overcome by condition or a third amended or additional information submission.

- 8.20 Taking the above factors into consideration, it is considered that the proposal should be refused on grounds of inadequate vehicular access and its related impact on the road network, contrary to LDP policies KP8, T5 and T6.

*(v) Impact on trees, landscaping, wildlife and habitats*

- 8.21 There would not be an unacceptable impact on the above matters. Following the submission of the requested tree assessment, the Council's Tree Officer confirms that there would not be any harm to any off-site trees of amenity value within the gardens of Nazareth House, subject to conditions to require an Arboricultural Method Statement and Tree Protection Plan. The illustrative landscaping, including hedgerow and new trees, is welcome and full landscaping details could be secured by condition in the event of a positive recommendation. Similarly, the County Ecologist's request for bird and bat boxes could be secured by condition to provide biodiversity enhancements to the site.

*(vi) Impact on contaminated land, air quality and noise*

- 8.22 There would not be any unacceptable impact with respect to the above matters. The Contaminated Land team have no objection, subject to conditions to control ground gas, unforeseen contamination, imported soil and aggregates and advice in respect of contamination and unstable land, which could be attached in the event of a positive determination. The applicant has submitted the requested Air Quality Assessment and Pollution Control have confirmed that they are satisfied, subject to a condition to control dust in the construction phase, in the form of a Construction Environmental Management Plan. Pollution Control (Noise) raise no objection and recommend conditions to require a scheme to protect future occupiers from road traffic noise and a scheme to protect neighbouring occupiers from noise from fixed plant and equipment on site, and an advisory in respect of construction noise, which could be attached in the event of a positive recommendation.

*(vii) Water Resources, Drainage and Flood Risk*

- 8.23 Policies EN10 and EN14 require water sensitive design solutions that do not increase the risk of flooding elsewhere to be incorporated within new development. The site falls within flood zone A and, therefore, outside areas considered at risk from flooding now and in the future. The development would require SuDS for surface water, designed and built in accordance with the Welsh Ministers' Statutory SuDS Standards, which would be required to be approved by the local authority acting in its SuDS Approving Body (SAB) role. It is proposed that surface and foul water would be disposed of via the public sewer, but no further details have been provided. Both DCWW and Drainage Services have advised that details could be secured via an appropriate drainage condition.

*(viii) Other Legal Considerations*

- 8.24 Crime and Disorder Act 1998 - Section 17(1) of the Crime and Disorder Act 1998 imposes a duty on the Local Authority to exercise its various functions with due regard to the likely effect of the exercise of those functions on, and the need to do all that it reasonably can to prevent, crime and disorder in its area. This duty has been considered in the evaluation of this application and it is considered that there would be no significant or unacceptable increase in crime and disorder as a result of the proposed decision. The proposals in respect of controlled entrances and means of enclosure are welcomed. South Wales Police have been consulted and have not objected to the application. The comments of SWP have been noted and could be recommended to the developer as an advisory in the event of a positive determination, noting that the majority of issues raised cannot reasonably be controlled by planning and/or are matters for building control.
- 8.25 Equality Act 2010 - The Equality Act 2010 identifies a number of 'protected characteristics', namely age; disability; gender reassignment; pregnancy and maternity; race; religion or belief; sex; sexual orientation; marriage and civil partnership. The Council's duty under the above Act has been given due consideration in the determination of this application. There is concern that access to the site for wheelchair users may be difficult. If they were dropped off by vehicle, that it would either have to be small enough to access the rear area, make an unsatisfactory stop on North Road or park elsewhere. This forms part of the basis for the recommended reason for refusal.
- 8.26 Well-Being of Future Generations Act 2016 - Section 3 of this Act imposes a duty on public bodies to carry out sustainable development in accordance with the sustainable development principle to act in a manner which seeks to ensure that the needs of the present are met without compromising the ability of future generations to meet their own needs (Section 5). This duty has been considered in the evaluation of this application and it is considered that there would be an unacceptable impact upon the achievement of wellbeing objectives as a result of and relating to the recommended decision, including the contribution towards the Welsh Ministers' well-being objective of building healthier communities and better environments.
- 8.27 The Environment (Wales) Act 2016 - The Environment (Wales) Act 2016 enshrines in law principles and policies for managing natural resources in a sustainable way. Amongst other things, it introduces a new biodiversity duty on public authorities to seek to maintain and enhance biodiversity when exercising their functions, and in so doing to promote the resilience of ecosystems, so far as consistent with the proper exercise of those functions. This duty and the resilience of ecosystems have been considered and discharged in the evaluation of this outline application. In the event of a positive recommendation, a condition could be attached in respect of nesting birds and bats, which has been agreed by the Applicant.
- 8.28 Flood and Water Management Act 2010 – Section 12 (3) of the Flood and Water Management Act 2010 places a duty on risk management authorities (e.g. a

county council for the area) to have regard to the national and local strategies and guidance when exercising any other function in a manner which may affect a flood risk or coastal erosion risk. The relevant strategies and guidance have been taken into consideration in the determination of this application.

*(ix) Response to third party and other objections*

8.29 The objections raised by third parties and other objectors have been duly noted. The following comments are provided in respect of matters not addressed above:

- Fire Service access is a Building Regulations matter (Part B) and the Fire Service are not consulted on planning applications. They are not a statutory consultee and access is considered by colleagues in Transportation and Highways, as the Local Highway Authority. The Fire Service are, however, sent a copy of the application weekly list and so can comment on an application should they wish to do so
- the publicity and consultation met legal requirements, having been advertised by press and site notice, and neighbour letters, both to the immediately adjoining neighbours and also including nos 9 - 14 Queen Anne Square
- neighbour letters are addressed to the owner/occupiers and recipients are advised to tell the owner about the proposal if they are a tenant, or other occupiers if they share the property.

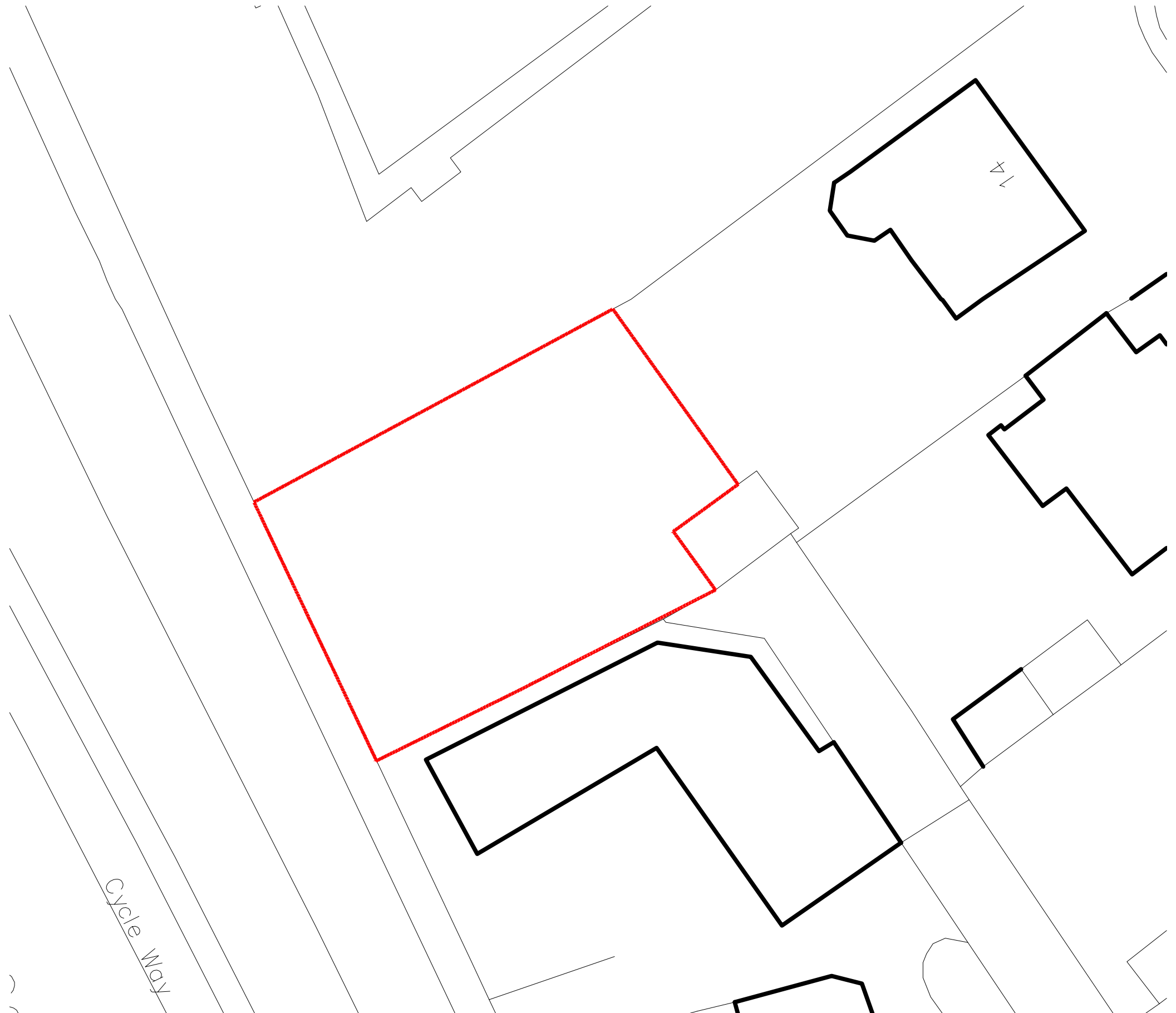
9. **S106 Requirements and Viability**

9.1 The developer has confirmed their willingness to provide a s106 financial contribution of £9,281, in the event of a positive determination, towards the provision of new public open space or the design, improvement and/or maintenance of existing open space in the locality. Were the LPA in a position to recommend approval of the application, it is considered that the above would reflect the local planning policy framework and related SPG, and meet the tests prescribed under the CIL Regulation 122 and Welsh Office Circular 13/97 Planning Obligations. In the event of a refusal of the application and subsequent appeal, a planning obligation would need to be submitted by the Applicant at appeal.

10. **CONCLUSION**

10.1 The proposal has been assessed against the policy and legislation cited above and the material factors, policy implications, and issues and objections raised considered. Whilst many issues are capable of being addressed by condition, the proposal is recommended for refusal for the single reason noted above. It is not considered that there are any factors that would outweigh the harm identified and justify a positive recommendation in this instance.


Date	Drawn	Check	Description	Rev.
15.02.18	***	***	Red line boundary indicated on plan.	A



Location Plan  
1:250



Location Plan  
1:1250

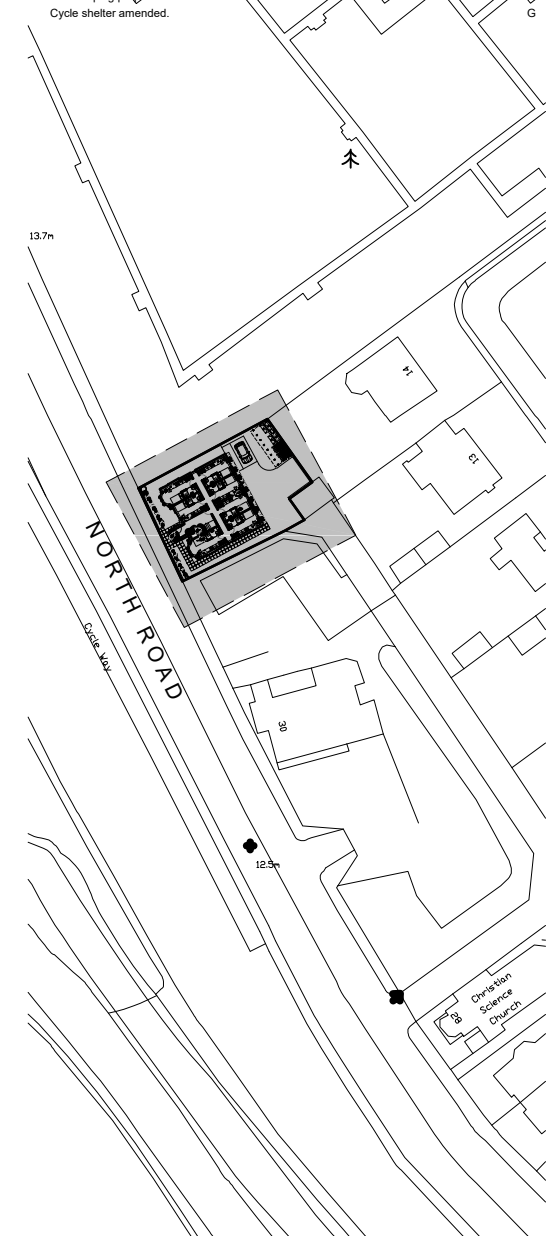
Proposed Residential Development at North Road, Cathays		Job No. 17_057
Location Plan		Dwg No. AL(00)01 Rev. A
Date 09/06/2017	Drawn WS	Scale 1:250/1250 @ A3
 <b>Architects</b> · <b>Town planners</b> <b>Environmental &amp; Urban design</b>		
Unit 1A, Compass Business Park, Pacific Road, Cardiff CF24 8HL		www.cjarchitects.co.uk tel: 029 20462100





Date	Drawn	Check	Description
17.04.18	WS	WS	Scheme amended following pre app meeting
23.10.18			Further amendments.
20.02.19			Block amended to indicate small porch
17.05.19			Colour added.
24.07.19			Site boundary made consistent with topographic survey.
04.09.19			Landscaping proposals added.
12.09.19			Cycle shelter amended.

Rev.	Description
A	Scheme amended following pre app meeting
B	Further amendments.
C	Block amended to indicate small porch
D	Colour added.
E	Site boundary made consistent with topographic survey.
F	Landscaping proposals added.
G	Cycle shelter amended.



**LOCATION PLAN**  
1:1250

**PROPOSED PLAN**  
1:100

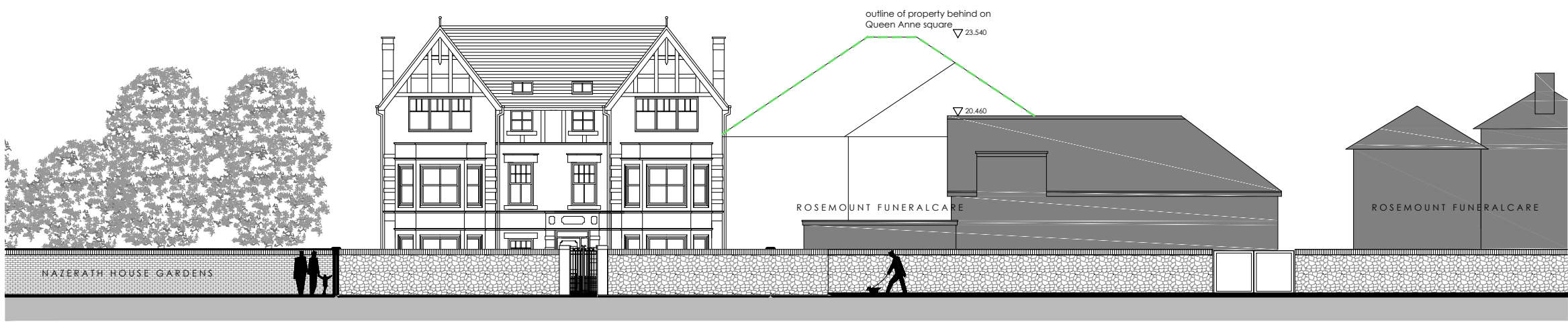
Proposed Residential Development at North Road, Cathays		Job No. 17_057	Rev. G
Title		Dwg No. AL(00)07	
Date 09/06/2017	Drawn	Scale As Shown @ A3	
<b>Architects</b>		<b>Town planners</b> <b>Environmental &amp; Urban design</b>	
<small>Unit 1A, Compass Business Park, Pacific Road, Cardiff. CF24 5HL</small>		<small>www.c-jarchitects.co.uk tel: 029 20452100</small>	



0m 5m 10m

NORTH ROAD  
A 470

SOUTH WEST FACING ELEVATION ADJACENT TO NORTH ROAD @ 1:100



0m 5m 10m

SOUTH WEST FACING ELEVATION ADJACENT TO NORTH ROAD @ 1:200

- ① Bath Stone.
- ② Render.
- ③ Brick.
- ④ New Aluminium windows, colour RAL 7016 Anthracite grey.
- ⑤ Synthetic slate.
- ⑥ Black Fascias, barge boards and rainwater goods.
- ⑦ Omitted
- ⑧ Painted timber panels

Proposed Residential Development at North Road, Cathays		Job No. 17_057	Rev. B
Title Elevations 1 of 3.		Dwg No. AL(00)15	
Date 18.04.18	Drawn	Scale 1:100 & 1:200 @ A3	

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NORTH FACING ELEVATION ADJACENT TO NAZERATH HOUSE GARDENS @ 1:100


NORTH ROAD  
A 470



- ① Bath Stone.
- ② Render.
- ③ Brick.
- ④ New Aluminium windows, colour RAL 7016 Anthracite grey.
- ⑤ Synthetic slate.
- ⑥ Black Fascias, barge boards and rainwater goods.
- ⑦ Omitted
- ⑧ Painted timber panels

REAR ELEVATION @ 1:100

REAR AREA

Proposed Residential Development at North Road, Cathays		Job No. 17_057
		Dwg No. Rev. AL(00)16 B
Title Elevations 2 of 3.		
Date 18.04.18	Drawn	Scale 1:100 @ A3
 Architects · Town planners Environmental & Urban design		www.c2architects.co.uk tel: 029 20452100
<small>Unit 1A, Compass Business Park, Pacific Road, Cardiff, CF24 6HL.</small>		




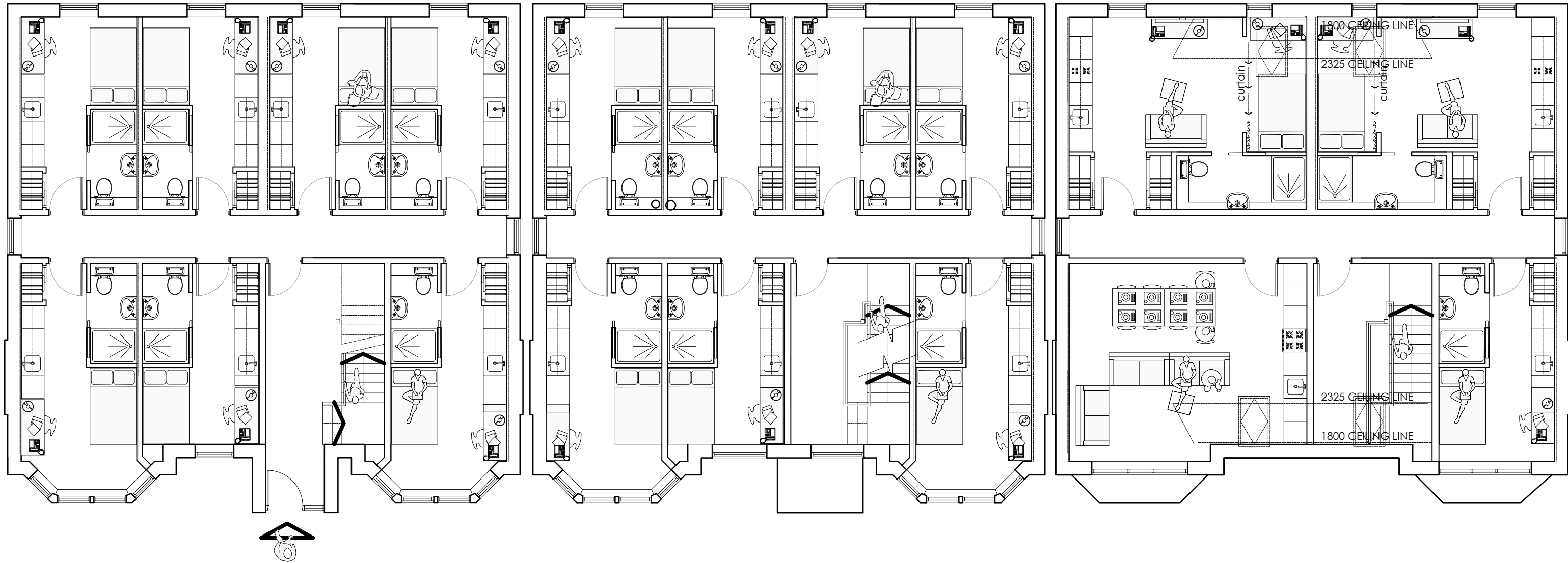
A 470  
NORTH ROAD

SOUTH FACING ELEVATION ADJACENT TO ROSEMOUNT FUNERAL SERVICES @ 1:100

④ DOTTED LINE DENOTES  
TOP OF BOUNDARY WALL

- ① Bath Stone.
- ② Render.
- ③ Brick.
- ④ New Aluminium windows, colour RAL 7016 Anthracite grey.
- ⑤ Synthetic slate.
- ⑥ Black Fascias, barge boards and rainwater goods.
- ⑦ Omitted
- ⑧ Painted timber panels

Proposed Residential Development at North Road, Cathays		Job No. 17_057	Rev. B
Title Elevations 3 of 3 Option 2		Dwg No. AL(00)17	
Date 18.04.18	Drawn	Scale 1:100 @ A3	
 <b>Architects</b> · Town planners <b>Environmental</b> & Urban design		www.cjarchitects.co.uk tel: 029 20452100	
<small>Unit 1A, Compass Business Park, Pacific Road, Cardiff, CF24 5HL</small>		<small>www.cjarchitects.co.uk tel: 029 20452100</small>	



GF PLAN

1st FLOOR PLAN

2nd FLOOR PLAN

17 rooms total

Proposed Residential Development at North Road, Cathays		Job No. 17_057
Title Floor Plans		Dwg No. Rev. AL(00)18 B
Date 12.02.19	Drawn	Scale 1:100 @ A3

**C<sup>2</sup>** Architects · Town planners  
Environmental & Urban design

Unit 1A, Compass Business Park,  
Pacific Road, Cardiff, CF24 5HL. www.c-jarchitects.co.uk  
tel: 029 20452100



C2J Architects have been commissioned by our client to secure planning permission for the vacant site on North Road. The proposal is for a three storey student accommodation.

The site is situated in a conservation area on a main route in to the city centre of Cardiff. Due to the sensitive nature in a prominent location care and attention is required for the appearance and detailing of the scheme.

The proposal seeks to imitate the character, nature and details of the existing properties within the street.

The adjacent colour elevation indicates the primary front elevation together with images of the properties within the street which the scheme looks to imitate.

20 North road




14 North road



6 North road



Proposed Residential Development at North Road, Cathays	Job No. 17_057	Rev.
	Dwg No. MAT 01	
Title		
Front elevation		
Date	Drawn	Scale
18.04.18		
 Architects · Town planners Environmental & Urban design		
Unit 1A, Compass Business Park, Pacific Road, Cardiff, CF24 9HL		www.c2jarchitects.co.uk tel: 029 20462100

**Applications decided by Delegated Powers between 01/11/2019 and 30/11/2019**

**Total Count of Applications: 201**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02728/MJR	09/10/2019	Walters (UK) Ltd	MARINE AND COASTAL ACCESS ACT 2009: PART 4 MARINE LICENSING LAMBY WAY SOLAR FARM CABLE CONNECTION	LAMBY WAY CLEANSING DEPOT, LAMBY WAY, WENTLOOG, CARDIFF, CF3 2HP	30	True	Response Sent	08/11/2019

**ADAM**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02552/DCH	08/10/2019	DTB Design	REAR GARAGE WITH GYM & GAMES ROOM OVER	36 STACEY ROAD, ADAMSDOWN, CARDIFF, CF24 1DU	48	True	Planning Permission be refused	25/11/2019
19/02865/DCH	29/10/2019	Ahmad	INCREASE IN FLAT ROOF EXTENSION TO FORM SECOND BEDROOM, AND RETENTION AND INCREASE IN FLOOR AREA OF EXISTING BATHROOM (INTERNALLY) - PREVIOUSLY APPROVED UNDER 19/01407/DCH	48 PLANET STREET, ADAMSDOWN, CARDIFF, CF24 0HZ	17	True	Permission be granted	15/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02726/MJR	11/10/2019	Crosslane Student Developments (Howard Gardens) Ltd	DISCHARGE OF CONDITION 18 (SOUND ATTENUATION MEASURES BETWEEN COMMERCIAL AND RESIDENTIAL ACCOMMODATION) OF 17/02618/MJR	BOWLING GREEN, HOWARD GARDENS, HOWARD GARDENS, ADAMSDOWN	33	True	Partial Discharge of Condition (s)	13/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
A/19/00115/MNR	13/09/2019	Dunelm	5 NO. FLEXFACE SIGNS 1 NO. WELCOME SIGN 1 SET OF 2 ENTRANCE/EXIT SIGNS	DUNELM (SOFT FURNISHINGS) LTD UNIT 1A, CITY LINK, NEWPORT ROAD, ADAMSDOWN, CARDIFF, CF24 1PQ	56	True	Permission be granted	08/11/2019
A/19/00114/MNR	10/09/2019	Vanstone	POST MOUNTED LED SCREEN TO ADVERTISE VARIOUS STATIC ADVERTS	MCKENZIE HOUSE, 30-36 NEWPORT ROAD, ADAMSDOWN, CARDIFF, CF24 0DE	56	True	Permission be granted	05/11/2019

#### **BUTE**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/01110/MJR	02/04/2019	Cadwyn Housing Association	DISCHARGE OF CONDITIONS 6 (SAMPLES OF EXTERNAL FINISHES) AND 7 (ARCHITECTURAL DETAILING TO MAIN ELEVATIONS) OF 16/00660/MJR	FORMER WHARF PUB, 121 SCHOONER WAY, ATLANTIC WHARF, CARDIFF, CF10 4EU	219	False	Full Discharge of Condition	07/11/2019
19/02407/MJR	23/09/2019	Platform	DISCHARGE OF CONDITION 4 (CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN (CEMP)) OF 18/02383/MJR IN RESPECT OF THE CONSTRUCTION PHASE ONLY, FOLLOWING PARTIAL DISCHARGE OF CONDITION 4 IN RESPECT OF THE DEMOLITION PHASE UNDER APPLICATION 19/01175/MJR	BROWNING JONES AND MORRIS, DUMBALLS ROAD, BUTETOWN, CARDIFF, CF10 5FE	46	True	Full Discharge of Condition	08/11/2019
19/01998/MJR	18/07/2019	Fusion Cardiff Capital Quarter LLP	DISCHARGE OF CONDITIONS 18 (TRAVEL PLAN) AND 20 (REMEDICATION) OF 16/03041/MJR	LAND AT HERBERT STREET, ATLANTIC WHARF, CARDIFF, CF10 4AY	118	False	Full Discharge of Condition	13/11/2019



19/02681/MJR	04/10/2019	Morgan Sindall	DISCHARGE OF CONDITIONS 9 (FUME EXTRACTION) AND 15 (CYCLE SPACES) OF 18/00792/MJR	HMS CAMBRIA, CARGO ROAD, CARDIFF BAY, CARDIFF, CF10 4RP	39	True	Full Discharge of Condition	12/11/2019
19/02686/MJR	11/10/2019	Hospitality and Capital Management Group	DISCHARGE OF CONDITIONS 3 (INVESTIGATION AND MONITORING SCHEME FOR PRESENCE OF GASES) 4 (CONTAMINATION ASSESSMENT) 5 (DETAILED REMEDIATION SCHEME AND VERIFICATION PLAN) 14 (SURFACE WATER DRAINAGE) 15 (CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN) 16 (CYCLE PARKING) 17 (SAMPLES OF EXTERNAL MATERIALS) 19 (TRAVEL MANAGEMENT PLAN) 20 (TREE PROTECTION PLAN) OF 18/00455/MJR	HOLIDAY INN EXPRESS, LONGUEIL CLOSE, ATLANTIC WHARF, CARDIFF, CF10 4EE	21	True	Full Discharge of Condition	01/11/2019

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A/19/00126/MNR	04/10/2019	Morgan Sindall	X2 FASCIA SIGNS TO NORTH AND SOUTH ELEVATIONS. 'HMS CAMBRIA' INDIVIDUAL LETTERING POSITIONED ABOVE ROOF PARAPET ON EAST AND WEST ELEVATIONS. ON WEST ELEVATION THERE ARE TWO LOGOS BEING PRINTED AND APPLIED AS MANIFESTATION TO THE CURTAIN WALL GLAZING ABOVE THE MAIN ENTRANCE. FLAG POLE TO BE POSITIONED ON PEDESTRIAN ISLAND ADJACENT TO MAIN ENTRANCE	HMS CAMBRIA, CARGO ROAD, CARDIFF BAY, CARDIFF, CF10 4LY	56	True	Permission be granted	29/11/2019
19/02558/MNR	27/09/2019	O'Sullivan	CHANGE OF USE TO 3 BED HOUSE OF MULTIPLE OCCUPANCY	18 ROXBY COURT, ATLANTIC WHARF, CARDIFF, CF10 4AG	45	True	Permission be granted	11/11/2019

19/02737/MNR	15/10/2019	Alun Griffiths (Contractors) Ltd	DISCHARGE OF CONDITION 3 (CONTAMINATION ASSESSMENT) OF 19/01120/MNR	FORMER BP OIL OCEAN TERMINAL WEST, ROVER WAY, CARDIFF BAY, CARDIFF, CF24 2RX	21	True	Full Discharge of Condition	05/11/2019
19/02955/MNR	08/11/2019	Engenie Ltd	DISCHARGE OF CONDITION 3 (DESIGN PACK) OF 18/02632/MNR	LAND OUTSIDE 7-8 BUTE CRESCENT, CARDIFF BAY, CARDIFF, CF10 5AY	7	True	Full Discharge of Condition	15/11/2019
19/02715/MNR	10/10/2019	S A Brain and Company Limited	FULL PLANNING APPLICATION FOR THE CHANGE OF USE OF PART OF THE FIRST FLOOR FROM CLASS A3 (PUB/RESTAURANT) TO CLASS C3 (RESIDENTIAL)	THE DOCK MANAGERS FLAT, MERMAID QUAY, STUART STREET, CARDIFF BAY, CARDIFF, CF10 5BZ	34	True	Permission be granted	13/11/2019
19/02791/MNR	21/10/2019	Spectrum Internet	INSTALLATION OF A TELECOMMUNICATIONS CABINET IN THE PAVEMENT TO DELIVER BROADBAND SERVICES IN THE AREA	LAND ADJACENT TO 4-7 COLUMBUS WALK, TYNDALL STREET, ATLANTIC WHARF	35	True	Permission be granted	25/11/2019
19/02804/MNR	25/10/2019	DS Properties (Cardiff Bay) Ltd	CHANGE OF USE OF PART OF GROUND FLOOR FROM OFFICE (CLASS B1(A)) TO PHYSIO (CLASS D1)	CARDIFF BAY STATION (PLATFORM), HEMINGWAY ROAD, ATLANTIC WHARF, CARDIFF, CF10 5LS	31	True	Permission be granted	25/11/2019

#### CAER

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02234/MNR	12/08/2019	Oakley	DISCHARGE OF CONDITION 13 (SAMPLES OF MATERIALS) OF 18/01462/MNR	THE CAERAU, BISHOPSTON ROAD, CAERAU, CARDIFF, CF5 5DZ	94	False	Full Discharge of Condition	14/11/2019

#### CANT

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19/02084/DCH	29/07/2019	Whitty	TWO STOREY SIDE EXTENSION, PROPOSED ENTRANCE PORCH, EXISTING VEHICULAR ACCESS WIDENED, LOFT CONVERSION INCLUDING HIP TO GABLE AND REAR DORMER, NEW GATE ACCESS TO SIDE SHARED WITH NO 15	13 BROADACRES, CANTON, CARDIFF, CF11 8DD	109	False	Permission be granted	15/11/2019
19/02429/DCH	06/09/2019	Plumb	PROPOSED REAR 2 STOREY EXTENSION OVER EXISTING FOOTPLATE WITH ADDITIONAL SINGLE STOREY EXTENSION	116 PEMBROKE ROAD, CANTON, CARDIFF, CF5 1QP	70	False	Permission be granted	15/11/2019
19/02765/DCH	23/10/2019	Owen	DEMOLISH EXISTING EXTERNAL STAIRS AND BUILDING OF NEW SINGLE STOREY KITCHEN EXTENSION	8 ST JOHN'S CRESCENT, CANTON, CARDIFF, CF5 1NX	33	True	Permission be granted	25/11/2019
19/02692/DCH	07/10/2019	Bell	ALTERATIONS TO FRONT ELEVATION INCLUDING THE INSERTION OF WINDOWS	91 BARTLEY WILSON WAY, CANTON, CARDIFF, CF11 8EN	35	True	Permission be granted	11/11/2019
19/02705/DCH	08/10/2019	Fowler	SINGLE STOREY TO SIDE OF PROPERTY	31 BROADSTAIRS ROAD, CANTON, CARDIFF, CF11 8DE	35	True	Permission be granted	12/11/2019
<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02679/MNR	04/10/2019	Heydenrych	DEVELOPMENT OF INDOOR SOFT PLAY AND ROLE PLAY AREA. CHANGE OF BUSINESS USE FROM A1 TO D2	UNIT 3, ALEXANDRA HOUSE, 307-315 COWBRIDGE ROAD EAST, CANTON, CARDIFF, CF5 1JD	49	True	Permission be granted	22/11/2019
19/02626/MNR	03/10/2019	Doyle	REAR SINGLE STOREY SIDE EXTENSION	67 GLAMORGAN STREET, CANTON, CARDIFF, CF5 1QT	39	True	Permission be granted	11/11/2019

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19/00102/DCH	22/01/2019	Harris	INTERNAL STRUCTURAL ALTERATIONS AND EXTERNAL WINDOWS AND DOOR ALTERATIONS INCLUDING 'JULIET' BALCONY TO REAR	28 QUEEN ANNE SQUARE, CATHAYS PARK, CARDIFF, CF10 3ED	287	False	Permission be granted	05/11/2019
19/02922/DCH	04/11/2019	Shamaun	LOFT CONVERSION WITH HIP TO GABLE ROOF EXTENSION, REAR DORMER ROOF EXTENSION AND INSTALLATION OF ROOFLIGHT TO FRONT ROOF ELEVATION	5 FITZROY STREET, CATHAYS, CARDIFF, CF24 4BL	21	True	Permission be granted	25/11/2019

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19/02484/MJR	10/09/2019	Rightacres Property Company Limited	DISCHARGE OF CONDITION 13 (CEMP) OF 19/01538/MJR	PARKGATE HOUSE, FORMER COUNTY COURT BUILDING AND ADJACENT LAND, WESTGATE STREET, CITY CENTRE, CARDIFF, CF10 1NW	64	False	Full Discharge of Condition	13/11/2019
19/02502/MJR	16/09/2019	National Museum Wales	DISCHARGE OF CONDITION 7 (EXISTING ROOF LANTERNS) OF 18/02709/MJR	NATIONAL MUSEUM AND GALLERY OF WALES, GORSEDD GARDENS ROAD, CATHAYS PARK, CARDIFF, CF10 3NP	56	True	Full Discharge of Condition	11/11/2019
19/00185/MJR	01/02/2019	Vita (Cardiff) 1 Limited	DISCHARGE OF CONDITIONS 33 (WRITTEN SCHEME OF INVESTIGATION FOR BUILDING RECORDING) AND 34 (WRITTEN SCHEME OF HISTORIC ENVIRONMENT MITIGATION) OF 18/02527/MJR	BRADLEY COURT, 11 PARK PLACE, CATHAYS PARK, CARDIFF, CF10 3DR	294	False	Full Discharge of Condition	22/11/2019

19/01391/MJR	08/05/2019	Vita (Cardiff) 1 Limited	DISCHARGE OF CONDITIONS 33 (HISTORIC BUILDING RECORD) AND 34 (ARCHAEOLOGICAL RECORD) OF 18/02527/MJR	BRADLEY COURT AND 11 PARK PLACE, CATHAYS PARK, CARDIFF, CF10 3DR	198	False	Full Discharge of Condition	22/11/2019
19/02443/MJR	10/09/2019	Cardiff University	DISCHARGE OF CONDITION 30 (ENVIRONMENTAL IMPROVEMENTS TO THE HIGHWAY AND FOOTWAY) OF 16/01739/MJR	46-48 PARK PLACE, CATHAYS PARK, CARDIFF, CF10 3LU	71	False	Partial Discharge of Condition (s)	20/11/2019
19/02974/MJR	12/11/2019	Rightacres Property Company Ltd	THE INSTALLATION OF LOUVRES (AUTOMATIC OPENING VENTS (AOV)) TO THE FACADES ABOVE THE FINAL EXIT DOORS, LOCATED TO THE EAST AND WEST ELEVATIONS AT THE GROUND FLOOR LEVEL OF PLOTS 6 AND 7 OF THE CENTRAL SQUARE MASTERPLAN AREA - PREVIOUSLY APPROVED UNDER 17/01751/MJR	LAND NORTH OF WOOD STREET, WEST OF HAVELOCK STREET, SOUTH OF PARK STREET AND EAST OF NO.6 PARK STREET. (THE SITE ENCOMPASSES PLOTS 6 (IN PART), 7 AND 8 OF THE CENTRAL SQUARE MASTERPLAN AREA), CITY CENTRE, CARDIFF, CF10 1XR	2	True	Permission be granted	14/11/2019
19/02954/MJR	13/11/2019	Mansford LLP	DISCHARGE OF CONDITION 5 (SOUND INSULATION) OF 17/00371/MJR	8-10 HIGH STREET, CITY CENTRE, CARDIFF, CF10 1AW	14	True	Full Discharge of Condition	27/11/2019
<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02518/MNR	17/09/2019	Winter	CONVERSION OF 3 BEDROOM HOUSE INTO 2 SELF CONTAINED FLATS. DEMOLITION AND RECONSTRUCTION OF EXISTING REAR EXTENSIONS	82 COBURN STREET, CATHAYS, CARDIFF, CF24 4BT	50	True	Planning Permission be refused	06/11/2019
18/02923/MNR	12/12/2018	CDF Planning	CHANGE OF USE TO THE FIRST FLOOR OF THE BUILDING TO SUI GENERIS STUDENT ACCOMMODATION	CAFE JUNIOR, FANNY STREET, CATHAYS, CARDIFF, CF24 4EH	352	False	Permission be granted	29/11/2019

19/00360/MNR	02/08/2019	Akhtar	CONVERSION FROM HOUSE IN MULTIPLE OCCUPATION TO 3 FLATS WITH GROUND & FIRST REAR EXTENSION AND DORMER LOFT CONVERSION	49 MERTHYR STREET, CATHAYS, CARDIFF, CF24 4JL	119	False	Permission be granted	29/11/2019
19/01093/MNR	11/04/2019	Blackler	INTERNAL ALTERATIONS TO ALLOW CHANGE OF USE OF THE FIRST FLOOR TO STUDENT ACCOMMODATION	CAFE JUNIOR, FANNY STREET, CATHAYS, CARDIFF, CF24 4EH	232	False	Permission be granted	29/11/2019
19/02496/MNR	11/09/2019	Mr Whittle	CERTIFICATE OF LAWFULNESS FOR EXISTING USE FOR 2X1 BEDROOM FLATS	11 COLUM PLACE, CATHAYS PARK, CARDIFF, CF10 3EP	56	True	Permission be granted	06/11/2019
19/02424/MNR	05/09/2019	Demipower LTD	INSTALLATION OF PLANT EQUIPMENT TO THE REAR OF THE BUILDING	112 ST MARY STREET, CITY CENTRE, CARDIFF, CF10 1DX	64	False	Permission be granted	08/11/2019
19/02425/MNR	05/09/2019	Demipower LTD	INSTALLATION OF PLANT EQUIPMENT TO THE REAR OF THE BUILDING	112 ST MARY STREET, CITY CENTRE, CARDIFF, CF10 1DX	64	False	Permission be granted	08/11/2019
19/01657/MNR	07/06/2019	ILILI Ltd	RETENTION OF STRUCTURE ANCILLARY TO MAIN RESTAURANT	39 WEDAL ROAD, CATHAYS, CARDIFF, CF14 3QX	159	False	Permission be granted	13/11/2019
19/01015/MNR	26/03/2019	Gardner	CONVERSION OF 4 FIRST AND SECOND FLOOR FLATS INTO 7 FLATS.	131-141 CRWYS ROAD, CATHAYS, CARDIFF, CF24 4XR	245	False	Permission be granted	26/11/2019
19/01352/MNR	27/09/2019	Mr Toni Venditto	VARIATION OF CONDITION 3 OF PLANNING PERMISSION 16/02486/MNR TO CHANGE THE OPENING HOURS FROM 08:00 - 21:00 HOURS TO 08:00 - 01:00 AM	BASEMENT AND GROUND FLOOR, 23 HIGH STREET, CITY CENTRE, CARDIFF, CF10 1PT	56	True	Permission be granted	22/11/2019
19/02565/MNR	20/09/2019	Committe Member at Cardiff Lawn Tennis Club (Chairman)	TO REPLACE THE EXISTING OLD TECHNOLOGY METAL HALIDE FLOOD LIGHTING SYSTEM WITH A NEW ENERGY EFFICIENT LED LIGHTING SYSTEM	CARDIFF LAWN TENNIS CLUB, NORTH ROAD, BLACKWEIR, CARDIFF, CF10 3EW	63	False	Permission be granted	22/11/2019

19/02639/MNR	03/10/2019	Cardiff Marriott Hotel	ALTERATIONS TO VEHICLE PICK-UP AND DROP-OFF AREA AND TERRACE ON NORTH ELEVATION. ALTERATIONS TO EAST ELEVATION AT GROUND LEVEL IN NORTH EAST CORNER OF SITE AND CONSTRUCTION OF NEW RAISED TERRACE	CARDIFF MARRIOTT HOTEL, MILL LANE, CITY CENTRE, CARDIFF, CF10 1EZ	56	True	Permission be granted	28/11/2019
A/19/00123/MNR	03/10/2019	Cardiff Marriott Hotel	REPLACEMENT OF EXISTING SIGNAGE ON EAST, NORTH, AND WEST ELEVATIONS	CARDIFF MARRIOTT HOTEL, MILL LANE, CITY CENTRE, CARDIFF, CF10 1EZ	56	True	Permission be granted	28/11/2019
19/02668/MNR	04/10/2019	Mocka Lounge	A PROPOSED SELF SUPPORTING ROOF CANOPY OVER THE EXISTING OUTDOOR SEATING AREA TO THE FRONT	1-2 MILL LANE, CITY CENTRE, CARDIFF, CF10 1FL	55	True	Permission be granted	28/11/2019
19/02375/MNR	28/08/2019	Mr Morris	GROUND FLOOR REAR EXTENSION, DORMER LOFT CONVERSION AND CONVERSION FROM C4 HOUSE IN MULTIPLE OCCUPATION TO 7 BED SUI GENERIS HOUSE IN MULTIPLE OCCUPATION	13 MAY STREET, CATHAYS, CARDIFF, CF24 4EW	79	False	Permission be granted	15/11/2019
19/02621/MNR	30/09/2019	Engenie Limited	INSTALLATION OF 2X ELECTRIC VEHICLE CHARGING POINTS ALONGSIDE EXISTING ON STREET CAR PARKING SPACES, WITH ANCILLARY BOLLARDS, POWER FEEDER PILLAR AND SIGNAGE	PARK PLACE, CATHAYS PARK, CARDIFF, CF10 3NP	43	True	Permission be granted	12/11/2019
19/02495/MNR	11/09/2019	MSM Homes	GROUND FLOOR REAR EXTENSION REAR DORMER AND CONVERSION TO TWO FLATS	56 MALEFANT STREET, CATHAYS, CARDIFF, CF24 4QH	63	False	Permission be granted	13/11/2019
19/02982/MNR	12/11/2019	Veness	EXISTING USE AS HOUSE OF MULTIPLE OCCUPANCY	4 FANNY STREET, CATHAYS, CARDIFF, CF24 4EH	2	True	Permission be granted	14/11/2019
A/19/00127/MNR	07/10/2019	Bar 44	ERECTION OF CANOPIES WITH BAR 44 LOGOS	15-23 WESTGATE STREET, CITY CENTRE, CARDIFF, CF10 1DD	30	True	Permission be granted	06/11/2019

19/02690/MNR	07/10/2019	Hanna	INSTALLATION OF NEW ALUMINIUM POWDER COATED AUTOMATIC ENTRANCE DOORS TO FRONT OF ELEVATION	WHARTON PLACE, 13 WHARTON STREET, CITY CENTRE	29	True	Permission be granted	05/11/2019
19/02903/MNR	01/11/2019	Mr C Davey	TAKE DOWN STRUCTURALLY UNSTABLE FRONT WALL AND REBUILD WITH SAND CEMENT RENDER FINISH PAINTED WHITE - PREVIOUSLY APPROVED UNDER 19/01475/MNR	13 LETTY STREET, CATHAYS, CARDIFF, CF24 4EJ	10	True	Permission be granted	11/11/2019
19/02806/MNR	25/10/2019	Telefonica O2 UK limited	INSTALLATION OF 6 NO. 0.3 METRE DISHES AND 1 NO. EQUIPMENT CABINET AND ASSOCIATED DEVELOPMENT	STADIUM HOUSE, 5 PARK STREET, CITY CENTRE, CARDIFF, CF10 1NT	31	True	Permission be granted	25/11/2019
19/02932/MNR	07/11/2019	Vanstone	USE OF HOUSE AS 4 FLATS	93 MALEFANT STREET, CATHAYS	11	True	Permission be granted	18/11/2019

#### CRE

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02400/DCH	02/09/2019	Davies	PROPOSED SINGLE STOREY REAR EXTENSION	28 MAES CADWGAN, CREIGIAU, CARDIFF, CF15 9TQ	70	False	Permission be granted	11/11/2019
19/02605/DCH	26/09/2019	Bradbury	PROPOSED DOUBLE STOREY REAR EXTENSION	3 FALCONWOOD DRIVE, ST FAGANS, CARDIFF, CF5 4SE	41	True	Permission be granted	06/11/2019
<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
18/02784/MJR	29/11/2018	Trustees of St Fagans No 1 & 2 Trust and Trustees of St Fagans	DISCHARGE OF CONDITIONS 18 (DESIGN CODE) AND 19 (STRATEGIC GREEN INFRASTRUCTURE MANAGEMENT STRATEGY) OF 14/02733/MJR	PLASDWR, NORTH WEST CARDIFF	341	False	Full Discharge of Condition	05/11/2019



19/02553/MJR	23/09/2019	Redrow Homes	DISCHARGE OF CONDITIONS 14 (TREES) AND 16 (CEMP) OF 14/02188/MJR IN RESPECT OF AREA COVERED BY RESERVED MATTERS APPLICATION 19/02289/MJR (PHASE 1F, SOUTH OF PENTREBANE RD)	LAND SOUTH OF PENTREBANE ROAD, PENTREBANE, CARDIFF	51	True	Partial Discharge of Condition (s)	13/11/2019
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## CYNC

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02448/DCH	06/09/2019	Nasser	SINGLE STOREY REAR EXTENSION, SIDE DORMER AND EXTERNAL ALTERATIONS	12 THE FAIRWAY, CYNCOED, CARDIFF, CF23 6RE	56	True	Permission be granted	01/11/2019
19/02205/DCH	12/08/2019	Clayton	REMOVAL OF EXISTING SINGLE STOREY SIDE GARAGE AND CONSTRUCTION OF TWO STOREY, PITCHED ROOF SIDE AND REAR EXTENSION. WORKS ALSO INCLUDE NEW TERRACE/RETAINING WALL AND NEW BOUNDARY TREATMENT TO REAR GARDEN.	10 JUSTIN CLOSE, LAKESIDE, CARDIFF, CF23 6HH	93	False	Permission be granted	13/11/2019
19/02714/DCH	15/10/2019	Stembridge	PROPOSED SINGLE STOREY SIDE EXTENSION	26 DAN-Y-COED ROAD, CYNCOED, CARDIFF, CF23 6NA	41	True	Permission be granted	25/11/2019
19/02631/DCH	30/09/2019	Jones	PROPOSED PORCH EXTENSION	17 TORRENS DRIVE, LAKESIDE, CARDIFF, CF23 6DQ	37	True	Permission be granted	06/11/2019
19/02712/DCH	08/10/2019	Denton-Powell	SINGLE STOREY ORANGERY TO REAR. CONSERVATORY TO SIDE. REBUILD GARAGE	15 HEOL ESGYN, CYNCOED, CARDIFF, CF23 6JT	35	True	Permission be granted	12/11/2019
19/02724/DCH	14/10/2019	Woodward	REPLACE LEAN-TO CONSERVATORY AT REAR OF PROPERTY WITH PITCHED ROOF	13 KESWICK AVENUE, ROATH PARK, CARDIFF, CF23 5PU	32	True	Permission be granted	15/11/2019

19/02807/DCH	23/10/2019	Stacey	CONVERSION OF AN EXISTING GARAGE AND NEW SIDE EXTENSION TO REPLACE AN EXISTING FRONT PORCH	3 LAKESIDE DRIVE, LAKESIDE, CARDIFF, CF23 6DD	36	True	Permission be granted	28/11/2019
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<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/02002/MNR	17/07/2019	Anderson	DETACHED 3 BEDROOM HOUSE	PART OF LAND AT 102 BLACK OAK ROAD, CYNCOED, CARDIFF, CF23 6QX	112	False	Permission be granted	06/11/2019
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**ELY**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/00513/MNR	26/03/2019	Webley	DISCHARGE OF CONDITIONS 3 (SAMPLES), 5 (DRAINAGE), 7 (INTERIM CERTIFICATE), 9 (CAR PARKING), 14 (BOUNDARY TREATMENT), 15 (LANDSCAPING) AND 21(CONSTRUCTION MANAGEMENT PLAN) OF 14/00417/DCO	PROPOSED DWELLING ON LAND AT 220, GRAND AVENUE, ELY	231	False	Full Discharge of Condition	12/11/2019
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**FAIR**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/01891/DCH	17/07/2019	REES	PROPOSED TWO STOREY SIDE AND SINGLE STOREY REAR EXTENSION. REMOVAL OF EXISTING GARAGE. NEW RETAINING WALL/GARDEN WORKS	7 DUXFORD CLOSE, FAIRWATER, CARDIFF, CF5 2PR	135	False	Permission be granted	29/11/2019
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19/02546/DCH	18/09/2019	Kendell	GROUND FLOOR SIDE EXTENSION TO CREATE GRANNY ANNEXE	BEECHFIELD HOUSE, 122 ST FAGANS ROAD, FAIRWATER, CARDIFF, CF5 3EU	44	True	Permission be granted	01/11/2019
19/02421/DCH	10/09/2019	Cronin	PROPOSED SINGLE STOREY PORCH TO FRONT ELEVATION	133 PENTREBANE ROAD, PENTREBANE, CARDIFF, CF5 3RB	56	True	Permission be granted	05/11/2019
19/02643/DCH	02/10/2019	Reece	ERECTION OF SINGLE STOREY EXTENSION TO THE SIDE OF THE PROPERTY IN THE LOCATION OF THE DEMOLISHED GARAGE AND CONSERVATORY TO REAR	10 TUDOR CLOSE, FAIRWATER, CARDIFF, CF5 3DE	51	True	Permission be granted	22/11/2019
19/02671/DCH	04/10/2019	PEERTUM	PROPOSED SINGLE STOREY REAR EXTENSION. PROPOSED HIP TO GABLE. PROPOSED REAR DORMER	164 KEYSTON ROAD, FAIRWATER, CARDIFF, CF5 3NJ	38	True	Permission be granted	11/11/2019
19/02793/DCH	21/10/2019	Nagi	PROPOSED TWO STOREY SIDE EXTENSION AND ROOF EXTENSION WITH DORMER TO THE REAR	41 ELDERBERRY ROAD, PENTREBANE, CARDIFF, CF5 3RG	37	True	Permission be granted	27/11/2019
<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/01903/MJR	03/07/2019	Redrow Homes	DISCHARGE OF CONDITIONS 2 - MATERIALS 3 - BLOCK PAVING MATERIAL 6 - CAR PARKING PROVISION 7 - CYCLE PARKING 8 - CELLWEB DETAILS 9 - PREVENTING THROUGH VEHICULAR ACCESS 10 - ADDITIONAL DRAINAGE DETAILS OF 18/01184/MJR	PART 1 OF PHASE 2A, SOUTH OF LLANTRISANT ROAD, NORTH WEST CARDIFF	141	False	Full Discharge of Condition	21/11/2019

19/00529/MJR	11/03/2019	Redrow Homes	DISCHARGE OF CONDITIONS 17 (TREES) AND 19 (CEMP) OF 14/02157/MJR IN RESPECT OF AREA COVERED BY RESERVED MATTERS APPLICATION 19/00435/MJR (PART 1 OF PHASE 2B, SOUTH OF LLANTRISANT RD) Please note this consultation relates to an amended CEMP in discharge of condition 19 only.	LAND NORTH AND SOUTH OF LLANTRISANT ROAD, NORTH WEST CARDIFF	263	False	Full Discharge of Condition	29/11/2019
19/00104/MJR	25/01/2019	United Welsh Housing Association	DISCHARGE OF CONDITIONS 5 (FOOTPATH CROSSOVER/RELOCATION OF LIGHTING COLUMN) AND 12 (LANDSCAPING/SOIL TESTING) OF 14/02770/MJR	LAND BETWEEN 53 AND 81 GORSE PLACE, FAIRWATER, CARDIFF	299	False	Full Discharge of Condition	20/11/2019

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<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02863/DCH	29/10/2019	Pascoe	PROPOSED SINGLE-STOREY REAR / SIDE EXTENSION (AND ASSOCIATED ALTERATION WORKS)	15 AUSTRALIA ROAD, GABALFA, CARDIFF, CF14 3BZ	29	True	Permission be granted	27/11/2019
19/02780/DCH	17/10/2019	Hickey	SINGLE STOREY REAR EXTENSION	94 AUSTRALIA ROAD, GABALFA, CARDIFF, CF14 3DA	26	True	Permission be granted	12/11/2019
<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02529/MNR	18/09/2019	TIC Developments Ltd	PROPOSED CONVERSION OF EXISTING DWELLING HOUSE TO 4 NO. FLATS	2 LLANISHEN STREET, GABALFA, CARDIFF, CF14 3QE	54	True	Permission be granted	11/11/2019

18/02967/MNR	16/01/2019	Dawah	TO CONVERT A 5 BEDROOM RESIDENTIAL PROPERTY INTO TWO SELF CONTAINED FLATS. EACH OF THESE WILL HAVE 2 BEDROOMS, ONE RECEPTION, KITCHEN AND BATHROOM	69 MANOR STREET, GABALFA, CARDIFF, CF14 3PX	317	False	Permission be granted	29/11/2019
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**GRAN**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02362/DCH	02/09/2019	Fflur	SIDE AND REAR SINGLE STOREY EXTENSION	79 CLARE ROAD, GRANGETOWN, CARDIFF, CF11 6QQ	60	False	Permission be granted	01/11/2019
19/02093/DCH	05/08/2019	Mrs Najiya Ali	REAR EXTENSION WITH BALCONY OVER AND TWO STOREY EXTENSION AT SIDE WITH REAR DORMER AND EXTENSION TO EXISTING ROOF HIP	17 GRANGE PLACE, GRANGETOWN, CARDIFF, CF11 7DB	100	False	Permission be granted	13/11/2019
19/02578/DCH	25/09/2019	Bryant	SINGLE STOREY REAR/SIDE EXTENSION	5 LUDLOW CLOSE, GRANGETOWN, CARDIFF, CF11 7DG	42	True	Permission be granted	06/11/2019
19/02562/DCH	20/09/2019	WALTON	SINGLE STOREY REAR EXTENSION	4 BARGOED STREET, GRANGETOWN, CARDIFF, CF11 7AF	42	True	Permission be granted	01/11/2019
19/02673/DCH	09/10/2019	PARKER	HIP TO GABLE AND REAR DORMER EXTENSION	27 MOORDALE ROAD, GRANGETOWN, CARDIFF, CF11 7DU	23	True	Permission be granted	01/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/02514/MJR	13/09/2019	Wales & West Housing Association	DISCHARGE OF CONDITIONS 8 (DRAINAGE SCHEME), 9 (GAS PROTECTION SCHEME), 10 (CONTAMINATED LAND MEASURES) AND 11 (REMEDICATION SCHEME OF PLANNING PERMISSION 16/03065/MJR	ST PAULS CHURCH, LLANMAES STREET, GRANGETOWN, CARDIFF, CF11 7LR	75	False	Full Discharge of Condition	27/11/2019
19/02516/MJR	13/09/2019	Wales and West Housing Association	DISCHARGE OF CONDITIONS 7 (DRAINAGE SCHEME), 9 (GAS PROTECTION MEASURES), 10 (CONTAMINATED LAND) AND 11 (REMEDICATION MEASURES) OF PLANNING PERMISSION 18/02941/MNR	ST PAUL'S CHURCH, PAGET STREET, GRANGETOWN, CARDIFF, CF11 7LA	76	False	Full Discharge of Condition	28/11/2019
19/02662/MJR	04/10/2019	J G Hale Construction	DISCHARGE OF CONDITIONS 7 (VERIFICATION REPORT), 9 (IMPORTED SOIL), 10 (AGGREGATES), 11 (SITE WON MATERIAL) AND 13 (NOISE) OF 16/02994/MJR	162-168 PENARTH ROAD, GRANGETOWN, CARDIFF, CF11 6NJ	49	True	Partial Discharge of Condition (s)	22/11/2019
19/01681/MJR	07/06/2019	Wales & West Housing Association Ltd	DISCHARGE OF CONDITION 3 (HISTORIC BUILDING RECORDING) OF 16/03066/MJR	ST PAULS CHURCH, LLANMAES STREET, GRANGETOWN, CARDIFF, CF11 7LR	171	False	Full Discharge of Condition	25/11/2019
19/02917/MJR	04/11/2019	Sytner Group	DISCHARGE OF CONDITIONS 3 (GAS MONITORING), 8 (TOPSOIL) AND 16 (CYCLE PARKING) OF 16/00124/MJR	SYTNER CARDIFF BMW, PENARTH ROAD, LECKWITH, CARDIFF, CF11 8TT	18	True	Full Discharge of Condition	22/11/2019
<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02554/MNR	23/09/2019	Wakely	CHANGE OF USE TO CLASS C4 HOUSE IN MULTIPLE OCCUPATION	62 PENARTH ROAD, GRANGETOWN, CARDIFF, CF11 6NG	53	True	Permission be granted	15/11/2019

19/02785/MNR	23/10/2019	Wales & West Housing Association	DISCHARGE OF CONDITION 3 (EXTERNAL FINISHING MATERIALS) OF 18/02941/MNR	ST PAULS CHURCH, LLANMAES STREET, GRANGETOWN, CARDIFF, CF11 7LR	21	True	Full Discharge of Condition	13/11/2019
19/02786/MJR	23/10/2019	Wales & West Housing Association	DISCHARGE OF CONDITIONS 3 (EXTERNAL FINISHING MATERIALS), 7 (CONSTRUCTION MANAGEMENT PLAN), 18 (TREE PROTECTION) AND 19 (LANDSCAPE WORKS) OF 16/03065/MJR	ST PAULS CHURCH, LLANMAES STREET, GRANGETOWN, CARDIFF, CF11 7LR	21	True	Partial Discharge of Condition (s)	13/11/2019

## HEAT

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02640/DCH	01/10/2019	Gould	REMOVAL OF AN EXISTING UPVC CONSERVATORY AND ERECTION OF A SINGLE STORY, PITCHED ROOF REAR EXTENSION, TOGETHER WITH THE CONVERSION OF THE EXISTING GARAGE AREA INTO INTERNAL SPACE	57 ST AMBROSE ROAD, HEATH, CARDIFF, CF14 4BH	42	True	Permission be granted	12/11/2019
19/02346/DCH	29/08/2019	Crossley	NEW AMENDED SIDE AND REAR EXTENSION	23 MAVIS GROVE, RHIWBINA, CARDIFF, CF14 4SA	81	False	Permission be granted	18/11/2019
19/02603/DCH	27/09/2019	Vellani	SINGLE STOREY REAR ELEVATION AND ALTERATION TO EXISTING ADJOINING MONO PITCH KITCHEN ROOF	222 HEATHWOOD ROAD, HEATH, CARDIFF, CF14 4BS	63	False	Permission be granted	29/11/2019
19/02890/DCH	31/10/2019	Elliott	MINOR ALTERATIONS TO SIZE AND DESIGN OF PROPOSED EXTENSIONS - PREVIOUSLY APPROVED UNDER 18/02570/DCH	120 HEATHWOOD ROAD, HEATH, CARDIFF, CF14 4BQ	5	True	Permission be granted	05/11/2019
19/02208/DCH	30/09/2019	Mr Nick Waters	SINGLE STOREY SIDE AND REAR EXTENSIONS	7 CRYSTAL AVENUE, HEATH, CARDIFF, CF23 5QJ	32	True	Permission be granted	01/11/2019

19/03013/DCH	15/11/2019	Tennant	ROOF OVER SINGLE STOREY ELEMENT TO BE AMENDED TO A FLAT ROOF FROM A PITCHED ROOF - PREVIOUSLY APPROVED UNDER 19/01180/DCH	8 ST AIDAN CRESCENT, HEATH, CARDIFF, CF14 4AU	10	True	Permission be granted	25/11/2019
19/02730/DCH	15/10/2019	Jones	PROPOSED SINGLE STOREY REAR EXTENSION	42 TAIR ERW ROAD, BIRCHGROVE, CARDIFF, CF14 4QZ	36	True	Permission be granted	20/11/2019

## LISV

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/01795/DCH	25/06/2019	Hedlund	THE EXTENSION AND ALTERATION OF THE COACH HOUSE AT THE TY GWYN ESTATE	ORCHARD COTTAGE, LISVANE ROAD, LISVANE, CARDIFF, CF14 0SG	154	False	Permission be granted	26/11/2019
19/01796/DCH	25/06/2019	Hedlund	THE EXTENSION AND ALTERATION OF THE COACH HOUSE AT THE TY GWYN ESTATE	ORCHARD COTTAGE, LISVANE ROAD, LISVANE, CARDIFF, CF14 0SG	154	False	Permission be granted	26/11/2019
19/02478/DCH	10/09/2019	Davies	INTERNAL ALTERATIONS, REDUCE GARAGE TO STORAGE AREA REPLACE CONSERVATORY. REPLACE GARAGE DOORS.	5 CHURCHILL CLOSE, LISVANE, CARDIFF, CF14 0EP	56	True	Permission be granted	05/11/2019
19/02695/DCH	17/10/2019	Allbrook	TO CONVERT 50% OFF EXISTING GARAGE TO NEW LOUNGE WITH LANTERNS IN EXISTING ROOF AND GLASS DOORS IN SIDE	37 MILL ROAD, LISVANE, CARDIFF, CF14 0XH	27	True	Permission be granted	13/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/01973/MJR	15/07/2019	Redrow Homes	RESERVED MATTERS APPLICATION IN RESPECT OF 14/02891/MJR RELATING TO LAYOUT, SCALE, APPEARANCE OF BUILDINGS, MEANS OF ACCESS (WHERE NOT ALREADY PERMITTED BY THE PERMISSION) AND LANDSCAPING FOR PHASE 2A OF THE SITE COMPRISING 11 DWELLINGS	PHASE 2A, CHURCHLANDS LAND NORTH AND EAST OF LISVANE, LISVANE, CARDIFF	112	False	Permission be granted	04/11/2019
19/02551/MJR	19/09/2019	Redrow Homes	DISCHARGE OF CONDITIONS 8 (INFRASTRUCTURE PHASING PLAN), 9 (CEMP) AND 26 (NOISE ASSESSMENT) OF 14/02891/MJR	PHASE 2A, CHURCHLANDS LAND NORTH AND EAST OF LISVANE, MAERDY LANE, LISVANE	55	True	Full Discharge of Condition	13/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/01432/MNR	10/05/2019	PHG CAPITAL	PROPOSED CONSTRUCTION OF DETACHED DWELLING AND CARPORT	LOZELLES, CHURCH ROAD, LISVANE, CARDIFF, CF14 0SJ	189	False	Permission be granted	15/11/2019
19/02897/MNR	31/10/2019	CAERPHILLY COUNTY BOROUGH COUNCIL	REQUEST FOR OBSERVATIONS - RETAIN AND COMPLETE THE RELOCATION OF AN EXISTING PACKAGE SEWAGE SYSTEM	TRAVELLERS REST INN, MOUNTAIN ROAD, THORNHILL, CAERPHILLY	6	True	Raise No Objection	06/11/2019

#### LLAN

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/02151/DCH	20/08/2019	Morris	ERECTION OF SINGLE STOREY WOODEN LEAN-TO SHED WITH WOODEN BASE AND COVERED SEATING AREA	23 GERAINT CLOSE, THORNHILL, CARDIFF, CF14 9BE	84	False	Permission be granted	12/11/2019
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19/02310/DCH	19/09/2019	Hugh Gates Building Services	SINGLE STOREY FLAT ROOF EXTENSION AT REAR OF PROPERTY, INCLUDING ALTERATIONS TO THE REAR GARDEN/DECKING AND PRIVACY SCREEN	10A CLOS-Y-CARLWM, THORNHILL, CARDIFF, CF14 9HN	67	False	Permission be granted	25/11/2019
19/02746/DCH	14/10/2019	Parsell	REAR GROUND FLOOR EXTENSION AND REAR DORMER ROOF EXTENSION	22 WHITE BARN ROAD, LLANISHEN, CARDIFF, CF14 5HB	30	True	Permission be granted	13/11/2019
<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02521/MNR	17/09/2019	McCarthy and Stone	RETENTION OF AIR SOURCE HEAT PUMPS	THE ORCHARDS, ILEX CLOSE, LLANISHEN, CARDIFF, CF14 5ZD	70	False	Permission be granted	26/11/2019
19/02535/MNR	18/09/2019	Acorns Nurseries Ltd	INCREASE CHILDREN NUMBERS FROM 65 TO 81	PRIORY HOUSE, LISVANE ROAD, LISVANE, CARDIFF, CF14 0SA	58	False	Permission be granted	15/11/2019
19/02789/MNR	21/10/2019	McDonald's Restaurants Ltd	REDUCTION OF SCOPE, REMOVAL OF EXTERNAL ENTRANCE LOBBIES AND MINOR AMEND TO EXTERNAL DESIGN - PREVIOUSLY APPROVED UNDER 19/01370/MNR	FORMER THE TY GLAS, 75 TY GLAS AVENUE, LLANISHEN, CARDIFF, CF14 5DX	38	True	Permission be granted	28/11/2019

#### LLDF

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/01777/DCH	19/06/2019	Warrington	PORCH EXTENSION TO FRONT CONVERSION OF GARAGE INTO HABITABLE ROOM SINGLE STOREY KITCHEN EXTENSION TO REAR TWO STOREY BEDROOM EXTENSION TO SIDE AND REAR SINGLE STOREY GRANNY ANNEX TO REAR LOFT CONVERSION AND ROOF DORMER EXTENSION.	8 ROOKWOOD AVENUE, LLANDAFF, CARDIFF, CF5 2NP	140	False	Permission be granted	06/11/2019
19/02460/DCH	11/09/2019	hoskins	REAR EXTENSION AND REAR DORMERS	2 IMPERIAL BUILDINGS ROW, LLANDAFF, CARDIFF, CF5 2EF	51	True	Permission be granted	01/11/2019
19/02564/DCH	20/09/2019	Beer	PROPOSED SINGLE STOREY REAR EXTENSION WITH ASSOCIATED EXTERNAL WORKS	383 WESTERN AVENUE, LLANDAFF, CARDIFF, CF5 2BD	52	True	Permission be granted	11/11/2019
19/02627/DCH	03/10/2019	pettican	DOUBLE STORY SIDE EXTENSION AND SINGLE STORY REAR EXTENSION	36 EDWARD CLARKE CLOSE, DANESCOURT, CARDIFF, CF5 2RW	50	True	Permission be granted	22/11/2019
19/02418/DCH	05/09/2019	Bannerji	NEW PITCHED ROOF TO SINGLE STOREY REAR EXTENSION & SINGLE STOREY EXTENSION TO SIDE AND RE-CONSTRUCTION OF PORCH TO FRONT	12 PECKHAM CLOSE, DANESCOURT, CARDIFF, CF5 2SL	77	False	Permission be granted	21/11/2019
19/02458/DCH	09/09/2019	Duff	RESURFACING OF DRIVEWAY TO FRONT AND SIDE OF SEMI DETACHED HOUSE BY LAYING BLOCK PAVING TO THE FRONT DRIVEWAY AND REPLACE STONE CHIPPINGS LIKE FOR LIKE ON THE SIDE DRIVEWAY. TO BUILD A SMALL BORDER WITH SOLID WOODEN OAK SLEEPERS AROUND THE SIDE (NEXT TO NO 7) AND FRONT ASPECT OF THE DRIVE . ERECTION OF A HEDGE BETWEEN NO 5 AND NO 7.	5 HOWELL'S CRESCENT, LLANDAFF, CARDIFF, CF5 2AJ	73	False	Permission be granted	21/11/2019
19/02691/DCH	08/10/2019	DAVIES	PROPOSED REAR AND SIDE EXTENSIONS LOFT CONVERSION WITH NEW GABLE WALL AND PORCH	19 ROOKWOOD CLOSE, LLANDAFF, CARDIFF, CF5 2NR	34	True	Permission be granted	11/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02181/MNR	08/08/2019	DOVEY	PROPOSED ALTERATIONS, EXTENSIONS AND CHANGE OF USE TO RETAIL UNIT AND OFFICE INCLUDING NEW SHOPFRONT AT GROUND FLOOR AND TWO NEW FLATS AT FIRST FLOOR WITH ROOF TERRACES	21 HIGH STREET, LLANDAFF, CARDIFF, CF5 2DY	106	False	Permission be granted	22/11/2019
19/02440/MNR	20/09/2019	Williams & Mathias	NEW SELF-CONTAINED SINGLE DWELLING IN PLACE OF EXISTING STRUCTURE (RESIDENTIAL ACCOMMODATION ANCILLARY TO EXISTING DWELLING) ON SURPLUS LAND WITHIN PLOT	MANY TREES, 15 BRUTON PLACE, LLANDAFF, CARDIFF, CF5 2ER	52	True	Planning Permission be refused	11/11/2019
19/02800/MNR	24/10/2019	Llandaff 50plus Group	DISCHARGE OF CONDITION 4 (HISTORIC ENVIRONMENT MITIGATION) OF 19/01605/MNR	FORMER DISUSED PUBLIC TOILETS, CATHEDRAL CLOSE, LLANDAFF	29	True	Full Discharge of Condition	22/11/2019

#### LLDN

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02594/DCH	07/10/2019	Gibson	PROPOSED SINGLE STOREY REAR EXTENSION	36 HAWTHORN ROAD WEST, LLANDAFF NORTH, CARDIFF, CF14 2FL	49	True	Permission be granted	25/11/2019
19/02798/DCH	21/10/2019	O'Connor	ERECTION OF A SINGLE SIDE STOREY EXTENSION. CONVERSION OF FRONT GARDEN TO EXTEND EXISTING DRIVEWAY BY USING PERMEABLE (OR POROUS) SURFACING	108 COLLEGE ROAD, LLANDAFF NORTH, CARDIFF, CF14 2NX	28	True	Permission be granted	18/11/2019

**LLRU**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02475/DCH	18/09/2019	Toft	TWO STOREY SIDE EXTENSION TO CREATE ADDITIONAL BEDROOM AT 1ST FLOOR LEVEL & LIVING ROOM & UTILITY AREA AT GROUND FLOOR.	19 ARNOLD AVENUE, LLANRUMNEY, CARDIFF, CF3 5PP	44	True	Permission be granted	01/11/2019
19/02503/DCH	16/09/2019	Hunt	FIRST STOREY SIDE EXTENSION	96 RIDGEWAY ROAD, LLANRUMNEY, CARDIFF, CF3 4AB	56	True	Permission be granted	11/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02043/MNR	24/07/2019	C/O Agent	PROPOSED EXTENSION TO RUGBY CLUB LOUNGE, PROPOSED REARRANGEMENT AND EXTENSION TO CAR PARK, NEW ACCESS INCLUDING REEN CROSSING.	RUMNEY R F C, HARTLAND ROAD, LLANRUMNEY, CARDIFF, CF3 4JL	111	False	Permission be granted	12/11/2019

**PENY**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/01997/DCH	24/07/2019	Mr & Mrs Song	VARIATION OF CONDITION 4 OF 18/00684/DCH TO AMEND MEANS OF ENCLOSURE	104 CYNCOED ROAD, PENYLAN, CARDIFF, CF23 5SJ	105	False	Permission be granted	06/11/2019
19/02123/DCH	31/07/2019	Stephens	TWO STOREY SIDE EXTENSION AND BAY WINDOW TO REAR	46 CARISBROOKE WAY, PENYLAN, CARDIFF, CF23 9HW	110	False	Permission be granted	18/11/2019

19/01588/DCH	30/05/2019	Thomas	DEMOLITION OF EXISTING GARAGE AND PROPOSED CONSTRUCTION OF A NEW GARAGE WITH ANNEXE ACCOMMODATION	4 ESKDALE CLOSE, PENYLAN, CARDIFF, CF23 5LF	155	False	Planning Permission be refused	01/11/2019
19/01909/DCH	05/07/2019	Vaughan	DOUBLE STOREY SIDE AND FRONT EXTENSION	107 OAK WOOD AVENUE, PENYLAN, CARDIFF, CF23 9EZ	119	False	Planning Permission be refused	01/11/2019
19/02828/DCH	25/10/2019	Mr Manpreet Singh	EXTERNAL ALTERATIONS AT BASEMENT LEVEL TO EXISTING GROUND FLOOR FRONT FLAT	FLAT 1, 335 NEWPORT ROAD, ROATH, CARDIFF, CF24 1RL	33	True	Permission be granted	27/11/2019
19/02902/DCH	01/11/2019	Mr Rashid Khalid	SINGLE STOREY REAR FIRST FLOOR & TWO STOREY SIDE EXTENSIONS WITH LOFT CONVERSION INCLUDING A REAR DORMER AND LOWERING PART REAR GARDEN AND EXTERNAL ALTERATIONS AND GARDEN STORE	9 WASDALE CLOSE, PENYLAN, CARDIFF, CF23 5NY	27	True	Permission be granted	28/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02702/MNR	11/10/2019	Jefferies	INCLUSION OF TWO ADDITIONAL ROOF WINDOWS TO FRONT ELEVATION - PREVIOUSLY APPROVED UNDER 18/02846/MNR	16 LLWYN-Y-GRANT TERRACE, PENYLAN, CARDIFF, CF23 9EW	45	True	Permission be granted	25/11/2019

## PLAS

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02610/DCH	26/09/2019	Pennrise Properties	ROOF EXTENSIONS AND ALTERATIONS TO EXISTING FIRST FLOOR FLAT	163 GLENROY STREET, ROATH, CARDIFF, CF24 3LA	60	False	Permission be granted	25/11/2019
19/02745/DCH	15/10/2019	Homestead Garden Rooms	SINGLE STORY TIMBER OUTBUILDING TO BE USED AS A GARDEN ROOM	61 OAKFIELD STREET, ROATH, CARDIFF, CF24 3RF	41	True	Permission be granted	25/11/2019

19/02645/DCH	18/10/2019	Khan	SINGLE STOREY REAR AND SIDE EXTENSION	107 ARABELLA STREET, ROATH, CARDIFF, CF24 4SX	24	True	Permission be granted	11/11/2019
<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02801/MJR	21/10/2019	DRAYCOTT CONSTRUCTION LIMITED	DISCHARGE OF CONDITIONS 16 (VERIFICATION REPORT), 18 (IMPORTED SOIL) OF 18/01053/MJR	199-209 CITY ROAD, ROATH, CAERDYDD, CF24 3JD	25	True	Full Discharge of Condition	15/11/2019
<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/01608/MNR	29/07/2019	Salih Ali	NEW BUILD, GROUND FLOOR CAR WASH, FIRST FLOOR SHISHA AND DESSERT/HOT FOOD AREA USE	105-109 CITY ROAD, ROATH, CARDIFF, CF24 3BN	108	False	Planning Permission be refused	14/11/2019
19/01850/MNR	27/06/2019	Hartley	TO ERECT A SINGLE DWELLING ON THE SITE WHICH HOUSED A TWO STOREY GARAGE AND UTILISE THE EXISTING ACCESS	REAR OF 13 PEN-Y-WAIN ROAD, ROATH, CARDIFF, CF24 4GD	132	False	Planning Permission be refused	06/11/2019
19/02511/MNR	13/09/2019	219 Ltd	CONVERSION OF A PROPERTY WITH TWO FLATS INTO FOUR FLATS WITH SINGLE AND TWO STOREY REAR EXTENSIONS AND EXTERNAL ALTERATIONS	83 SHIRLEY ROAD, ROATH, CARDIFF, CF23 5HL	59	False	Permission be granted	11/11/2019
A/19/00113/MNR	06/09/2019	Joe's Ice Cream	CIRCULAR ILLUMINATED SIGNAGE APPROX 600MM DIAMETER IN WHITE AND BLUE COMPANY COLOURS	69 WELLFIELD ROAD, ROATH, CARDIFF, CF24 3PA	77	False	Permission be granted	22/11/2019
19/02678/MNR	04/10/2019	Mr Stephen Walters	ALTERATION AND FIRST FLOOR EXTENSION TO PROVIDE ANCILLARY ADMINISTRATIVE ACCOMMODATION AND STAFF AMENITIES	THE CROFTS, CROFT STREET, ROATH, CARDIFF, CF24 3DZ	42	True	Permission be granted	15/11/2019

**PON**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02752/MJR	23/10/2019	Taff Housing Association	AMENDMENT OF SURFACING MATERIALS TO REPLACE THE BRICK PAVING APPROVED FOR THE ACCESS ROAD WITH TARMAC. CAR PARKING SPACES WOULD REMAIN AS BRICK PAVING - PREVIOUSLY APPROVED UNDER 17/01801/MJR	948-950 NEWPORT ROAD, OLD ST MELLONS, CARDIFF, CF3 5UA	22	True	Permission be granted	14/11/2019
19/02650/MJR	03/10/2019	Persimmon Homes East Wales	DISCHARGE OF CONDITION 24 (NOISE) OF 19/00235/MJR	LAND EAST OF CHURCH ROAD AND NORTH AND SOUTH OF, BRIDGE ROAD, OLD ST MELLONS	35	True	Full Discharge of Condition	07/11/2019

**PYCH**

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02303/DCH	09/09/2019	OWEN	PROPOSED SINGLE STOREY EXTENSION TO REAR NEW PITCHED ROOF OVER STUDY AND NEW EXTERNAL FINISHES	2 CLOS CAEWAL, PENTYRCH, CARDIFF, CF15 9QT	63	False	Permission be granted	11/11/2019
19/02646/DCH	01/10/2019	Langmay-Gibbs	SINGLE STOREY PITCHED ROOF REAR EXTENSION	1 TROED-Y-GARTH, PENTYRCH, CARDIFF, CF15 9AB	41	True	Permission be granted	11/11/2019
19/02729/DCH	10/10/2019	DAVIES	SINGLE STOREY FRONT BAY EXTENSION	HAZELHURST, PEN-Y-WAUN, PENTYRCH, CARDIFF, CF15 9SJ	33	True	Permission be granted	12/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/02575/MNR	20/09/2019	Mr Martin Lewis	PROPOSED CONVERSION / CHANGE OF USE OF STABLE BLOCK TO RESIDENTIAL (GRANNY FLAT)	SOAR COTTAGE, EFAIL ISAF ROAD, PENTYRCH, CARDIFF, CF15 9NQ	53	True	Planning Permission be refused	12/11/2019
19/02762/MNR	16/10/2019	Short Bros (Developments) Ltd	REVISED SITE LAYOUT AND BUILT ENVELOPE TO FACILITATE GAS EASEMENT - PREVIOUSLY APPROVED UNDER 18/02759/MNR	LAND AT FAIRFIELD INDUSTRIAL ESTATE, MAIN ROAD, GWAELOD-Y-GARTH, CARDIFF, CF15 8LA	16	True	Permission be granted	01/11/2019

## RADY

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02741/DCH	14/10/2019	Ayles	ERECTION OF SINGLE AND TWO STOREY EXTENSION TO EXISTING 3 BEDROOM DETACHED HOUSE TO CREATE 5 BEDROOM FAMILY HOME	17 DAN-Y-BRYN AVENUE, RADYR, CARDIFF, CF15 8DD	32	True	Permission be granted	15/11/2019
19/02718/DCH	11/10/2019	Ip-Chung	SINGLE STOREY SIDE (SINGLE PITCHED) AND REAR (DUO-PITCHED) EXTENSION. REMOVAL OF THE BOUNDARY FENCE LINE AND REPLACE WITH BRICKWORK WALL WHICH WILL FORM PART OF THE EXTERNAL LEAF OF THE SIDE/REAR EXTENSION	75 ARUDUR HEN, RADYR, CARDIFF, CF15 8FX	38	True	Permission be granted	18/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02814/MNR	25/10/2019	Vigliotta	INCREASE IN SIZE OF SINGLE REAR UPSTAIRS WINDOW, REMOVAL OF SINGLE DOWNSTAIRS WINDOW AND ALTERED POSITION OF DOORS - PREVIOUSLY APPROVED UNDER 12/01104/DCO	TY NEWYDD, CAE'R GRAIG, RADYR, CARDIFF, CF15 8RD	25	True	Permission be granted	19/11/2019

## RHIW

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02122/DCH	08/08/2019	Souto	EXTENSION AND ALTERATIONS INCLUDING THE CONSTRUCTION OF A NEW PITCHED ROOF TO NOS 147-149 PEN-Y-DRE	147-149 PEN-Y-DRE, RHIWBINA, CARDIFF, CF14 6EN	105	False	Permission be granted	21/11/2019
19/02592/DCH	26/09/2019	Wynne	PROPOSED SINGLE STOREY SIDE EXTENSION TO THE EXISTING SEMI DETACHED DWELLING	52 HEOL-Y-NANT, RHIWBINA, CARDIFF, CF14 6BT	56	True	Permission be granted	21/11/2019
19/02616/DCH	27/09/2019	SHEPPARD	REAR SINGLE STOREY EXTENSION, REAR DORMER WITH RAISED RIDGE DORMER	1 LEAMINGTON ROAD, RHIWBINA, CARDIFF, CF14 6BX	47	True	Permission be granted	13/11/2019
19/02499/DCH	11/09/2019	Salim	ALTERATION TO EXISTING ROOF TO FORM FIRST FLOOR ACCOMMODATION	77 TY'N-Y-PARC ROAD, RHIWBINA, CARDIFF, CF14 6BJ	72	False	Permission be granted	22/11/2019
19/02118/DCH	06/08/2019	GERRISH	REAR SINGLE STOREY EXTENSION, SIDE GARAGE CONVERSION, SIDE PORCH, LOFT CONVERSION WITH HIP TO GABLES AND REAR DORMER. NEW HIGHWAY ENTRANCE TO FRONT ELEVATION	19A HEOL-Y-COED, RHIWBINA, CARDIFF, CF14 6HQ	87	False	Permission be granted	01/11/2019
19/02607/DCH	27/09/2019	Elliott	THE CONSTRUCTION OF A SEMI-DETACHED GARAGE AND A DETACHED EXTERNAL OUTBUILDING TO REAR OF GARDEN	ALAUNA, LON-Y-WINCI, RHIWBINA, CARDIFF, CF14 6UG	63	False	Permission be granted	29/11/2019
19/02665/DCH	04/10/2019	Summerfield	VARIATION OF CONDITION 2 TO CHANGE BUILDING FORM TO AVOID THE INSPECTION CHAMBER BY 500MM ON ALL SIDES IN RESPECT OF PREVIOUS PERMISSION NUMBERED 18/02713/DCH	5 CLOS YR AER, RHIWBINA, CARDIFF, CF14 6NH	56	True	Permission be granted	29/11/2019
19/02515/DCH	16/09/2019	Brace	PROPOSED CONVERSION OF THE EXISTING LOFT SPACE, INCLUDING INSERTION OF REAR DORMER AND MINOR INTERNAL & EXTERNAL ALTERATIONS	48 HEOL UCHAF, RHIWBINA, CARDIFF, CF14 6SP	74	False	Permission be granted	29/11/2019

19/02799/DCH	23/10/2019	Katherine Hopkins and Doug McLellan	ALTERATIONS AND SINGLE STOREY REAR EXTENSION AND EXTERNAL RAISING PATIO/DECKING AREA	13 CAE'R WENALLT, PANTMAWR, CARDIFF, CF14 7HN	36	True	Permission be granted	28/11/2019
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## RIVE

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02138/DCH	31/07/2019	LAVALETTE	PROPOSED LOFT CONVERSION - TO INCLUDE A PITCHED ROOF REAR DORMER, WITH ONE ADDITIONAL CONSERVATION STYLE VELUX WINDOW TO THE REAR ELEVATION	19A CONWAY ROAD, PONTCANNA, CARDIFF, CF11 9NT	105	False	Permission be granted	13/11/2019
19/02145/DCH	01/08/2019	Mr Ian Browne	REPLACEMENT WINDOWS TO THE FRONT ELEVATION	65 PONTCANNA STREET, PONTCANNA, CARDIFF, CF11 9HR	97	False	Permission be granted	06/11/2019
19/02253/DCH	13/08/2019	Bryant	SINGLE STOREY SIDE RETURN EXTENSION AND REAR EXTENSION. NEW DOOR AND WINDOWS TO FRONT ELEVATION	91 SEVERN GROVE, PONTCANNA, CARDIFF, CF11 9EQ	94	False	Permission be granted	15/11/2019
19/02660/DCH	03/10/2019	Durnell	PROPOSED SINGLE STOREY REAR/SIDE EXTENSION	49 PONTCANNA STREET, PONTCANNA, CARDIFF, CF11 9HR	43	True	Permission be granted	15/11/2019
19/02697/DCH	07/10/2019	Hart	PROPOSED SINGLE STOREY REAR KITCHEN EXTENSION	66 LLANFAIR ROAD, PONTCANNA, CARDIFF, CF11 9QB	49	True	Permission be granted	25/11/2019
19/02738/DCH	11/10/2019	GHAZAL	ERECTION OF SHED - PREVIOUSLY APPROVED UNDER 19/01623/DCH	1 SIR WALTER MEWS, SEVERN GROVE, PONTCANNA, CAERDYDD, CF11 9EQ	28	True	Permission be granted	08/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/02630/MNR	01/10/2019	Hecksagon Ltd	VARIATION OF CONDITIONS 2 (APPROVED DRAWINGS) AND 6 (NORTH WEST WINDOW) OF 18/03003/MNR	68 WYNDHAM CRESCENT, PONTCANNA, CARDIFF, CF11 9EF	43	True	Permission be granted	13/11/2019
19/02869/MNR	30/10/2019	Vodafone Ltd	ALTERNATIVE ANTENNA SUPPORT POLES TO ALLOW FOR CLIMBING ACCESS. AMENDED REMOTE RADIO UNIT REQUIREMENTS TO MAXIMISE EFFICIENT OPERATION OF SITE AND DATA HANDLING CAPABILITIES. ADDITIONAL CABLE MANAGEMENT ACCESSORIES AND ADDITIONAL HAND RAILING ON MAIN ROOF - PREVIOUSLY APPROVED UNDER 19/00600/MNR	T-MOBILE (92744) AT ROOF TOP ROMBOURNE LTD, 14 CATHEDRAL ROAD, PONTCANNA	28	True	Permission be granted	27/11/2019

#### RUMN

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02498/DCH	11/09/2019	Cleveland	REMOVAL OF EXISTING CONSERVATORY AND REAR EXTENSION AND CONSTRUCTION OF A NEW SINGLE STOREY REAR EXTENSION	22 WENTLOOG ROAD, RUMNEY, CARDIFF, CF3 3LP	55	True	Permission be granted	05/11/2019
19/02207/DCH	14/08/2019	Monteverde	PROPOSED TWO STOREY SIDE EXTENSION, REAR DORMER EXTENSION AND GARDEN STORAGE EXTENSION TO GARAGE	22 MEADVALE ROAD, RUMNEY, CARDIFF, CF3 1UF	85	False	Permission be granted	07/11/2019

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/02947/MJR	07/11/2019	ELMYA	TO AMEND CONDITIONS 3, 4, 5 AND 16 IN RESPECT OF THE FENCING - PREVIOUSLY APPROVED UNDER 19/00397/MJR	LAMBY WAY LANDFILL SITE, LAMBY WAY, WENTLOOG, CARDIFF, CF3 2HP	11	True	Permission be granted	18/11/2019
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<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/02676/MNR	04/10/2019	Mr Paul Anderson	DETACHED DWELLING	LAND AT 6 PWLL MAWR AVENUE, RUMNEY, CARDIFF, CF3 3HH	49	True	Permission be granted	22/11/2019
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### SPLO

<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
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19/02642/MJR	02/10/2019	Bayliss Recovery Ltd	VARIATION OF CONDITION 3 TO INCREASE PART OF THE INTERNAL FENCE HEIGHT AND VARIATION OF CONDITION 5 TO AMEND THE APPROVED PLAN LAYOUT IN RESPECT OF PREVIOUS PERMISSION NUMBERED 14/01870/DCI	BAYLISS METALS RECYCLING CENTRE, TREMORFA INDUSTRIAL ESTATE, MARTIN ROAD, EAST MOORS, CARDIFF, CF24 5SD	44	True	Permission be granted	15/11/2019
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19/02772/MJR	28/10/2019	Princes Ltd	REMOVAL OF CONDITIONS 12, 13 & 14 OF 12/00959/DCI RELATING TO BREEAM	PRINCES SOFT DRINKS UNIT 68-69 AND 72, PORTMANMOOR ROAD INDUSTRIAL ESTATE, PORTMANMOOR ROAD, SPLOTT, CARDIFF, CF24 5HB	4	True	Permission be granted	01/11/2019
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19/02773/MJR	28/10/2019	Princes Ltd	A REDUCTION IN SIZE OF THE OFFICE / WELFARE ELEMENT ADJOINING PORTMANMOOR ROAD BOTH IN OVERALL FOOT PRINT AND HEIGHT. THE OMISSION OF A SIDE LOAD CANOPY TO THE NORTH YARD IN FAVOUR OF 2NO DOCK LEVELLERS. A NEW TRANSFORMER AND SWITCH ROOM EXTENSION ADJACENT TO THE EXISTING PLANT ROOM BUILDING OFF PORTMANMOOR ROAD A MINOR ALTERATION TO THE ROOF FORM ALONG THE GABLE END WITHIN THE SOUTH YARD OMITTING A CANOPY AREA IN THE SOUTH EAST CORNER PLUS AN EXTENSION OF ONE BAY ON THE GABLE BY NOMINALLY 500MM - PREVIOUSLY APPROVED UNDER 12/00959/DCI	PRINCES SOFT DRINKS UNIT 68-69 AND 72, PORTMANMOOR ROAD INDUSTRIAL ESTATE, PORTMANMOOR ROAD, SPLOTT, CARDIFF, CF24 5HB	8	True	Permission be granted	05/11/2019
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<u>Application Number</u>	<u>Registered</u>	<u>Applicant Name</u>	<u>Proposal</u>	<u>Location</u>	<u>Days taken to decision</u>	<u>8 Week target Achieved?</u>	<u>Decision</u>	<u>Decision Date</u>
19/02340/MNR	23/08/2019	Doherty	RETROSPECTIVE APPLICATION FOR A GARAGE TO THE REAR OF 71 SPLOTT ROAD	GARAGE TO THE REAR OF 71 SPLOTT ROAD, SPLOTT, CARDIFF, CF24 2BX	84	False	Permission be granted	15/11/2019
19/02388/MNR	09/09/2019	Zurich Assurance Limited	NEW WIDER ENTRANCE GATE TO IMPROVE LOADING ACCESS. NEW TALLER ROLLER SHUTTER DOOR TO IMPROVE LOADING ACCESS TO WAREHOUSE. SMALL SECTION OF EXISTING BRICK BIN STORE REMOVED. EXISTING CANOPY CURRENTLY SUPPORTED OFF BIN STORE WALL ALTERED TO PROVIDE NEW SUPPORT. REMOVAL OF EXISTING BOUNDARY TREE TO IMPROVE ACCESS	UNIT 42, PORTMANMOOR ROAD INDUSTRIAL ESTATE, PORTMANMOOR ROAD, SPLOTT, CARDIFF, CF24 5HB	57	False	Permission be granted	05/11/2019

19/02722/MNR	09/10/2019	Cardiff Council - Lifelong learning (Education)	REMOVE EXISTING DEMOUNTABLE UNIT AND REPLACE WITH A NEW TEMPORARY (DEMOUNTABLE UNIT). THE UNIT WILL CONSIST OF A LARGE COMMERCIAL KITCHEN WITH FOOD STORE, STAFF CHANGING ROOM, SINGLE WC AND A 180 CAPACITY DINING AREA. A PEDESTRIAN ZONE IS PROVIDED TO ACCESS THE DEMOUNTABLE UNIT. NEW FOOTPATHS WILL BE PROVIDED TO ACCESS THE REAR PLAYGROUND.AND FOR FUTURE MAINTENANCE	BADEN POWELL PRIMARY SCHOOL, MUIRTON ROAD, TREMORFA, CARDIFF, CF24 2SJ	47	True	Permission be granted	25/11/2019
19/02466/MNR	09/09/2019	Cardiff Council	PROVIDE A NEW 3G FOOTBALL PITCH ON AN EXISTING BASKETBALL COURT. REMOVE EXISTING BASKETBALL HARD SURFACING & BASKETBALL POST. INSTALL A NEW PERMEABLE 40x22m ARTIFICIAL 3G SURFACE. 5m HIGH MESH FENCING & GATES. 8m HIGH FLOODLIGHTS. LEVEL ACCESS FROM PATHWAY TO PITCH	SPLOTT PARK, MUIRTON ROAD, TREMORFA, CARDIFF, CF24 2SJ	70	False	Permission be granted	18/11/2019
A/19/00120/MNR	30/09/2019	Collins	9NO NON-ILLUMINATED FOLDED DIBOND PANELS, 16NO WINDOW GRAPHICS, 4NO DOOR GRAPHICS, 2NO FLAT DIBOND PANELS MOUNTED TO POSTS	ENCON INSULATION UNIT 4, TRIDENT TRADE PARK, GLASS AVENUE, SPLOTT, CARDIFF, CF24 5EP	49	True	Permission be granted	18/11/2019

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19/02490/DCH	11/09/2019	Johnson	DEMOLITION OF REAR SECTION OF BUNGALOW & DETACHED GARAGE & CONSTRUCTION OF SINGLE STOREY REAR EXTENSION & REPLACEMENT GARAGE.	48 PARK AVENUE, WHITCHURCH, CARDIFF, CF14 7AN	71	False	Permission be granted	21/11/2019

19/02162/DCH	05/08/2019	Kelly	WINDOW REPLACEMENT THROUGHOUT PROPERTY	17 ALFREDA ROAD, WHITCHURCH, CARDIFF, CF14 2EH	108	False	Permission be granted	21/11/2019
19/02709/DCH	09/10/2019	Wilding	SINGLE STOREY DETACHED STORAGE SHED	9 CLAS DYFRIG, WHITCHURCH, CARDIFF, CF14 1JN	44	True	Permission be granted	22/11/2019
19/02539/DCH	18/09/2019	Walters	ADDITIONAL SIDE ELEVATION WINDOW IN BEDROOM 3	16 HEOL DON, WHITCHURCH, CARDIFF, CF14 2AU	48	True	Permission be granted	05/11/2019
19/02536/DCH	17/09/2019	CARDIFF COUNTY COUNCIL	CONSTRUCTION OF A SINGLE STOREY EXTENSION TO THE REAR OF THE PROPERTY TO INCREASE THE SIZE OF THE EXISTING BATHROOM.	25 MAES GLAS, WHITCHURCH, CARDIFF, CF14 1NW	45	True	Permission be granted	01/11/2019
19/02370/DCH	29/08/2019	SAMPSON	REAR EXTENSION	21 WELLINGTON STREET, TONGWYNLAIS, CARDIFF, CF15 7LP	88	False	Permission be granted	25/11/2019
19/02870/DCH	31/10/2019	Harris	CREATION OF A DORMER AND ASSOCIATED INTERNAL WORKS TO THE REAR ELEVATION OF AN EXISTING BUILDING	227 COLLEGE ROAD, WHITCHURCH, CARDIFF, CF14 2NU	19	True	Permission be granted	19/11/2019
19/02862/DCH	29/10/2019	Mistry	REMOVAL OF EXISTING GARAGE AND CONSTRUCTION OF A SINGLE STOREY REAR EXTENSION AND HIP TO GABLE LOFT CONVERSION	37 HEOL GABRIEL, WHITCHURCH, CARDIFF, CF14 1JU	30	True	Permission be granted	28/11/2019
19/02795/DCH	28/10/2019	COLES	A HIP TO GABLE EXTENSION WITH REAR DORMER TO ATTIC	58 TY NEWYDD, WHITCHURCH, CARDIFF, CF14 1NQ	28	True	Permission be granted	25/11/2019
19/02831/DCH	25/10/2019	Fear	THE ADDITION OF TWO SUNPIPES, ADDITION OF SIDE WINDOWS AND AMENDMENT TO JULIET BALCONY BALUSTRADE DETAILS	71 BISHOP'S ROAD, WHITCHURCH, CARDIFF, CF14 1LW	12	True	Permission be granted	06/11/2019

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19/02213/MNR	10/08/2019	Woods	DISCHARGE OF CONDITION 17 (SURFACE WATER DISPOSAL) OF 17/02775/MNR	43 SILVER BIRCH CLOSE, WHITCHURCH, CARDIFF, CF14 1EL	101	False	Full Discharge of Condition	19/11/2019
19/02185/MNR	08/08/2019	Woods	DISCHARGE OF CONDITIONS 10 (GASES), 11 (CONTAMINATION), 12 (REMEDICATION), 13 (REMEDICATION IMPLEMENTATION) AND 14 (CONTAMINATION SCHEME) OF 17/02775/MNR	43 SILVER BIRCH CLOSE, WHITCHURCH, CARDIFF, CF14 1EL	103	False	Partial Discharge of Condition (s)	19/11/2019
19/02771/MNR	16/10/2019	Casemorgan Developments	DISCHARGE OF CONDITION 6 (EXTERNAL FINISHING MATERIALS) OF 18/01092/MNR	GREENMEADOWS, 102 PENDWYALLT ROAD, WHITCHURCH, CARDIFF, CF14 7EH	28	True	Full Discharge of Condition	13/11/2019

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